

**MEETING SUMMARY  
FOR  
PUBLIC MEETING**

**PEARL RIVER BRIDGES  
AND  
EAST PEARL RIVER BRIDGE  
STATE PROJECT NO. H.000284 and NO. H.000286  
FEDERAL AID PROJECT NO. H000284 AND NO. H000286  
ST. TAMMANY PARISH, LOUISIANA  
AND  
HANCOCK COUNTY, MISSISSIPPI**

**Open House  
Thursday, August 22, 2019  
4:00 pm – 7:00 pm  
Pearlington Recovery Center  
5265 Hwy. 604  
Pearlington, MS 39572**





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# **OPEN HOUSE PUBLIC MEETING NOTICE**

# OPEN HOUSE PUBLIC MEETING

## H.000284 US 90 PEARL RIVER BRIDGES (HBI) St. TAMMANY PARISH, LOUISIANA AND HANCOCK COUNTY, MISSISSIPPI

The Louisiana Department of Transportation and Development (LADOTD) and the Mississippi Department of Transportation (MDOT) Federal Highway Administration (FHWA) will conduct a Public Meeting in support of the rehabilitation or replacement of the five US 90 bridges crossing the Pearl River waterways. The project limits extend along US 90 from US 190 in Louisiana to US 90 intersection with Kelly Road/7th Avenue in Pearlington, Hancock County.

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Should anyone require special assistance due to a disability to participate in this Open House Public Meeting, please contact Ms. Cathy Mastin of the LADOTD Project Management Section, P.O. Box 94245, Baton Rouge, Louisiana 70804, or by telephone at (225) 379-1652 at least five working days prior to the Public Meeting.

US 90 Bridges  
Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

# **— OPEN HOUSE PUBLIC MEETING —**

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Photo courtesy of the Waveland Police Department. Officers of the Waveland Police Department recovered a great deal of property stolen from unlocked cars in the city, including handguns, credit cards, cameras, sunglasses and a laptop computer. Police urge residents to always lock their cars, trucks and SUVs to prevent theft, especially when a firearm is in the vehicle.

## Waveland P.D. recovers guns, other valuables stolen from unlocked vehicles

BY GEOFF MELCHER  
NEWS EDITOR

Waveland investigators this week arrested two juveniles who allegedly stole several guns, credit cards and other loot from unlocked vehicles. "We were very fortunate

we were able to recover those firearms," Waveland Police Chief Mike Prendergast said Tuesday. "Investigator Eddie Hursey and Investigator Chad Dorn put a lot of hours into this. We made the arrests yesterday. The suspects were both 16-year-old boys. More arrests are possible. ... One of the guns

recovered was from out in the county, so apparently these guys were hitting cars out in the county, too." Prendergast said many burglars seek unlocked vehicles or houses because they are easy targets. "Always lock your car, doors and windows," Prendergast said. "People need to be more careful."

## Field House

Continued from Pg. 1A

Patano Engineering + Architecture Design Group, told the board last month.

As per the construction schedule, the field house should be completed by the beginning of next football season, a construction length of 10 months, Patano said.

Patano said that the new field house will be about 10,000 square feet and will feature some of the following items: A locker room; weight room; a separate co-ed training room; coaches office with showers; and two meeting spaces/classrooms.

The groundbreaking is scheduled for 10 a.m.

Saturday.

Prior to the groundbreaking, the district will host media day for the football players from 8 to a.m. Events include an inter-squad scrimmage, newspaper interviews, and player photos.

In other action: The board on Monday accepted several of the lowest quotes for the following:

Replace existing hand sinks in the 300 building of Hancock North Central Elementary School from Marks Plumbing in the amount of \$11,878.

The removal of existing manual winches and replacement of electric winches for all basketball goals at Hancock High School from Hahn Enterprise, Inc. in the amount of \$5,928.

Upgrades to the current fire alarm system at the D

building of Hancock High School from Rebel Sound in the amount of \$49,705.

Certified playground mulch for the elementary schools from Bourgeois Trucking in the amount of \$23,250.

Roof repairs at Hancock Middle School from Charlie Stroud in the amount of \$15,226.

Upgrade and replace security cameras in all four elementary schools, the Winzone, and the Career Technical Center from Omni Technologies in the amount of \$22,361.

Purchase 120 Chromebook computers, 120 Google Chrome licenses and four charging carts from CDW-G in the amount of \$32,928.72

The next meeting is scheduled for Sept. 5 at 5:30 p.m. at the district office, located at 17304 Hwy. 603, KJN.

## Bark Park

Continued from Pg. 1A

Waveland City Clerk Mickey Lagasse said the dog park is located adjacent to the softball field on Central Avenue.

The park features waste stations, exercise equipment for dogs, and two separate parks for small and large dogs, he said.

There are also shaded areas with benches and a non-working fire hydrant, Lagasse said.

Rules for the dog park are posted and include:

• Use park at your own risk.

• Owners are legally responsible for the behavior of their dog(s) at all times.

• Dogs must be leashed while entering and exiting the park.

• Dog waste must be cleaned up by their owners immediately.

• Owners must be within the dog park and supervising their dog with leash

readily available.

• Dog handlers must be at least 16 years of age.

• Children under 13 must be accompanied by an adult and supervised at all times.

• Aggressive dogs must be removed immediately.

• Dogs should be under voice control.

Things that are prohibited include:

• Human and dog food/treats.

• Glass containers.

• Dogs in heat.

• Sick dogs.

• Aggressive dogs.

• Puppies (under four months).

The dog park's listed hours are 7 a.m. to 3 p.m. and the park is closed the first Wednesday of each month for maintenance.

During this week's trial period, Smith said, the response to the new park has been a positive.

"The only issue we've had is people not cleaning up after their dogs," he said.

The city plans to schedule a ribbon cutting at the park in the near future.

## Misfits Street Krewzers host Cruise-in

The Misfits Street Krewzers will host a classic car Cruise-In at Choctaw Plaza next to Arby's in Waveland this Saturday, Aug. 10, from 5-8 p.m. Check out the classic cars and listen to the "Oldies." Please bring a non-perishable food item for King's Kitchen.

## Reform

Continued from Pg. 4A

decide if that certification is important.

Because if we really want more jobs and a smaller government footprint, it starts by creating an environment that encourages work; not one that encourages the creation of hurdles and obstacles.

Brett Kittredge is the Director of Marketing and Communications for Mississippi Center for Public Policy, the state's non-partisan, free-market think tank.

## Rotary Satellite Club hosts Beach Trash Pick up Saturday

Join the Rotary of Bay St. Louis St. Louis Satellite Club this Saturday, Aug. 10, from 8-9 a.m., for a Beach Trash Pick up.

Participants are asked to meet at the Washington Street Pavilion. Bring your own gloves and grabbers. Bags will be available.

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## Local Eye Doctor Helps Visually Impaired To See

Advanced Technology Allows Many To See Better or Drive Again



Grandchildren as viewed by a person with Macular Degeneration

Some scene of grandchildren as viewed without Macular Degeneration

For many patients with macular degeneration, diabetic retinopathy and other conditions that cause low vision, the loss of vision can signal the end of independence and the joy of life. Dr. Rod Fields prescribes miniature telescope glasses to help people to see better.

"Many of my patients consider me their last chance for vision improvement" said Dr. Fields, one of few doctors specially trained by the International Academy of Low Vision Specialists.

Imagine special glasses that can improve your vision enough to change your life.

Custom designed telescopic glasses are an advanced optical technology that can give you back your independence.

Special glasses designed by Dr. Fields can also help those with side vision loss from stroke.

"My job is to figure out everything and anything possible to allow a person to see better" says Dr. Fields.

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(228) 200-5921

Dr. Rod Fields  
Low Vision Optometrist  
SouthernLowVision.com



# West Nile Virus: Five Human Cases in Mississippi So Far in 2019

**SEA COAST ECHO**

Today the Mississippi State Department of Health (MSDH) reports two new human case of West Nile virus (WNV) for 2019, bringing the state total to five this year. The new cases were reported in Forrest and Lamar counties. Three previous cases were reported in Smith,

Hinds and Copiah counties in July.

In 2018, Mississippi had 50 WNV cases and no deaths. The first case was reported in mid-July.

"While most Mississippians are aware of the existence of West Nile virus, it's easy to forget how dangerous it can be. We need to remember that we have WNV cases in Mississippi every year, and

that everyone needs to act now to reduce their risk of infection regardless of where they live in the state. Most cases occur from July through September," said MSDH State Epidemiologist Dr. Paul Byers. "While most infected people recover without any long-term problems, some develop a more severe infection that can lead to complications and

even death - especially in those over 50 years of age."

Symptoms of WNV infection are often mild and may include fever, headache, nausea, vomiting, a rash, muscle weakness or swollen lymph nodes. In a small number of cases, infection can result in encephalitis or meningitis, which can lead to paralysis, coma and possibly

death.

The MSDH suggests the following precautions to protect yourself and your family from mosquito-borne illnesses:

Use a mosquito repellent with an EPA-registered ingredient such as DEET while you are outdoors.

Remove all sources of standing water around your home and yard to prevent mosquito breeding.

Wear loose, light-colored, long clothing to cover the arms and legs when outdoors.

Avoid areas where mosquitoes are prevalent. For more information on mosquito-borne illnesses, visit the MSDH website at [HealthyMS.com/west-nile](http://HealthyMS.com/west-nile).

Follow MSDH by email and social media at [HealthyMS.com/connect](http://HealthyMS.com/connect).

**• PAYMENT**

Continued from Page 1A

speaking with Moran's attorney, Fredrick Feeney, II, the city item that he or the board could speak about was the matter of the July invoice.

Moran said that for the past two-and-a-half years, PPC has turned in its invoice the week before the second board meeting.

"And we did that in July, it should have been put on the docket to be paid in August at the first meeting," Moran said. "This report would technically be turned in next week because you all meet the following Monday, but we turned it in this week, a week early, just as we have done for two-and-a-half

years. Now, as far as the complaints, we've never heard of that on the contract before. That's never been asked before."

Yarborough asked Moran if PPC had received any complaints for the month of July.

Moran said he would have to go back and look at PPC's log.

"But that's never been asked before, never been a part of it," Moran said. "If you go back and look, there's never been a complaint filed with the report in two-and-a-half years. Don't know where it came from."

The Sea Coast Echo — via a public records request — obtained a copy of the contract between the county and PPC.

According to page seven of the contract, under

PPC operations, "All complaints shall be made directly to PPC and shall be given prompt and courteous attention. All complaints received must be responded to by PPC within 48 hours, excluding weekends and holidays. PPC shall provide the county with a full explanation of the disposition of any complaint involving a customer's chain of damage to private property as the result of actions of contractor's employees or agents."

On page eight of the contract, it states that PPC shall submit the following reports to the county:

• P P C shall submit a weekly report which shall include larval/cove counts, light trap counts, adult/catching activities, and complaints.

• P P C shall submit a monthly report of activities to reflect mosquito abatement work accomplished.

• In addition, PPC shall submit an annual report of all above categories.

On page 10 of the contract, under Basis and Method of Payment, it states, "PPC shall bill the county monthly for services rendered, with the bill to be submitted in advance of the county's docket deadline in order for it to be considered by the county for approval and payment at its first regular meeting of the following month with payment made by the 20th of the month. Such billing and payment shall be based on 1/12 of the annual contract rate set

forth in the contract documents plus any approved additional compensation. PPC's bill shall have attached a monthly report of all activities completed, complaints received, and their disposition for the billing period. In the event the county contests an invoice, it will notify PPC of the basis of the contest and non-payment by the 20th of the month following the date of the invoice submitted."

The contract was signed by board president Blaine LaFontaine and Moran on Oct. 3, 2016, with a termination date of Sept. 30, 2020.

An amendment to the contract for mosquito control services was signed on Jan. 17, 2017 by LaFontaine and Moran and states "remove the obliga-

tion for PPC to provide weekly reporting and to only require PPC to provide monthly reporting. Hancock County Board of Supervisors, however, reserves its right to in the future require weekly reporting to the extent it may deem it necessary for the public health and safety of the citizens of Hancock County."

Supervisor Bo Ladner made a motion to pay PPC's invoice with the contingency that Moran bring in a report about complaints. The motion passed four to one. Ladner, Supervisors Scotty Adam, Greg Shaw, and David Yarborough voted "yes." LaFontaine voted "no."

The next regular meeting is scheduled for Aug. 19 at 9 a.m.

**• MILESTONES**

Continued from Page 4A

Despite the losses, my sister and brother-in-law decided to try again, this time with a great doctor in New York. They move frequently because my brother-in-law is a member of the U.S. Coast Guard.

I believe it was meant for them to be in New York so that she would be under great care to bring Nate into the world.

I became an aunt the day Evan was born and passed away. Not a day goes by that I don't think about what Evan and Gracie would have looked

like now or what their personalities would be like.

The journey leading up to Nate's arrival is probably one of the main reasons he is always the center of our world.

Of course, I think most are grateful for the gift of new life.

But I think we are more aware of how precious this gift of life is to our family.

Nate's arrival is one of, if not the sole reason, for my dad's desire to fight kidney failure for six years. He never lost hope that one day he would receive a kidney transplant so that he could watch Nate grow up.

Nate is old enough to understand a lot. Prior to my dad's transplant, Nate knew that Paw Paw would

have to leave him for a few hours to go to dialysis.

During his latest visit, Nate was glad to learn that Paw Paw had more free time to spend with him.

According to the March of Dimes website, more than "half a million babies are born premature or with birth defects in the U.S. each year."

There are many strides being made to ensure safe and healthy deliveries for mothers and babies and I hope that those numbers start to decline.

Modern medicine is a wonderful thing and without some of these strides and information, our family might not have a Nate.

For those of you who struggle with either infert-

tility problems or other pregnancy problems, I would say to you to not lose hope.

There are so many options available to those who want to have a family of their own and experience difficulties.

Our hope, no matter how small or tested sometimes, is one of the reasons why we will all celebrate Nate's birthday with my dad and his new lease on life.

Learn more about March of Dimes at [www.marchofdimes.org](http://www.marchofdimes.org).

Cassandra is a staff writer at the Sea Coast Echo and can be reached at [cassandra@seacoastecho.com](mailto:cassandra@seacoastecho.com).

**• BIRTHDAY**

Continued from Page 4A

age, which is nothing for his age.

As I tell my wife, I now like to think of my heart arteries as Roman aqueducts lined with gleaming polished protective calcium.

A new study looks at calcium density versus volume. Turns out high calcium density is actually protective, counteracting the risky volume. The current Agatston scoring calculation is volume times density which can be deceptive. This is similar to how we gradually learned there is a good cholesterol as well as a bad cholesterol.

I was explaining all this to my doctor and friend Scott Carlton who responded, "Okay, so now what are you going to die from?" Good point, a sudden heart attack isn't a bad way to go.

There were other big changes in my life this year. After 25 days of flying, I sold my airplane. There were many reasons. For one, I just wasn't flying enough. The thrill was gone. Been there, done that. And the tax depreciation had run out.

I thought by now I would need an airplane to fly to my far flung media empire, but Google, Facebook and Big Data nixed that dream. My newspapers are nearby and it's really as easy to drive as fly. Plus advances in

communication have made it much easier to monitor a business with online reports and data. It's hard to think all my accumulated flying knowledge is to go to waste. But in the end, all your knowledge goes to waste.

N210BA is now half way to Thailand, where it was purchased by an Australian real estate entrepreneur. Amazingly, I sold it for more than I paid for it 15 years ago. Brewer Pearson was a great broker. My partner Judson Colley is a prince of a guy. We never once had a harsh word over 10 years.

So far, I don't miss it, except for moments of longing every now and then when I hear an airplane engine and look up into the sky.

In truth, golf killed my flying. As a child, I loved golf, but gave it up when my own children were born. Now as my children are grown, golf is back in my life. I had no extra time so something had to give. Golf is far less expensive than flying. I don't think I would have taken up golf if I had not realized I could hit plastic golf balls off my brick patio into an open area behind my backyard. These balls are called Almost Golf balls and they go about a third as far as a real golf ball yet the flight path is true allowing you to see the result of your swing. I video record myself with my camera on a tripod and then compare to pro swings online.

It took me about a year and a half, but I am now breaking 80, better golf than when I was a teenager on the Greenwood High School golf team. You have to study the proper swing and then force yourself to swing the way the pros do, even if it feels awkward. There are many ways to swing a golf club, but if you follow conventional wisdom you increase the probability of scoring well.

I am still playing tons of tennis, which according to a recent massive study, prolongs your life by 9.3 years, three times longer than any other physical activity. I am totally mainstream now: golf, tennis, yard work and guitar. I love the beauty of a golf course. If you can really practice and master the swing, it's actually pleasant and fun. I have a whole new set of golfing buddies and the comradery is great. Jackson is a great golf town with tons of affordable courses.

When people ask me how things are going, I say, "Nobody's in the hospital or jail, so things are great." I've had an interesting ride so far, but when your 21-year-old hellion calls you on your birthday and tells you how much he loves and respects you and apologizes for the rough patches, it puts it all in perspective. My daughter is a dream. Son John means my life will never be boring. And Ginny still pulls my heart strings.

I was hoping to be rich by now, but it didn't turn

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Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

**Subject:**

**FW: Open House Public Meeting | Rehabilitation or Replacement of 5 Bridges on US 90  
Crossing Pearl River Waterways**

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# **OPEN HOUSE ATTENDANCE RECORD**

PROJECT TEAM

NAME	ADDRESS	PHONE	EMAIL	REPRESENTING
Stacie Palmer	LADOTD - HQ - Baton Rouge			LADOTD
Cathy Martin	LADOTD - Baton Rouge			LADOTD
Adam Johnson	MDOT - ENN			MDOT
Nathan Green	MDOT - MUD			MDOT
Carole Foyard	MDOT - D6			MDOT
Tonya Smith	NSI			m NSI
Marsha Swin	LADOTD HR			LADOTD
Ryan Fenner	LA DOTD			LA DOTD
Andrew Michael	LA DOTD			LADOTD
Mathilda Rilovich	LADOTD			LADOTD
Barry Brubacher	NSI			NSI
Alexandra Mai Nguyen	NSI			NSI
Melissa Stephens	LADOTD R/W			LA DOTD
Connie Standise	Neel-Schaffer			haffers.com NSI
Rhonda Smith	<del>NSI</del>			conn ESI
KREG OVERSTREET	Neel-Schaffer			NSI
Wm. Kenneth Dean	US EPA Region 4			USEPA
Ashum Brock	MDOT H'burg			MDOT

Phone numbers and email addresses redacted.







PUBLIC

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PUBLIC

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Janet Aushill	16085 9th St.		
Genevieve West	16395 Hwy 90		
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Phone numbers and email addresses redacted.

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ALVIN ATENHOFFER	14050 Hwy 90		
Jennifer Crowe	16006 1st St Pearlnington		
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Chad McArthur	5004 East Blvd. 11		
Dr. J. B. McArthur	16160 Hwy 90		
Paul Tami	16160 Hwy 90		
G. Hobart Shaw	PO Box 851 Pearlnington 39572		
Thomas A. Dean	PO Box 5113 Pearl Ave Pearlnington		
Pearly Dean	" "		
Lisa Canada	4195 Riverside Dr. Pearlnington		
Susan Taylor	6058 Tatnall Ave. Pearlnington		
Jan Davis	6107 W. L. Hunt Dr. Pearlnington		

Phone numbers and email addresses redacted

# **OPEN HOUSE PUBLIC MEETING HANDOUT**

# **PUBLIC MEETING**

## **US 90 Pearl River Bridges Environmental Assessment St. Tammany Parish, LA and Hancock County, MS**

**STATE PROJECT NO. H.000284 & NO. H.000286  
FEDERAL AID PROJECT NO. H000284 & NO. H000286**

**Open House 4:00 PM to 7:00 PM**

**Thursday, August 22, 2019  
Pearlington Recovery Center  
5265 Hwy. 604  
Pearlington, MS 39572**



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## OPEN HOUSE FORMAT

This Public Meeting for the US 90 Bridges project is being conducted as an Open House. Attendees are welcome at any time during the hours of 4:00 PM and 7:00 PM. As you enter the building you will be asked to sign-in, so that a record of your participation can be maintained.

Throughout the Open House, the following Stations will be available (See Layout Plan which follows this page.)

Station 1 – Sign-in. You will receive a Handout and a copy of the PowerPoint presentation;

Station 2 – PowerPoint Presentation. A continuous, recorded presentation describing the proposed project.

Station 3 - This station will consist of a series of maps that illustrate the potential limits of construction super-imposed over aerial photographs for and several graphics of the typical design sections for the East Pearl River Bridge (Station 3A) and the West Pearl River Bridge and the three Middle Pearl River Bridges (Station 3B). Staff will be available to answer questions.

Station 4 - Real Estate. Additional right-of-way may be required for the undertaking. You may speak to representatives of the LADOTD and MDOT Real Estate Sections if you have questions regarding real estate acquisition and relocation. They will have brochures available regarding right-of-way acquisition and relocation assistance.

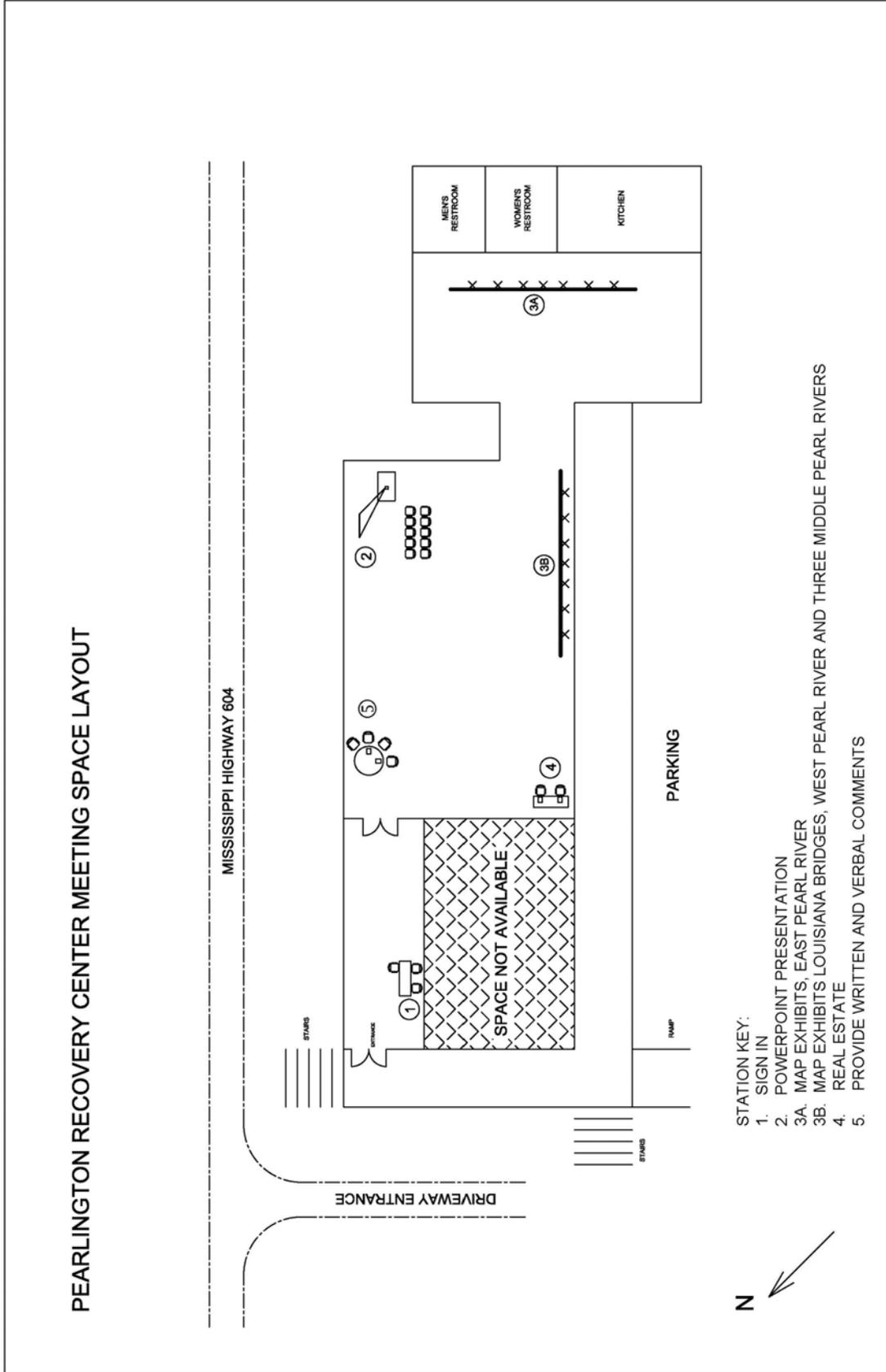
Station 5 - Comment Area - At this station, comments can be made verbally or in writing. A tape recorder is available at this table for verbal comments. The last page of this handout is a comment form that you may use. Comments can be turned in during this meeting or mailed to the address on the back of the form. Additional comment forms are also available to be taken with you. Please note that comments mailed or emailed after this meeting must be postmarked / sent no later than Tuesday September 3, 2019 to be included as part of the meeting transcript.

We hope you will take advantage of this opportunity to provide input on the proposed project. Thank you for attending this meeting and for providing input.

Project staff will be available to assist and discuss project related issues with you.

The next page is a plan of the Meeting site with each of these areas shown. To get the most from your visit to the Meeting, it is recommended that you visit all the areas.

# Meeting Site Diagram



## GENERAL PROJECT INFORMATION

The Louisiana Department of Transportation and Development (LADOTD) and the Mississippi Department of Transportation (MDOT) in cooperation with the Federal Highway Administration (FHWA) proposes to improve five bridge structures associated with the Pearl River system along US 90 in St. Tammany Parish, LA and Hancock County, MS. These include the West Pearl River, the West Middle Pearl River, the Middle Middle Pearl River, the East Middle Pearl River, and the East Pearl River. The limits of the study area extend along US 90 from US 190 in Louisiana to the intersection with Kelly Road/7<sup>th</sup> Avenue in Pearlinton, MS. **Figure 1** shows the limits of the study area. The project includes the planning, engineering, and environmental studies supporting improvements to the five US 90 bridges crossing the Pearl River waterways.

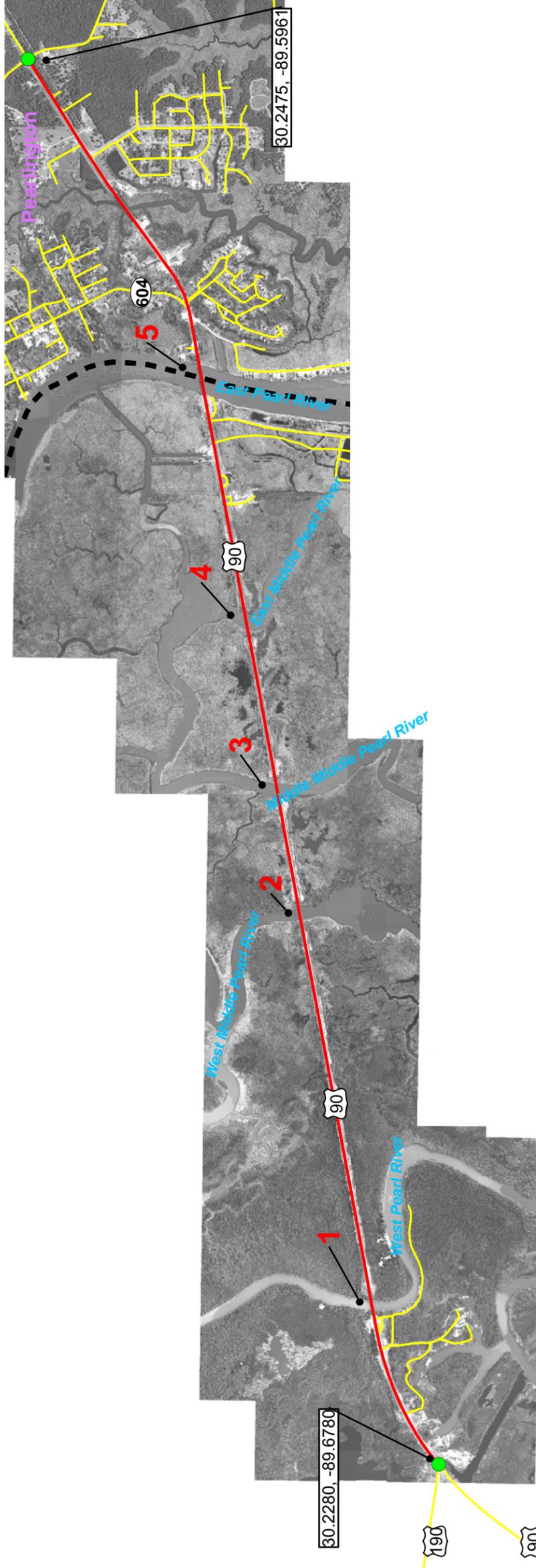
The following chart outlines the steps anticipated to be carried out under the National Environmental Policy Act or NEPA process for this US 90 Bridges project.

### NEPA PROCESS

- Preliminary** 1. Purpose and Need of Project (Ongoing)
  - ONGOING** 2. Develop Reasonable and Feasible Alternatives to Address Need
  - ONGOING** 3. Evaluate Effects of the Alternatives to the Community and Environment
  - NEXT** 4. Summarize Evaluation in an Environmental Assessment
  - NEXT** 5. Hold Public Hearing on the Environmental Assessment
  - NEXT** 6. Prepare a Final Environmental Assessment with request for Finding of No Significant Impact
- 

Map No.	Structure Number	Structure Type	Water Body Crossing
1	62520060705291	Movable: Lift-Span	West Pearl River
2	62520060706701	Pony Truss - Warren Truss	West Middle Pearl River
3	62520060707111	Pony Truss - Warren Truss	Middle Middle Pearl River
4	62520060707701	Pony Truss - Warren Truss	East Middle Pearl River
5	62520060800001	Movable: Swing - Through Truss (Parker Truss)	East Pearl River

NOTE: Logical Termini are the intersections of US 90 with US 190 and Kelly Rd/7th Ave and latitude and longitude data are in decimal format



# Figure 1 Study Area

## Legend

- Logical Termini
- Project Limits
- Roadway Network
- LA/MS State Boundary Line

XX.XXX, XX.XXX Latitude, Longitude



Source (Citation) for 2010 six inch pixel imagery Parish Mosaics  
 This imagery was provided by the Louisiana Governor's Office of Homeland Security and Emergency Preparedness (GOHSEP) as the sole source owner, and is shared with Louisiana state government agencies in the interest of good government practices.  
 Reproduction and distribution of the data is prohibited. Please refer any requests for data to the Deputy Director for Management, Finance and Interoperability of GOHSEP. This imagery was distributed by the Regional Planning Commission for Jefferson, Orleans, Plaquemine, St. Bernard and many Parishes (RPC) with permission from GOHSEP.  
 GOHSEP and the RPC are not responsible for any errors arising from any use of alterations made to the data. Under no circumstances is resale or distribution of the data permitted.  
 Mosaics were produced and compressed by the Louisiana State University Coastal Studies Institute.  
 Imagery Information: The red, green, blue (true color) and near-infrared four-band aerial imagery was captured between February 10th and April 1st of 2010 (re-flights for any corrections flown until April 28th, 2010) by Sanborn Map Company, Inc.  
 The imagery is projected to UTM 15 NAD 83; unit of measure is meters.  
 The spatial resolution is approximately a six inch pixel.  
 Any use of the data for purposes other than those intended by the originator and accompanying seals and logos embedded within.



H.000284 US 90: PEARL RIVER BRIDGES AND  
 H.000286 EAST PEARL RIVER BRIDGE  
 ROUTE US 90  
 ST. TAMMANY PARISH, LA

Prepared For:



Prepared By: Neel-Schaffer, Inc.

## **Purpose and Need:**

The Purpose and Need of the project is preliminary. The purposes of the proposed action identified to date, are:

- To maintain a system link to provide a diversion route in the event of an incident on I-10
- To improve the structural integrity and functionality of the bridges.
- To support hurricane evacuations

## **Planning Considerations:**

### *Section 106 of the National Historic Preservation Act of 1966 (NHPA)*

Section 106 of the National Historic Preservation Act of 1966 requires Federal agencies to take into account the effects of their undertakings on historic properties. Historic properties are properties that are included in the National Register of Historic Places or that meet the criteria for the National Register. All five bridges are eligible for the National Register of Historic Places and subject to the Section 106 process. Should other historic properties be identified, those properties would also be subject to the Section 106 process.

In addition, this project will follow the measures set forth in the 2015 Programmatic Agreement (PA) among FHWA, LADOTD, ACHP, and the LA SHPO Regarding Management of Historic Bridges.

As part of the Section 106 process, the Federal Highway Administration, along with LADOTD and MDOT work with consulting parties. Consulting parties for this project would include the Louisiana and Mississippi State Historic Preservation Officers, Federally-recognized Indian Tribes, local governments, as well as other individuals and organizations with an interest in the project. If you are interested in requesting to be a consulting party in the Section 106 process for this project, please see a staff member at the sign-in table.

### *United States Coast Guard (USCG) Permits*

All five bridges are located on navigable waterways and USCG permits are required demonstrating that the new bridges provide continued access to navigation. USCG permits consider the bridges horizontal channel clearance (the channel width) as well as the bridges “low chord elevation” which is the lowest point of the bridge structure over the navigable waterway.

### *LADOTD Floodplain Guidance*

LADOTD requires that the low chord elevation of bridges remain 1-foot above the flood elevation. On this project the Department evaluated two criteria before establishing the minimum low chord flood elevations.

- The St. Tammany Parish DFIRM elevation plus 1-foot

- An LADOTD determination of the projected hurricane surge, plus the projected wave height plus 1-foot

### *Consideration of Alternatives Developed to Date*

Drawings of the alternative concepts developed to date can be viewed at the Meeting Exhibits Stations. Meeting documents also can be viewed at the following LADOTD web page: [http://wwwapps.dotd.la.gov/administration/public\\_info/projects/home.aspx?key=99](http://wwwapps.dotd.la.gov/administration/public_info/projects/home.aspx?key=99)

### **East Pearl River Alternatives:**

#### *Existing Swing Span Structure*



The existing East Pearl Span is a swing span structure. The navigation channel is 90-foot and there is no vertical restriction. The USCG has requested that the navigation channel clearances of the replacement structure match the navigation clearances of the I-10 span crossing of the East Pearl River to the north. The horizontal clearance would be increased to 115' and the vertical clearance would be 73' above High Water. To date, both fixed and movable span replacement alternatives have been considered.

#### *Traffic Study*

Subsequent to the 2016 Public Meetings, MDOT and LADOTD collected traffic data within Pearlington, and analyzed the data utilizing appropriate engineering practice. A primary finding of the traffic analysis is that the bridge in Pearlington needs to be widened to include an eastbound turn lane.

### *No Build Alternative*

Under the No Build Alternative, no rehabilitation of the existing bridge or new construction would occur. The bridge would continue to operate in its current capacity.

### *Long Span Replacement Proposed Alternative*

This proposed alternative would construct a new long span bridge south of the existing East Pearl River Bridge. West of the river, a new road, Boat Launch Access Road, would be constructed to the north of existing US 90 which would tie back to the existing US 90 centerline. This road would provide continued access to the existing bridge during construction of the new bridge. Access to Honey Island Marina road and the boat launch would remain unchanged. The boat launch would be impacted by the placement of bridge support piers.

For the Long Span Bridge Alternative in Pearlinton, two concepts for realigning MS 604 to US 90 have been developed: the Loop Road Option and the Gooseneck Frontage Road Option.

**Figure 2** shows the Loop Road option between US 90 east of the new bridge and MS 604. This concept was presented at the September 2016 Public Meeting. The MS 604 / US 90 intersection would be relocated east of the existing intersection and remain as a stop control intersection. A left turn lane is provided from eastbound US 90 to northbound MS 604. Connectivity will be provided to Tahiti Road with a newly constructed frontage road located north of the new bridge, extending west from existing MS 604. The existing Riverside Drive would be routed under the new bridge to tie in with the new frontage road.

The loop road would support continued east–west access between MS 604 and US 90. During construction, access can be provided from US 90 to the existing bridge with the use of the loop road or a diversion. Access would be provided to existing residences and commercial properties as shown in the meeting drawings. Some properties may be impacted by the loss of direct access to existing US 90.

**Figure 3** shows the Gooseneck Frontage Road option between US 90 east of the new bridge and MS 604. This concept was developed subsequent to the September 2016 Public Meeting. MS 604 currently terminates at US 90. In this option, MS 604 will turn east through the new roundabout and continue parallel to the new bridge to connect with US 90.

A new frontage road would extend west from the roundabout to provide access to developed properties. Existing Riverside Drive would be routed under the new bridge to tie in with the new frontage road. Tahiti Drive would connect directly with the roundabout.

The new Gooseneck Frontage Road would support continued east–west access between MS 604 and US 90. During construction, access can be provided from US 90 to the existing bridge with the use of the Gooseneck Frontage Road or a diversion. Access would be provided to existing residences and commercial properties as shown in the meeting drawings. Businesses located along the north side of existing US 90 will be impacted by construction of the new frontage road. Other properties may be impacted by the loss of direct access to US 90.

### *Movable Span Replacement Proposed Alternatives*

This proposed alternative would construct a lift span or bascule span south of the existing bridge. To allow for the location of a downstream movable span at 52.5' offset, modifications would have to be made to the existing swing span structure allowing the structure to reverse the direction of its swing. The work to modify the existing span could take up to a month, and there would be temporary disruptions of bridge operations relating to the modifications. During this period when the electrical system is down, the bridge would require manual opening, potentially resulting in navigation delays from a half hour to up to four hours. USCG permits would be required for the work and coordination with the USCG will be accomplished to establish protocols supportive of continuous river navigation.

A review of three years of navigation data showed that approximately 80% of the marine traffic requiring the existing bridge to open would be able to transit the river unimpeded if the clearance of the new bridge in the closed position was 25' or greater above MHW. The mid-level alternatives were developed to provide 25' vertical clearance in support of this concept. Both lift span and bascule span concepts have been evaluated. Exhibits are provided in the meeting showing details of the lift and bascule span alternatives as proposed.

West of the river, new road would be constructed south of and parallel to the alignment of the new mid-level span to provide continued access to Honey Island Marina Road and the boat launch. Access to the existing bridge would remain as is during construction of the new bridge.

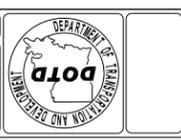
**Figure 4** shows the Movable Span bridge plan view in Pearlington. A left turn lane is provided from eastbound US 90 to northbound MS 604 and right turn lane is provided from westbound US 90 to northbound MS 604. East of the river, Riverside Drive would be routed east to tie into Tahiti Road, and a new intersection would be constructed including US 90, relocated Tahiti Road and MS 604. Access would also be provided as shown to existing commercial and residential properties.

### *Rehabilitation Alternatives*

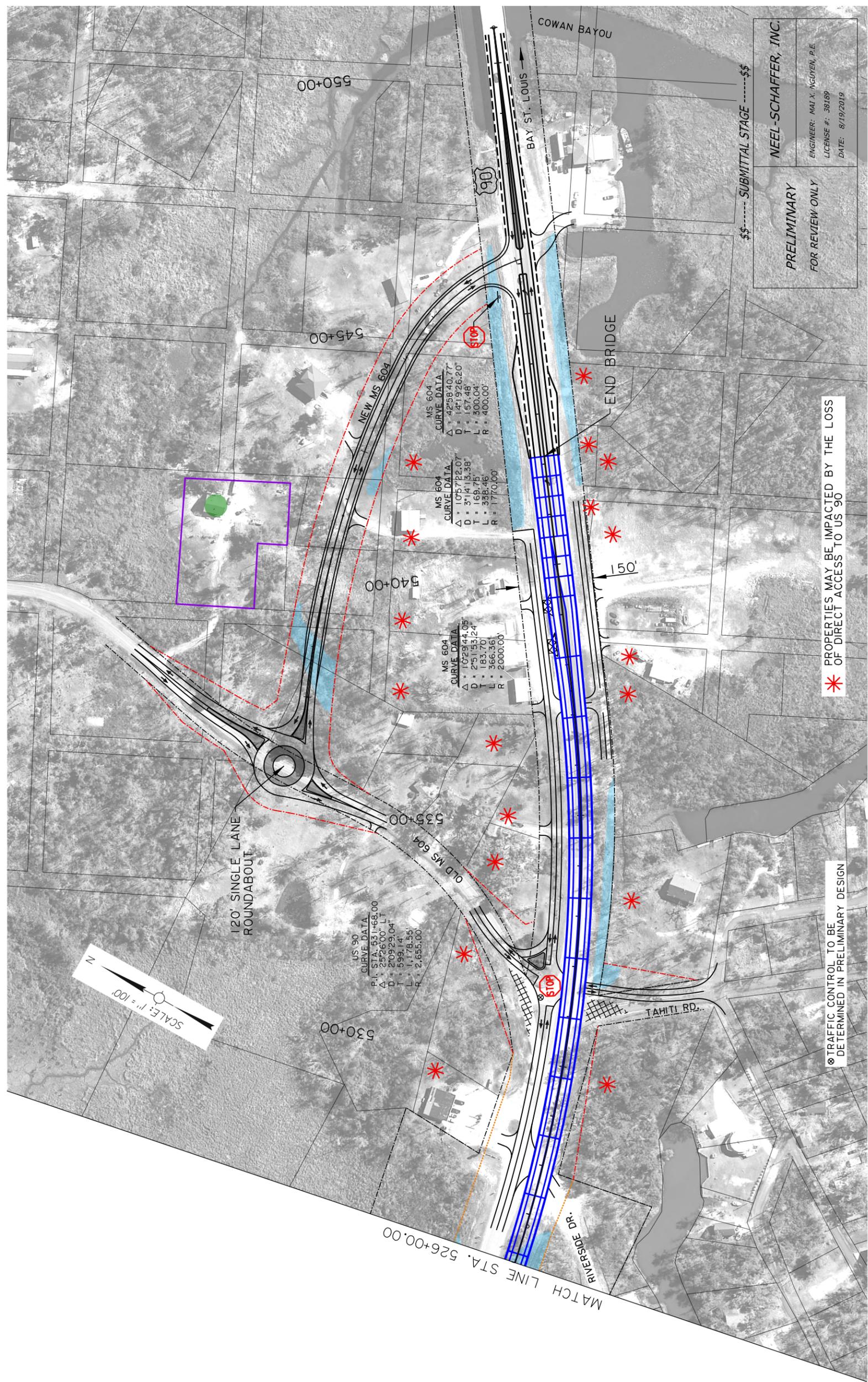
Rehabilitation alternatives to be evaluated for the existing East Pearl River structure include Rehabilitation, Rehabilitation with construction of a new parallel bridge together forming a 1-way couplet pair and Adaptive Reuse.



EAST PEARL RIVER BRIDGE  
FIXED BRIDGE ALTERNATIVE  
PLAN VIEW



## FIGURE 2



US 90: EAST PEARL RIVER BRIDGE  
LONG SPAN BRIDGE ALTERNATIVE LOOP OPTION ROAD  
STATE PROJECT NO. H.000286  
ST. TAMMANY PARISH, LA / HANCOCK COUNTY, MS

PREPARED BY NEEL-SCHAFFER, INC.

LEGEND

- HISTORIC STRUCTURE
- NATIONAL REGISTER PROPERTY LIMITS
- PROPOSED BRIDGE
- REQ'D R/W
- WETLANDS (EXCLUDED WATER OF THE U.S.)
- REMOVAL
- LIMITS OF CONSTRUCTION
- EXIST. R/W

TRAFFIC CONTROL TO BE DETERMINED IN PRELIMINARY DESIGN

PROPERTIES MAY BE IMPACTED BY THE LOSS OF DIRECT ACCESS TO US 90

PRELIMINARY FOR REVIEW ONLY

NEEL-SCHAFFER, INC.  
ENGINEER: MAI X. NGUYEN, P.E.  
LICENSE #: 38189  
DATE: 8/19/2019

US 90 EA  
8/19/2019 10:20:59 AM  
EM Lay\_EP Fix 3 Loop Option.dgn



PREPARED BY NEEL-SCHAFFER, INC.

US 90: EAST PEARL RIVER BRIDGE  
 LONG SPAN BRIDGE ALTERNATIVE GOOSENECK OPTION FRONTAGE ROAD  
 STATE PROJECT NO. H.000286  
 ST. TAMMANY PARISH, LA / HANCOCK COUNTY, MS

- LEGEND**
- HISTORIC STRUCTURE
  - NATIONAL REGISTER PROPERTY LIMITS
  - PROPOSED BRIDGE
  - REQ'D R/W
  - BUSINESS IMPACTED
  - WETLANDS (EXCLUDED WATER OF THE U.S.)
  - REMOVAL
  - LIMITS OF CONSTRUCTION
  - EXIST. R/W

NEEL-SCHAFFER, INC.  
 ENGINEER: MAI X. NGUYEN, P.E.  
 LICENSE #: 38189  
 DATE: 8/19/2019

PRELIMINARY  
 FOR REVIEW ONLY

PROPERTIES MAY BE IMPACTED BY THE LOSS  
 OF DIRECT ACCESS TO US 90

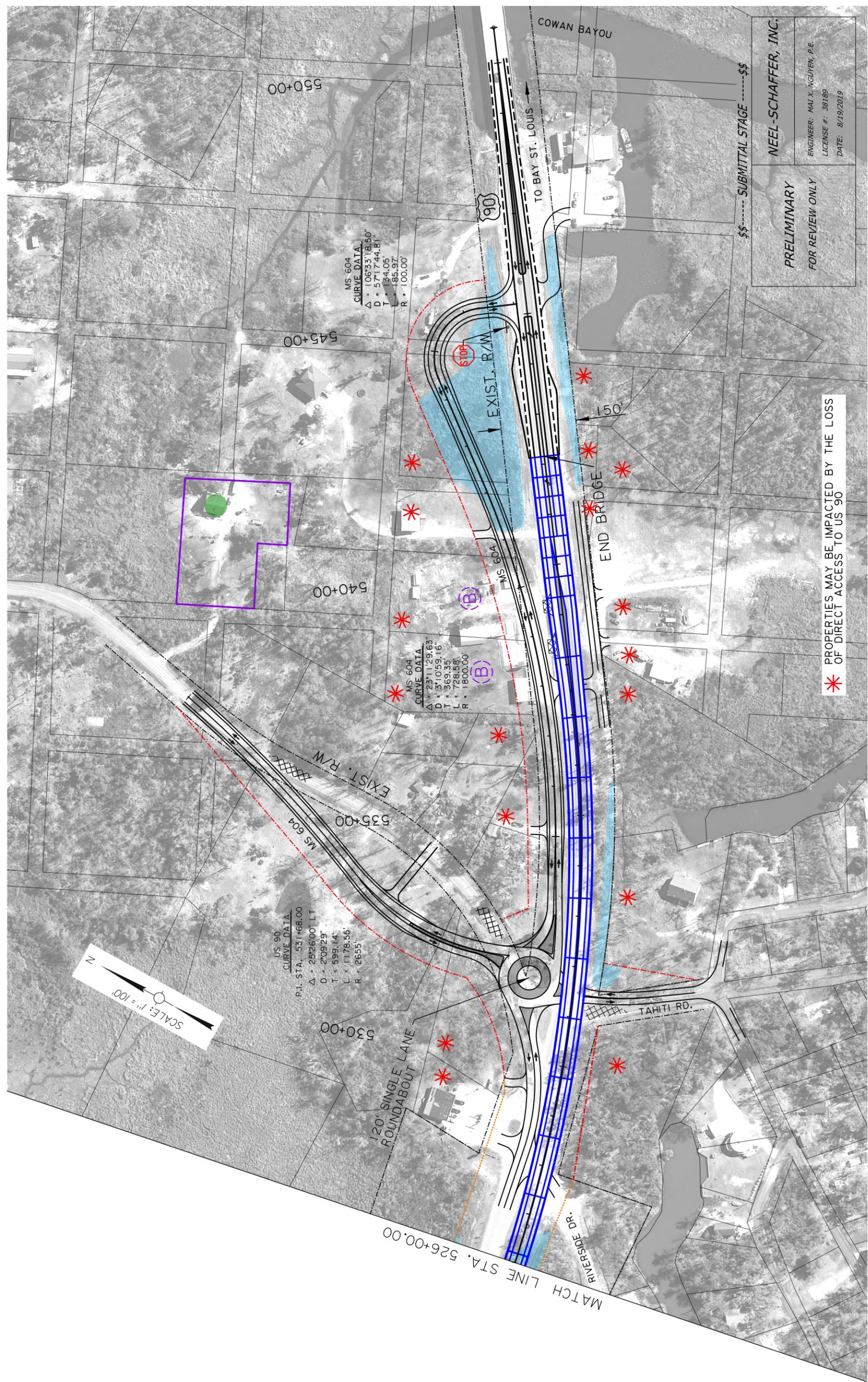
\$\$\$ SUBMITTAL STAGE

TO BAY ST. LOUIS

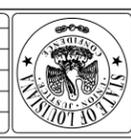
COWAN BAYOU

TAHITI RD.  
 RIVERSIDE DR.

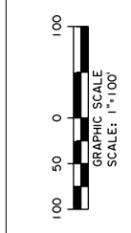
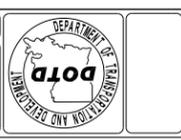
**FIGURE 3**



DESIGNED	CHECKED	CONTROL	SECTION	PARISH	ST. TAMMANY
BETTERED	CHECKED	CONTROL	SECTION	STATE	H.000286
DATE	3 OF 3	DATE	03-09-17	PROJECT	H.000286
NO.		REVISION DESCRIPTION			
DATE					
BY					



EAST PEARL RIVER BRIDGE  
 FIXED BRIDGE ALTERNATIVE  
 PLAN VIEW





PREPARED BY NEEL-SCHAFFER, INC.

US 90: EAST PEARL RIVER BRIDGE  
MOVABLE BRIDGE ALTERNATIVES  
STATE PROJECT NO. H.000286  
ST. TAMMANY PARISH, LA / HANCOCK COUNTY, MS

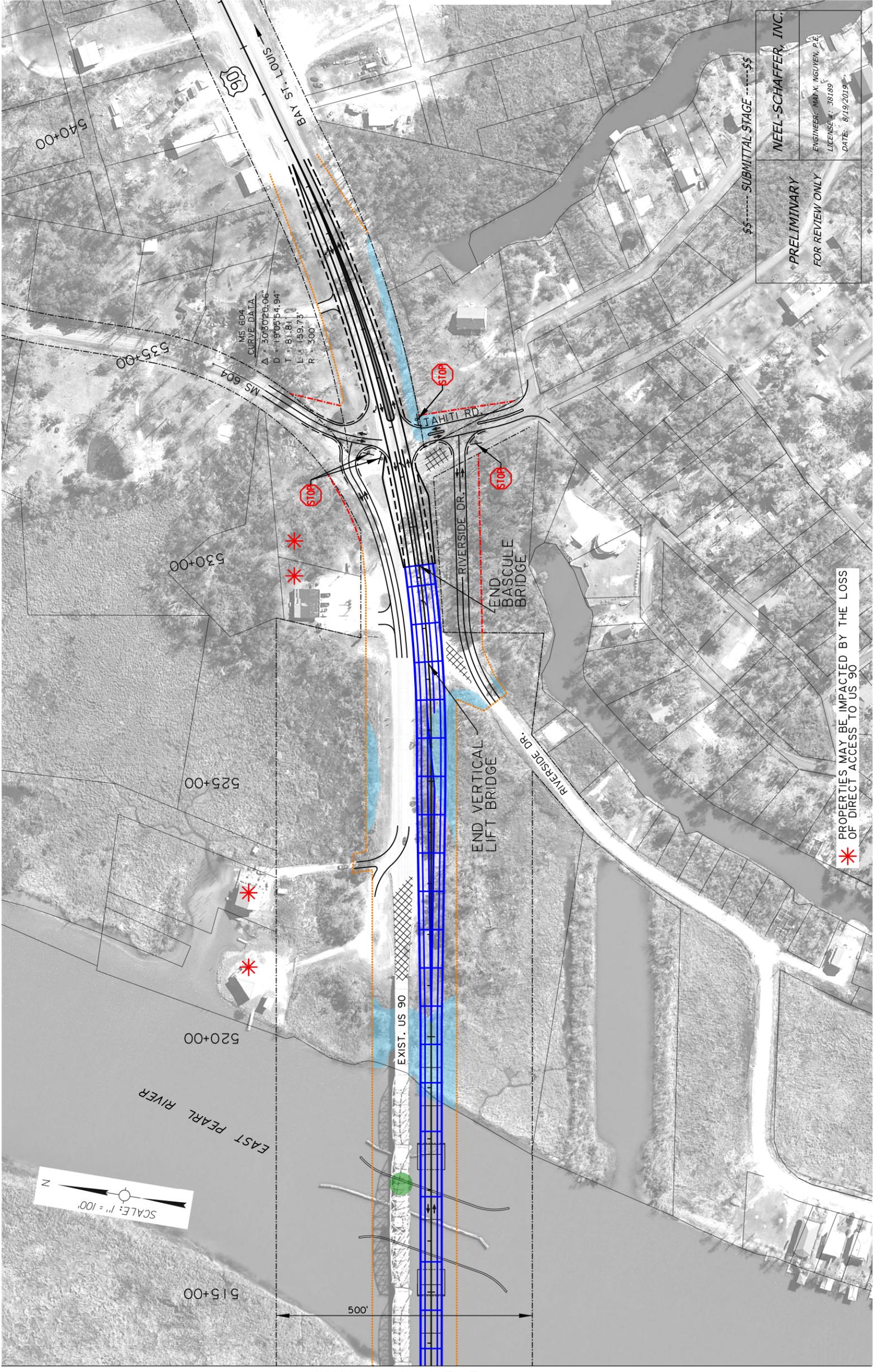
LEGEND  
● HISTORIC STRUCTURE  
--- REQ'D R/W  
--- LIMITS OF CONSTRUCTION

PROPOSED BRIDGE  
WETLANDS (EXCLUDED WATER OF THE U.S.)  
REMOVAL  
--- EXIST. R/W

\* PROPERTIES MAY BE IMPACTED BY THE LOSS OF DIRECT ACCESS TO US 90

NEEL-SCHAFFER, INC.  
ENGINEER: MAX X. NGUYEN, P.E.  
LICENSE #: 38189  
DATE: 8/19/2019  
PRELIMINARY  
FOR REVIEW ONLY  
\$\$\$ SUBMITTAL STAGE \$\$\$

FIGURE 4



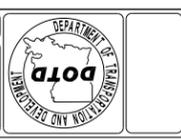
MATCH LINE STA. 514+00.00

SCALE: 1"=100'  
N

SCALE: 1"=100'  
GRAPHIC SCALE  
0 50 100

SHEET NUMBER	DESIGNED	ST. TAMMANY	PARISH	CONTROL SECTION	STATE PROJECT	H.000286
	CHECKED					
	REVISION DESCRIPTION	DATE	NO.	BY		
		03-09-17	2 OF 2			

EAST PEARL RIVER BRIDGE  
MOVABLE BRIDGE ALTERNATIVES  
PLAN VIEW



## West Pearl River Alternatives:

### *Existing Lift Span Structure*



The existing West Pearl River lift span provides horizontal navigation clearance of 90'. Vertical clearance of the existing lift span is 50' with the bridge in the open position. In the closed position, the existing span is approximately 10' above high water. The alternatives developed to date conform to the existing navigation clearances. However, the vertical clearance of the upstream I-10 West Pearl River span is 35' above high water. LADOTD may pursue adjusting the vertical clearance of the US 90 span from 50' to 35' at a later date. This decision would include future consultations with the USCG, and upstream property owners would be notified as part of the USCG consultations.

### *No Build Alternative*

Under the No Build Alternative, no rehabilitation of the existing bridge or new construction would occur. The bridge would continue to operate in its current capacity.

### *Fixed Span Proposed Alternative*

The new fixed span structure would be located south of the existing bridge and provide 50' vertical clearance above High Water. The existing 90' wide channel would be retained. West of the river, a new road would be constructed to the north of existing US 90 which ties back to the existing US 90 centerline. This road would provide continued access to the existing bridge during construction of the new fixed span bridge. Access to Camp Road would remain essentially as it is in the existing condition.

If the future span is constructed at a 35' clearance, it is anticipated that impacts to resources such as wetlands would be reduced by lowering the fixed span elevation as the runout approach to the bridge would be shortened.

### *Movable Span Proposed Alternatives*

The horizontal navigation channel would remain at 90'. The vertical clearance with the movable span in the open position would be at least 50' above High Water. The clearance of the bridge in the closed position would be at the 100-year flood plus 1-foot elevation, which is 19'. This would place the bridge low chord clearance approximately 7.5' higher than the elevation of the existing lift span in the closed position. Both lift span and bascule span concepts have been evaluated. Exhibits are provided in the meeting showing details of the lift and bascule span alternatives as proposed.

A new intersection would be created located slightly west of the existing US 90 intersection with Camp Road. The new intersection would connect Camp Road and a new road constructed to the north of existing US 90 which ties back to the existing US 90 centerline. This new road would provide continued access to the existing bridge during construction of the new movable span bridge.

There would be a reduction in construction costs if the future movable span is constructed at a 35' vertical clearance, but no changes in environmental impacts are anticipated as the approaches to the new 35' vertical clearance movable span bridge would have the same geometric characteristics as the approaches to the 50' vertical clearance movable span.

### **East Middle Pearl River Alternatives:**

The existing East Middle Pearl River bridge is a pony truss fixed span structure which provides horizontal navigation clearance of 60'. Vertical clearance of the existing fixed span is approximately 10' above high water. Replacement bridge alternatives would provide the existing horizontal clearance, and the low chord elevation of the bridge would be governed by the LADOTD storm surge flood criteria. The low chord elevation of the East Middle Pearl River alternatives would be approximately 3.3' higher than the existing bridge low chord elevation.

### *No Build Alternative*

Under the No Build Alternative, no rehabilitation of the existing bridge or new construction would occur. The bridge would continue to operate in its current capacity.

### *Fixed Span Proposed Alternative on Offset Alignment*

A replacement alternative locating the new bridge downstream from the existing span. The existing bridge would remain operational during the construction of the replacement bridge.

### *Fixed Span Proposed Alternative on Existing Alignment*

A replacement alternative located on the existing alignment. In this alternative a detour bridge would be constructed so that the entire US 90 corridor remains open during the construction of the replacement bridge on existing alignment.

### **Middle Middle Pearl River Alternatives:**

The existing Middle Middle Pearl River bridge is a pony truss fixed span structure which provides horizontal navigation clearance of 60'. Vertical clearance of the existing fixed span is approximately 10' above high water. Replacement bridge alternatives would provide the existing horizontal clearance, and the low chord elevation of the bridge would be governed by the LADOTD storm surge flood criteria. The low chord elevation of the Middle Middle Pearl River alternatives would be approximately 3.2' higher than the existing bridge low chord elevation. There is a public boat launch located downstream on the west side of the Middle Middle River. The launch serves as the primary water access point of departure to the Pearl River Wildlife Management Area. Continuous access from US 90 to the launch would be maintained during construction the new bridge, and thereafter.

#### *No Build Alternative*

Under the No Build Alternative, no rehabilitation of the existing bridge or new construction would occur. The bridge would continue to operate in its current capacity.

#### *Fixed Span Proposed Alternative on Offset Alignment*

A replacement alternative locating the new bridge upstream from the existing span. The existing bridge would remain operational during the construction of the replacement bridge.

#### *Fixed Span Proposed Alternative on Existing Alignment*

A replacement bridge located on the existing alignment. In this alternative a detour bridge would be constructed so that the entire US 90 corridor remains open during the construction of the replacement bridge on existing alignment.

### **West Middle Pearl River Alternatives:**

The existing West Pearl River bridge is a pony truss fixed span structures which provides horizontal navigation clearance of 60'. Vertical clearance of the existing fixed span is approximately 10' above high water. Replacement bridge alternatives would provide the existing horizontal clearance and the low chord elevation of the bridge would be governed by the LADOTD storm surge flood criteria. The low chord elevation of the West Middle Pearl River alternatives would be approximately 2.5' higher than the existing bridge low chord elevation.

#### *No Build Alternative*

Under the No Build Alternative, no rehabilitation of the existing bridge or new construction would occur. The bridge would continue to operate in its current capacity.

*Fixed Span Proposed Alternative on Offset Alignment*

A replacement alternative locating the new bridge upstream from the existing span. The existing bridge would remain operational during the construction of the replacement bridge.

*Fixed Span Proposed Alternative on Existing Alignment*

A replacement bridge located on the existing alignment. In this proposed alternative a detour bridge would be constructed so that the entire US 90 corridor remains open during the construction of the replacement bridge on existing alignment.



US 90 Bridges  
Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

# **POWERPOINT PRESENTATION**

# Welcome

This 15-minute presentation will be repeated throughout tonight's meeting and will begin shortly.

This presentation is available on the LADOTD website at

[http://wwwapps.dotd.la.gov/administration/public\\_info/projects/home.aspx?key=99](http://wwwapps.dotd.la.gov/administration/public_info/projects/home.aspx?key=99)



Slide 1

## US 90 PEARL RIVER BRIDGES ENVIRONMENTAL ASSESSMENT ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS

STATE PROJECT NO. H.000284 and NO. H.000286  
FEDERAL AID PROJECT NO. H000284; FEDERAL AID PROJECT NO. H000286

Open House Public Meeting  
August 22, 2019  
4:00 PM- 7:00 PM



Welcome to the Open House Public Meeting on the Environmental Assessment, or EA, for the US 90 Pearl River Bridges. The meeting is sponsored by the Louisiana Department of Transportation and Development, the Mississippi Department of Transportation, and the Federal Highway Administration.

This recorded presentation provides an overview of planning undertaken to-date and the work that will be accomplished in preparation of an Environmental Assessment document for this proposed project.

Slide 2

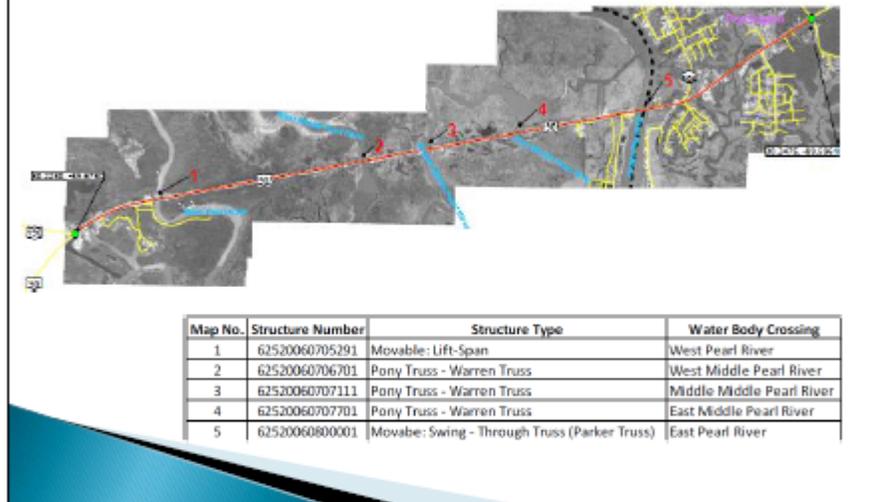
## OBJECTIVES OF PUBLIC MEETING

- Provide information about the proposed project and alternatives currently under consideration
- Solicit comments about the proposed project and alternatives from the public and other interested parties
- Provide the public opportunity to learn about Right-of-Way Acquisition and Relocation Assistance

Read from slide.

Slide 3

## PROJECT DESCRIPTION AND LIMITS



The project limits extend along US 90 from US 190 in Louisiana to the US 90 intersection with Kelly Road / 7<sup>th</sup> Avenue in Pearlington, MS. The project provides improvements to the five US 90 bridges crossing the Pearl River waterways.

Slide 4

## NEPA PROCESS

- Preliminary** 1. Purpose and Need of Project (Ongoing)
- ONGOING** 2. Develop Reasonable and Feasible Alternatives to Address Need
- ONGOING** 3. Evaluate Effects of the Alternatives to the Community and Environment
- NEXT** 4. Summarize Evaluation in an Environmental Assessment
- NEXT** 5. Hold Public Hearing on the Environmental Assessment
- NEXT** 6. Prepare a Final Environmental Assessment with request for Finding of No Significant Impact

An Environmental Assessment, known as an EA, is being prepared for the proposed project.

An EA contains a Purpose and Need for the Proposed Action; an Analysis of Alternatives; a Comparison of Impacts; Public Comments; and Findings.

The Purpose and Need of the project is preliminary. Currently, LADOTD and MDOT are in the process of developing reasonable and feasible alternatives to address the need and evaluating the effects of the alternatives to the community and the environment.

This evaluation will be summarized in an Environmental Assessment. LADOTD and MDOT plan on holding a public hearing in each state to present the EA. After the public comment period, the EA will be finalized and submitted by LADOTD and MDOT to the Federal Highway Administration with a recommendation of the preferred alternative and a request that a "Finding of No Significant Impact," be made.

*Slide 5*

## PRELIMINARY PURPOSE AND NEED

The purposes of the proposed action identified to date, are:

- To maintain a system link to provide a diversion route in the event of an incident on I-10.
- To improve the structural integrity and functionality of the bridges.
- To support hurricane evacuations.

Read from slide.

Please provide us with your thoughts, comments, and concerns.

Slide 6

## Planning Considerations

### Section 106 of the National Historic Preservation Act of 1966 (NHPA)

Section 106 of the National Historic Preservation Act of 1966 requires Federal agencies to take into account the effects of their undertakings on historic properties.

In addition, this project will follow the measures set forth in the 2015 Programmatic Agreement (PA) among FHWA, LADOTD, ACHP, and the LA SHPO Regarding Management of Historic Bridges.

Read from slide.

Slide 7

## Planning Considerations

### Section 106 of the National Historic Preservation Act of 1966 (NHPA)

All five bridges are eligible for the National Register of Historic Places and subject to the Section 106 process. Should other historic properties be identified through the EA process, those properties will also be subject to the Section 106 process.

Read from Slide.

*Slide 8*

## Planning Considerations

### Section 106 Coordination

As part of the Section 106 process, the Federal Highway Administration, along with LADOTD and MDOT, works with consulting parties. Consulting parties for this project would include the Louisiana and Mississippi State Historic Preservation Officers, Federally-recognized Indian Tribes, local governments, as well as other individuals and organizations with an interest in the project. If you are interested in being a consulting party in the Section 106 process for the East Pearl River Bridge, please see a staff member at the sign-in table.

Read from slide.

*Slide 9*

## Planning Considerations

### United States Coast Guard (USCG) Permits

All five bridges are located on navigable waterways and USCG permits are required demonstrating that the new bridges provide continued access to navigation. USCG permits consider the bridges horizontal channel clearance (the channel width) as well as the bridges "low chord elevation" which is the lowest point of the bridge structure over the navigable waterway.



Read from slide.

Slide 10

## Planning Considerations

### LADOTD Floodplain Guidance

LADOTD requires that the low chord elevation of bridges remain 1-foot above the flood elevation. On this project the Department evaluated two criteria before establishing the minimum low chord flood elevations for each bridge.

- The St. Tammany Parish DFIRM elevation plus 1-foot
- An LADOTD determination of the projected hurricane surge, plus the projected wave height plus 1-foot

Read from slide.

Slide 11

## Reason for this Public Meeting

Two public meetings for this project were conducted in September 2016. In response to comments received at those meetings and further analysis, new concepts were developed for the Long Span Bridge Alternative. The purpose of tonight's meeting is to present the latest concepts and receive public comment on the proposed alternatives and concepts.

Read from slide.

Slide 12

## Traffic Study

Subsequent to the 2016 Public Meetings, MDOT and LADOTD collected traffic data within Pearlington and analyzed the data utilizing appropriate engineering practices. A primary finding of the traffic analysis is that the bridge in Pearlington needs to be widened to include an eastbound turn lane.

Slide 13

## East Pearl River Alternatives

### Existing Bridge



The existing East Pearl span is a swing span structure. The navigation channel is 90-foot, and in the open position there is no vertical restriction. In the closed position, the existing span is approximately 10' above high water.

The US Coast Guard requested that the navigation channel clearances of the replacement US 90 structure match the navigation clearances of the I-10 span crossing of the East Pearl River to the north. To accomplish this, the horizontal clearance of new structures are increased from 90' to 115' and the vertical clearance is set at 73' above High Water. Both fixed and movable span alternatives are proposed. The movable span alternatives would be set at 25' above high water with the bridge in the closed position.

Slide 14

## East Pearl River Alternatives

### Alternatives developed to date include:

1. A mid-level, movable, lift span bridge
2. A mid-level, movable, bascule span bridge
3. A long span structure (2 options)
  - Option 1, Loop Road Option
  - Option 2, Gooseneck Frontage Road Option
4. Rehabilitation of the existing bridge

Read from slide.

Slide 15

## East Pearl River Alternatives

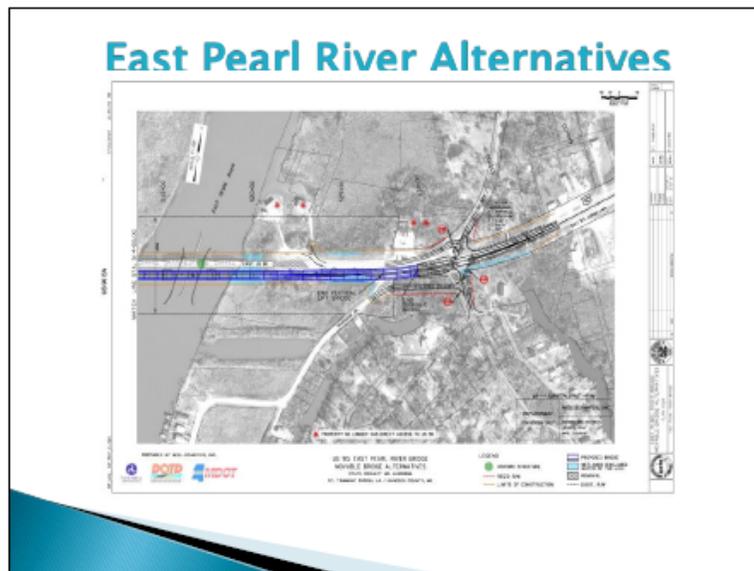
### Movable Span Alternatives Planning to Set the Bridge Elevation

A review of three years of navigation data showed that approximately 80% of the marine traffic requiring that the existing bridge be opened would be able to transit the river unimpeded if the clearance of the new bridge in the closed position was 25' or greater. The proposed mid-level alternatives were developed to provide a 25' vertical clearance in support of this concept.

Read from slide.

Both lift span and bascule span alternatives have been developed. The following slide shows the plan views for the lift span and the bascule span alternatives.

Slide 16



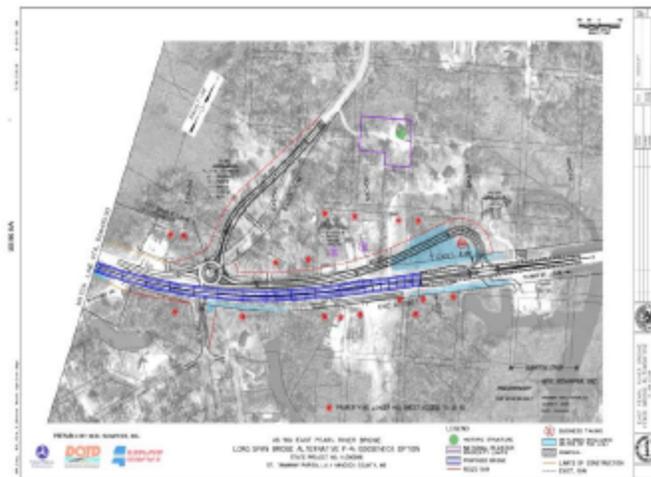
This slide presents the plan view of the mid-level, movable bridge alternatives. Both lift and bascule concepts have been evaluated. These alternatives were presented during the previous public meetings, but the intersections have been modified in response to the findings of the traffic study.

A left turn lane is provided from eastbound US 90 to northbound MS 604 and right turn lane is provided from westbound US 90 to northbound MS 604. East of the river Riverside Drive would be routed east to tie into Tahiti Road, and a new intersection would be constructed including US 90, relocated Tahiti Road and MS 604. Access would also be provided as shown to existing commercial and residential properties.

Slide 17



## East Pearl River Alternatives



This slide presents the plan view of the long span structure with the Gooseneck Frontage Road connection between US 90 east of the new bridge and MS 604. This concept was developed subsequent to the September 2016 Public Meeting. The existing MS 604 / US 90 intersection would be reconstructed as a roundabout and a left turn lane is provided from eastbound US 90 to northbound MS 604.

A frontage road would tie into the roundabout extending west to provide access to developed properties. Existing Riverside Drive would be routed under the new bridge to tie in with the new frontage road.

Tahiti Drive would connect directly with the roundabout. A frontage road would extend east from the roundabout, north of the new bridge, and intersect existing US 90 east of the new bridge.

The new Gooseneck Frontage Road would support continued east-west access between MS 604 and US 90. During construction, access can be provided from US 90 to the existing bridge with the use of the Gooseneck Frontage Road or a diversion. Access would be provided to existing residences and commercial properties as shown in the meeting drawings. There are two business

relocations anticipated. Some properties may be impacted by the loss of direct access to existing US 90.

Slide 20

## East Pearl River Alternatives Modifications to the Existing Bridge

To allow for the location of a downstream movable span, modifications would have to be made to the existing swing span structure allowing the structure to reverse the direction of its swing. USCG permits would be required for the work and coordination with the USCG would be accomplished to establish protocols supportive of continuous river navigation.

Read from slide.

Slide 21

## Louisiana Bridge Alternatives

- ▶ The following slides provide an overview of the alternatives for each bridge location.
- ▶ It should be noted that there was a change to the West Pearl River Bridge exhibits in order to update the assumed ROW based on information received at the 2016 public meeting.

Slide 22

## West Pearl River Alternatives

### Existing Bridge



The existing West Pearl River lift span provides horizontal navigation clearance of 90'. Vertical clearance of the existing lift span is 50' with the bridge in the open position. In the closed position, the existing span is approximately 10' above high water. The existing navigation clearance will be maintained.

Both fixed and movable span alternatives are proposed. The movable span alternatives are set above the FEMA DFIRM elevation. This would place the new bridge low chord approximately 7.5' higher than the elevation of the existing lift span in the closed position.

Slide 23

## West Pearl River Alternatives

### Alternatives developed to date include:

1. A fixed span structure
2. A movable, lift span bridge
3. A movable, bascule span bridge

Read from slide.

Slide 24

## Middle Pearl River Bridges

### Existing Bridges



### East Middle Pearl

All three of the existing middle pearl river bridges are pony truss fixed span structures which provide horizontal navigation clearance of 60'. Vertical clearance of the existing fixed spans is approximately 10' above high water. New bridges will provide the existing horizontal clearance, and the low chord elevation of each bridge will be governed by the LADOTD storm surge flood criteria.

Slide 25

## West Middle, Middle Middle, and East Middle Pearl River Alternatives

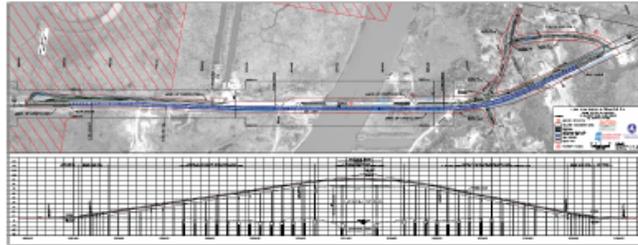
### Alternatives developed to date include:

1. A fixed span structure on offset alignment, upstream of the existing alignment
2. A fixed span structure on existing alignment, with the construction of temporary detour structures

Read from slide.

Slide 26

## Meeting Exhibits (Example)



The meeting exhibits station presents detailed drawings of the proposed alternatives currently under consideration.

The roadway corridor will be continuously open to traffic as new bridges are constructed.

Access to abutting properties is shown on the exhibits and staff are available to answer your questions.

*Slide 27*

The information presented today represents a conceptual plan. The alternatives as presented may be modified and other alternatives may be developed for consideration.

Your comments tonight will help us carry the project forward through the Environmental Assessment.

Please note that the information presented today represents a conceptual plan. The alternatives as presented may be modified and other alternatives may be developed for consideration.

Your comments tonight will help us carry the project forward through the Environmental Assessment.

*Slide 28*

## COMPLETING THE ENVIRONMENTAL PROCESS

- ❖ Review comments from this meeting
- ❖ Complete data review and field work
- ❖ Complete Environmental Assessment document
- ❖ Hold Public Hearings
- ❖ Submit document to Federal Highway Administration and request approval of a Finding Of No Significant Impact

As part of the environmental process, LADOTD and MDOT will address any comments from this meeting; complete data review, field work, and the Environmental Assessment document; hold a public hearing in each state; and request approval of a Finding of No Significant Impact from the Federal Highway Administration.

Slide 29

## Right-of-Way Acquisition and Relocation Information

The only requirement for additional right-of-way in Louisiana is associated with the East Pearl River Movable Span alternatives. All of the East Pearl River alternatives in Mississippi require additional right-of-way as shown on the exhibit drawings. Brochures explaining the Acquisition of Right-of-Way and Relocation Assistance Program for both LADOTD and MDOT are available tonight at the relocation table or can be obtained later from:

LADOTD Real Estate Section  
P.O. Box 94245  
Baton Rouge, LA 70804-9245  
(225) 242-4593

MDOT ROW District Coordinator  
Attn: **Ashlyn Brock**  
P.O. Box 551  
Hattiesburg, MS 39403-0551  
(601) 544-6511

If you have questions regarding your individual situation, staff will take your contact information and have a relocation specialist contact you.

Read from slide.

Slide 30

# WHY COMMENT?

**BECAUSE...**

- ❖ Community concerns and preferences are factors that are considered.
- ❖ All comments must be considered in the Environmental Assessment.
- ❖ We need to know if there are any issues or opportunities with the project as proposed.



**PUBLIC MEETING COMMENT CARD**  
 PROJECT: **SHREVEPORT TO LAKE CHARLES TO BOSSIERE**  
 FEDERAL CORRIDOR: **701**  
 FEDERAL ROUTE: **1701**

Please print your name and address on this card and return it to the project manager at the meeting location. Comments received at the meeting will be included in the Environmental Assessment. Comments received at the meeting will be included in the Environmental Assessment. Comments received at the meeting will be included in the Environmental Assessment.

Name: \_\_\_\_\_  
 Title: \_\_\_\_\_  
 Organization: \_\_\_\_\_  
 Address: \_\_\_\_\_  
 City: \_\_\_\_\_  
 State: \_\_\_\_\_  
 Zip: \_\_\_\_\_

E-MAIL: \_\_\_\_\_

PLEASE PRINT YOUR COMMENTS CAREFULLY

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NAME: \_\_\_\_\_ DATE: \_\_\_\_\_

ADDRESS: \_\_\_\_\_

**Comments received tonight or post-marked by September 3, 2019 will be part of the official transcript of this public meeting.**

Thank you for your interest in tonight's meeting. If you wish to make a comment, please see a staff member at the Comment Table. Comment forms are provided as the last page of your meeting handout.

Your comments are important because community concerns and preferences are factors that are considered in the Environmental Assessment being prepared for this project. We need to know if there are any issues or opportunities with the project as proposed.

Comments received tonight or post-marked by **September 3, 2019** will be part of the official transcript of this public meeting.

Slide 31

Thank you for your time.  
 Please visit the remaining stations  
 to view the exhibits and  
 provide your comments.

This is the end of the presentation.

**(One Minute Break)**



Again, thank you for your participation in this Public Meeting. Please visit the remaining stations to view the exhibits and provide your comments.

Slide 32

# **WRITTEN COMMENTS**

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS  
STATE PROJECT NO. H.000284 & NO. H.000286  
FEDERAL AID PROJECT NO. H000284 & NO. H000286

IN ORDER TO INSURE THAT YOUR COMMENTS BECOME PART OF THE OFFICIAL TRANSCRIPT OF THIS MEETING, THEY SHOULD BE SENT TO ONE OF THE FOLLOWING ADDRESSES, POSTMARKED, OR RECEIVED ELECTRONICALLY, NO LATER THAN September 3, 2019.

Mail to: US 90 Bridges  
Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com).

PLEASE CONSIDER THE FOLLOWING COMMENTS:

*Pearlington is an unincorporated town with 1 Quik Mart, a restaurant, a sno-ball stand and an ATM machine. Whether it's work or basic necessities like food, residents must go elsewhere. This Hwy 90 is the only quick direct route to anywhere - Walmart, pharmacy employment. If the bldg of the bridges disrupts the traffic flow forcing residents back to I-10 they have will suffer a huge hardship. There are no alternatives for them.*

NAME: Rev. Maurice Fields DATE: 8/22/2019  
ADDRESS: Pearlington UMC

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
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Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com).

PLEASE CONSIDER THE FOLLOWING COMMENTS:

We like Option 1: Loop Road  
We think it would bring traffic and possibly  
business opportunities to Pearlington - but not  
into the center of Pearlington. This option allows  
Pearlington to keep the small town feel.

NAME: Robin & Lucy Deltor DATE: 8/22/19

ADDRESS:

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
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Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com).

PLEASE CONSIDER THE FOLLOWING COMMENTS:

We in Pearlington are asking that you take very careful consideration in regards to taking LA US 90 back to grade level. Although the levees in New Orleans are killing us during hurricane flood surge, HWY 90 (even Louisiana side of East Pearl River) HELPS Pearlington (North of HWY 90) during a hurricane. Thanks in advance for your (re-thinking)!

NAME: Rick Bayet DATE: 22 Aug 2019

ADDRESS:



~~Exis~~ • Traffic from Orleans comming  
from New Orleans East will be  
in the way of the Pearlinton people  
getting out from storm on Hwy 90  
We have problems with Slidell  
Peoples comming over to Bay St  
Louis to Shoppe Traffic something  
Bumher to Bumher Can you'll look at  
the Traffic from Slidell to Bay St  
Louis concern Citizer Mae Bradler

*emailed*

**Cheryl Trumps**

---

**From:** Alvin Attenhofer  
**Sent:** Monday, August 26, 2019 10:45 AM  
**To:** US90 Bridges  
**Subject:** East Pearl River Bridge

If I had my choice of the proposed options for the East Pearl River Bridge, I would like to see the high rise and not a bridge that opens.

Thanks  
Alvin Attenhofer

**Cheryl Trumps**

---

*emailed*

**From:** Calinda Aucoin  
**Sent:** Wednesday, August 28, 2019 4:26 PM  
**To:** US90 Bridges  
**Subject:** US 90 residents Bridge concerns  
**Attachments:** 20190828\_162019.jpg

Jessica Calinda Crowe Aucoin,

*Please See Comments on next page*

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS  
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TRANSCRIPT OF THIS MEETING, THEY SHOULD BE SENT TO ONE OF THE  
FOLLOWING ADDRESSES, POSTMARKED, OR RECEIVED ELECTRONICALLY, NO  
LATER THAN September 3, 2019.

Mail to: US 90 Bridges  
Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com).

PLEASE CONSIDER THE FOLLOWING COMMENTS:

My main Concernes : what happens if my home starts  
Sinking, settling, moves or cracks walls ect... from pile  
driving pounding ect... will ya'll fix issues from the  
Construction Causes? or buy us out pending Problems?  
\* I have prime property on the river and being under  
a bridge now is gonna bring my property value down,  
because things happen under bridges like Bums, Homeless  
people, Drugs ect...  
Out of all, figure 4 would be best choice for all concerned...  
I believe! And most in town, I have talked with about in, we  
+ I would rather see our Historic Bridge on East Pearl  
River be restored & raised, why wasn't that proposed?

NAME: Jessica Calinda Crowe Aucoin DATE: 8-26-19

## Cheryl Trumps

---

**From:** Joe Spell  
**Sent:** Monday, September 02, 2019 10:14 PM  
**To:** US90 Bridges

I am writing you in regards to the US 90 bridge in Pearlinton, Ms. I am truly upset that we have to remove the original bridge. These bridges are landmarks and should not be removed but merely updated. But if a new bridge has to be done I would rather it be figure 4 option. This is the least detrimental to our small town. It's bad enough this whole bridge is going to be a severe inconvenience to many residents rerouting our daily commute. But option 4 is the least impactful to our homes.

Thanks,  
Christina Spell

Sent from Yahoo Mail for iPhone

## Cheryl Trumps

---

**From:** jcspell  
**Sent:** Monday, September 02, 2019 9:34 PM  
**To:** US90 Bridges  
**Subject:** Pearlinton bridge

I am a resident of Belle Isle subdivision. The option I would most like to see would be a refurbish of the original bridge. There are not many historical bridges left that are this functional. No matter what you build, if you get rid of this bridge you change the whole community of Pearlinton, forever.

If keeping the bridge is not an option then the only acceptable option is in figure 4, as it is going to have the least effect on the community, and be the smallest footprint and eyesore of the 3 proposals.

B'rgds  
Joe Spell

## Cheryl Trumps

---

**From:** hasselvanderllc  
**Sent:** Tuesday, September 03, 2019 10:32 PM  
**To:** US90 Bridges  
**Subject:** Fw: Fwd: Pearlington bridge

[Sent from Yahoo Mail for iPhone](#)

Begin forwarded message:

Date: Sep 2, 2019 9:34 PM  
Subject: Pearlington bridge  
To: [us90bridges@neel-schaffer.com](mailto:us90bridges@neel-schaffer.com)  
Cc:

I am a resident of Belle Isle subdivision. The option I would most like to see would be a refurbish of the original bridge. There are not many historical bridges left that are this functional. No matter what you build, if you get rid of this bridge you change the whole community of Pearlington, forever.

If keeping the bridge is not an option then the only acceptable option is in figure 4, as it is going to have the least effect on the community, and be the smallest footprint and eyesore of the 3 proposals.

B'rgds

Brian Hasselvander

**From:** [virle\\_reid](mailto:virle_reid)  
**To:** [Neel-Schaffer Inc.](mailto:Neel-Schaffer.Inc)  
**Cc:** [dbaria@house.ms.gov](mailto:dbaria@house.ms.gov); [Catherine Mastin](mailto:Catherine.Mastin); [Stacie Palmer](mailto:Stacie.Palmer); ["tsanders@mdah.ms.gov"](mailto:tsanders@mdah.ms.gov); [Barry Bruebacher](mailto:Barry.Bruebacher); [Jessica Richardson](mailto:Jessica.Richardson)  
**Subject:** US 90 bridges comments, Virle Sheffield Reid, 5098 Highway 604, Pearlinton, MS 39572  
**Date:** Sunday, September 01, 2019 7:49:36 PM

---

Please consider the following comments:

This is my second round of comments regarding the project: Stacie Palmer has those emails on file.

In my heart, I would hope that the present bridge-eligible for the National Register of Historic Places-be restored, but raised to an elevation which meets with the criteria of LDOT's three year study, regarding the percentage of river traffic which requires a bridge to be opened if the height from mean tide to the underside of the bridge is 25 feet.

If restoration of the bridge is deemed impossible for PHYSICAL reasons, I cast my vote for the bascule bridge. According to the drawings/schematics, and the input of the engineers at the second meeting, it would be the less intrusive of the new bridge options, yet achieve the desired ends. Construction would happen quickly; fewer residents and businesses (in particular, our local store) would be adversely affected by its construction impact (especially airborne debris, noise pollution, and traffic re-routing); and it would meet with Stennis' desire to have a river passage without any height limits on watercraft. And it would also offer a more aesthetically pleasing option to the fixed bridge option. To this end, I had asked, at the meeting, what the cost would be to restore our beautiful bridge, and the answer was "embarrassingly expensive." When I asked to see the figures, I was told they exist, but were not at the meeting, and received the same answer when I inquired regarding figures for the other bridges too. When I asked why the figures were not there-in order for us to have a better grasp of the scope of the project-I was told "they" told us not to bring them. When I asked who "they" were, the answer was: LDOT.

I am alarmed by what feels like a lack of transparency surrounding this project. After questioning those in charge of the project for over three years regarding NASA's/Stennis' role in the construction of the bridge, and receiving either no answer, or the answer that the aforementioned had no preference or impact on the project, we finally received, on Thursday afternoon, a verbal answer from one of the senior engineers of Neel Schaffer: NASA/Stennis, in lieu of the planned missions to the moon in 2022 and 2024, and the missions to Mars in the coming decade, would like to have a fixed bridge in place which would accommodate the size of the newest SLS system AND allow unhindered transport of liquid hydrogen to the test site. To that end, I would like to officially request that an Environmental Impact Study be done to address the potential dangers and impact of Liquid Hydrogen and other fuels to be transported upstream, unhindered, 24/7. With increased traffic/transport comes increased

risk; and though the safety elements built into the new LH bunker transports are considered state-of-the-art, an accident/rupture/spill of this highly combustible fuel, when coming into contact with the atmosphere, can be extremely catastrophic to the surrounding flora and fauna.

Since construction on the East and West Pearl River bridges is imminent, I would also like to request that the powers that be look into the feasibility of creating an elevated stretch of hwy 90 between the two. It would allow freer flow of the Pearl River system in times of flood, reducing physical impact/damage on surrounding structures and communities; allow fauna to travel freely through the corridor, thus reducing their present highway crossing mortality rates; and in the spirit of this project, reduce the chance of that stretch of highway being inundated during times of storm/flooding.

I am grateful that Neel Schaffer, LDOT, and MDOT have come up an alternate re-route of highway 604 for the fixed bridge option. I feel that the revisit by the historical assessment team the Thursday prior to the meeting allowed them to see the extreme proximity of the initial Hwy 604 re-route to the historic structure at 5098 Hwy 604, and the potential dangers/consequences it posed. But this new option still leaves my immediate neighbors in a lurch, and it pains me to see them stressed out by the situation. I have not yet spoken to my neighbor whose property would be required for the alternate re-route, so I have not been able to gauge their feelings. I hope that neither of these options for a re-route become necessary.

In closing, I would like to pull something from an email I had sent to Barry Brupbacher:

"Barry, a thought came to me after I had received your last email: I believe that we differ greatly regarding our definition of negligible, at least when it comes to the re-route project. A few feet, even as an individual who feels comfortable in urban and rural environs, is a lot to me. And a few more vehicles traveling down the road, to me, constitutes a significant amount of noise and visual pollution, especially if both are happening within earshot and line of sight. What might seem like a small increment in distance to the engineering crew translates into a violation of space for many out here in such a rural area. Most here covet their space and privacy, and a loss of even a few feet can mean a huge impact of their lives, and their psyches. Personally, I feel as if the engineering crew on this endeavor must strive for a greater sense of, well, sensitivity, in how this project is approached. You are dealing with a community that essentially became a refuge for people who were displaced by the Stennis Space Project back in the 1960's. And you are dealing with a community that was all but forgotten in the days shortly after Katrina. People here are very sensitive to the feeling of possibly being steamrolled. This project has the potential to rekindle those feelings."

I look forward to serving on the Sec. 106 panel/committee.

Following comments from Virle Reid received between October 17, 2016 and July 29, 2019.

**From:** [virle\\_reid](mailto:virle_reid)  
**To:** [barry.brupbacher@neel-schaffer.com](mailto:barry.brupbacher@neel-schaffer.com)  
**Cc:** [Stacie Palmer](#); [Catherine Mastin](#)  
**Subject:** Re: Hwy 90 Bridge Project update  
**Date:** Monday, July 29, 2019 10:50:02 AM

---

Hi Barry:

Also, Catherine, two questions that perhaps both you and Barry could answer: One, has any consideration been given to elevating Hwy 90 between the West and East Pearl bridges, to restore the traditional water flow between the two rivers, and allow local fauna to travel less dangerously along that corridor?

And two, what is the diameter of the proposed roundabout/rotary option on 604?

Best,

Virle Reid

Best,

Virle Reid

Sent from my iPhone

1/2-

**From:** [virle reid](#)  
**To:** [Barry Brupbacher](#)  
**Cc:**

**Subject:** Re: US 90 Bridges - Hancock County Parcel Layer  
**Date:** Friday, October 28, 2016 9:12:20 PM

---

Barry:

I had a couple of additional comments, if its not too late?

I had forgotten to talk about the turtles...I noticed several large female turtles, not snappers or map turtles, coming onto the property to lay their eggs. I never got close enough to do a thorough ID, but I would say the smallest was 8-10 inches, the largest, 10-12 inches on the shell. Their favorite place to lay their eggs is along the fence line perpendicular and horizontal to the house, that is 2, and part of 3, of Block 23 on the map.

Also-and this might be more directed toward your team: What happens to the value of the property? With the loss of a significant buffer of land; an increase in light, sound, and air pollution; and a loss of private space along the entire south side of the structure, how will that affect what the entire property will now be worth?

And finally, if this re-route does go through, what about the sound and, more importantly, the air quality? It will be loud for a long period of time. Dust will be stirred up, and into the air; a fine layer of what is largely clay will cover the surrounding structures; and inevitably, get into the lungs of everyone who lives within a considerable distance of the construction site. How is that to be addressed? Once again, Barry, please keep in mind we are talking about a rural and not an urban area: I do not believe what is good for the goose is good for the gander.

On a side note, some research done earlier this year suggests that this house was built by one of Simon Favre's slave crew bosses, the slave Usan Vaughn, inventor of the log carrier. The construction methodology in my house is almost identical to the Usan Vaughn House off of Gin Road in Pearlinton (the house he built for his wife in 1859, after having purchased her freedom.) Some have dated my home to the 1880's, some as early as the 1850's. I had some of the framing nails dated as Pre-Civil War, so I'm in the 1850's camp...my house and the house on Gin Road (still standing, and owned by Jerry Bolte) are, I believe, the last two Antebellum homes left in the area. I'm hoping to find more information on that when I am able to spend more time in the Historical Society Archives in Bay St. Louis.

Best,

Virle S. Reid

---

**From:** virle reid

**Sent:** Monday, October 17, 2016 3:09:45 PM

**To:** Barry Brupbacher

Barry:

As I type this email, I am looking at my bedroom window (2nd floor) directly down on, and toward, the proposed MS604 re-route through my property.

Not happy.

With the proposed plan, MS604 would essentially border to sides of my property.

It will also cut my neighbors property in two. They have young children. The proposed route would make play in their yard a bit tenuous; and the notion of crossing MS604 to visit their relatives, in the other structure on their property, a dangerous one without adult supervision (let alone during the actual construction process.) Let alone the fact that their privacy will all but be erased. My backyard would no longer be my backyard, a private space. Both my neighbor and I will be more susceptible to uninvited public scrutiny and/or curiosity. This would create an additional risk to my neighbors children; their business; and the structures on their property.

For me, it could mean more walk-ups to the property by uninvited individuals. More importantly, it could mean people stopping and parking their vehicles along the side of the reroute in an unsafe manner-to look at/take photos of the historic structure-and thusly, create a potential road/traffic hazard.

The noise levels of the present MS604-even with the protection of a sizable swath of flora between my property and the road-can be quite loud, depending on the type of vehicles that pass through. The proposed re-route will pass directly in front of, and behind, the two existing structures on my neighbors property, making sound buffering virtually impossible, thus forcing them to endure traffic noise levels-especially something like large cc motorcycles, which do come through Pearlington on a frequent basis-that I believe even someone from the city would even consider excessive and stress inducing. Even with an aggressive strategy to place flora between my home and the proposed re-route, the noise levels for me would remain very loud for a long period of time. But there is no guarantee that that would even work. I have a second story, and what is still a wonderful view of the surrounding property/land.

The rotaries/roundabouts are a novel idea, and I realize their necessity if you are trying to avoid additional costs and maintenance of stoplights/flashing lights, but they will bear the responsibility, for a time, of being responsible for accidents involving people who are unaccustomed to their use, and/or just poor drivers

When word gets out to the commercial world that a bigger and better bridge system is in place, one that 18-wheelers no longer have to negotiate with the concern of height clearance or tonnage limits, the commercial traffic levels will increase dramatically, and with it will come a tremendous increase in noise pollution and vibration in our surrounding area. And when people in general discover that our fixed bridge affords a more beautiful and dramatic view, upstream and downstream, of the East Pearl River, they will come too, bringing more traffic volume and noise pollution as well.

What will happen to our local convenience store? Will it die a slow death because it will essentially be cut off from the main flow of traffic? Looking at the proposed re-route, it will essentially sit in a "backwater" this project will create. This might be inevitable, regardless of what option is chosen, but it is sad.

Have any or all of the property owners south of our properties, but north of the present Highway 90, been approached regarding their outlooks on the futures of their properties? I know that at least one owner on the corridor wants to sell. What about the others? Or has this not been looked into, with the hope that the present re-route could stay in effect, and the costs of securing the lands for the present re-route plans would be much less expensive than having to spend additional monies to acquire and demolish any structures that could be in the re-route's path?

My historical property is described, in the NRHP description, as sitting on 3.15 acres. And again as approximately 3 acres. It also gives a description of the property immediately surrounding the structure, which does not include all of the 3.15 acres. I will check with my lawyer, but I believe that the inclusion of the 3.15 phrase in the description defines the property. I also need a better understanding from you as to how the project team views FHWA section 4f and its impact on this historic property...? And when will there be a Section 106 review regarding this historical property, and will I be included?

And all of the adverse physical effects of the construction process, especially airborne debris and noise pollution, pretty much go unsaid.

Barry, a thought came to me after I had received your last email: I believe that we differ greatly regarding our definition of negligible, at least when it comes to the re-route project. A few feet, even as an individual who feels comfortable in urban and rural environs, is a lot to me. And a few more vehicles traveling down the road, to me, constitutes a significant amount of noise and visual pollution, especially if both are happening within earshot and line of sight. What might seem like a small increment in distance to the engineering crew translates into a violation of space for many out here in such a rural area. Most here covet their space and privacy, and a loss of even a few

feet can mean a huge impact of their lives, and their psyches. Personally, I feel as if the engineering crew on this endeavor must strive for a greater sense of, well, sensitivity, in how this project is approached. You are dealing with a community that essentially became a refuge for people who were displaced by the Stennis Space Project back in the 1960's. And you are dealing with a community that was all but forgotten in the days shortly after Katrina. People here are very sensitive to the feeling of possibly being steamrolled. This project has the potential to rekindle those feelings

Best,

Virle S. Reid

**BELLE ISLE PROPERTY OWNERS ASSOCIATION, INC.**

**POST OFFICE BOX 32  
PEARLINGTON, MISSISSIPPI 39572**

**DIRECTORS:** Glenn Snyder - President  
Jeffrey Johnson - Vice President  
Linda Solis - Secretary/Treasurer  
Sharon Graham  
Robert L. Jordan

**September 23, 2019**

**US 90 Bridges  
Attn. Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, Louisiana 70505  
[US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com)**

**Subject: US 90 Pearl River Bridges  
Environmental Assessment  
St. Tammy Parish, LA & Hancock County, MS  
State Projects H.000284 & H.000286  
Federal Aid Projects H000284 & H000286**

**Gentlemen:**

**Following is our assessment of bridge construction least impact:**

**1, East and West Pearl River Bridges: We suggest "modification" of existing bridges.**

- a) Less impact on right-of-way acquisitions.**
- b) Less damage to property values.**
- c) Less damage to property accesses.**
- d) Less traffic interruption.**
- e) Less costly.**
- f) Better cost/benefit ratio.**

**2, All Middle Pearl River Bridges: We suggest "fixed-span structures on offset alignments**

- a) Less impact on boat and vehicular traffic.**
- b) Likely most cost affective.**
- c) Better cost benefit ratio.**

**Respectfully,**

**Belle Isle Property Owners Association, Inc.**

**Glenn R. Snyder  
P.E. La #9190 & P.E. Ms #9755  
President**

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS  
STATE PROJECT NO. H.000284 & NO. H.000286  
FEDERAL AID PROJECT NO. H000284 & NO. H000286

IN ORDER TO INSURE THAT YOUR COMMENTS BECOME PART OF THE OFFICIAL TRANSCRIPT OF THIS MEETING, THEY SHOULD BE SENT TO ONE OF THE FOLLOWING ADDRESSES, POSTMARKED, OR RECEIVED ELECTRONICALLY, NO LATER THAN September 3, 2019.

Mail to: US 90 Bridges  
Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com).

PLEASE CONSIDER THE FOLLOWING COMMENTS:

The public meeting was full of great information. My thoughts about the project. I am of the opinion that no bridge with any working parts to lift or lower bridge should be built. These parts will break and the people of Pearlington are impacted the most when bridges break. A bridge high enough to allow all boat traffic 24 hours a day access. I live on the East Pearl and I see the traffic on the River. I feel all government facilities up river from Pearlington should have 24 hour access. Also, an elevated roadway would be nice between the West Pearl & East Pearl Rivers.

NAME: Debra E Guthans DATE: 8-27-2019

ADDRESS:

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS  
STATE PROJECT NO. H.000284 & NO. H.000286  
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Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com).

PLEASE CONSIDER THE FOLLOWING COMMENTS:

11646.3 Hwy, 90 Pearlinton Ms, Commercial Building  
2-story concrete & slab business, 18 yrs this  
is a bar & restaurant, I don't want long span  
bridge because of the pile driving it will destroy  
my building structure. What will be the financial  
impact on my business & for how long.  
What is the projected financial decrease of property  
value for Pearlinton Ms.  
I have had my business up for sale  
since April 2019, what is the projected time of  
starting & ending the construction.  
Would appreciate some answers.

NAME: Janyne Craspeaux DATE: 08/24/19

ADDRESS:

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS  
STATE PROJECT NO. H.000284 & NO. H.000286  
FEDERAL AID PROJECT NO. H000284 & NO. H000286

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LATER THAN September 3, 2019.

Mail to: US 90 Bridges  
Attn: Barry Brupbacher  
Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com).

PLEASE CONSIDER THE FOLLOWING COMMENTS:

I have the most property on Hwy. 90  
with 3 businesses. The high rise  
bridge would put me out of business.  
The draw bridge would be better  
and cheaper. Several other businesses  
would like to come but will not  
because of the high rise bridge.

NAME: JOHN PARLOVICH

DATE: 8-30-19

ADDRESS:

John Parlovich

PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019

US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS  
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Neel-Schaffer, Inc.  
P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com)

PLEASE CONSIDER THE FOLLOWING COMMENTS:

Ranked as to my preference:

- #1- End Vertical Lift bridge
- #2- End Bascula. bridge
- #3- Long Span bridge: Gussmerk option (Fig 3)
- #4- Long Span bridge: Loop option (Fig 2)

Thank you

NAME: Robert T. Bastian

DATE: 8/30/2019

ADDRESS:

us mail

**PUBLIC MEETING COMMENT FORM  
PEARLINGTON, LOUISIANA  
August 22, 2019**

**US 90 PEARL RIVER BRIDGES  
ENVIRONMENTAL ASSESSMENT  
ST. TAMMANY PARISH, LA and HANCOCK COUNTY, MS  
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P.O. Box 52565  
Lafayette, LA 70505

E-Mail Comment to [US90bridges@neel-schaffer.com](mailto:US90bridges@neel-schaffer.com)

PLEASE CONSIDER THE FOLLOWING COMMENTS:

AFTER LIVING IN PEARLINGTON FOR OVER 20YRS  
AND SEEING THE NEW EAST PEARL RIVER BRIDGE BREAK  
DOWN IT'S TIME FOR SOMETHING TO BE DONE.

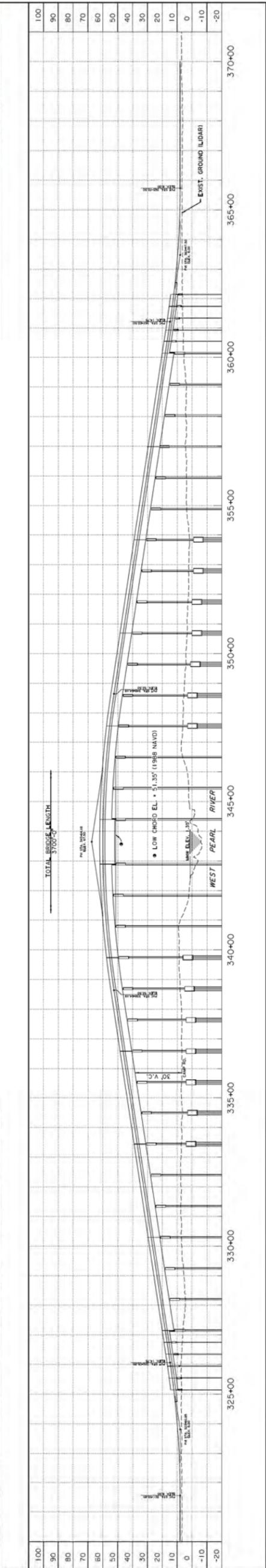
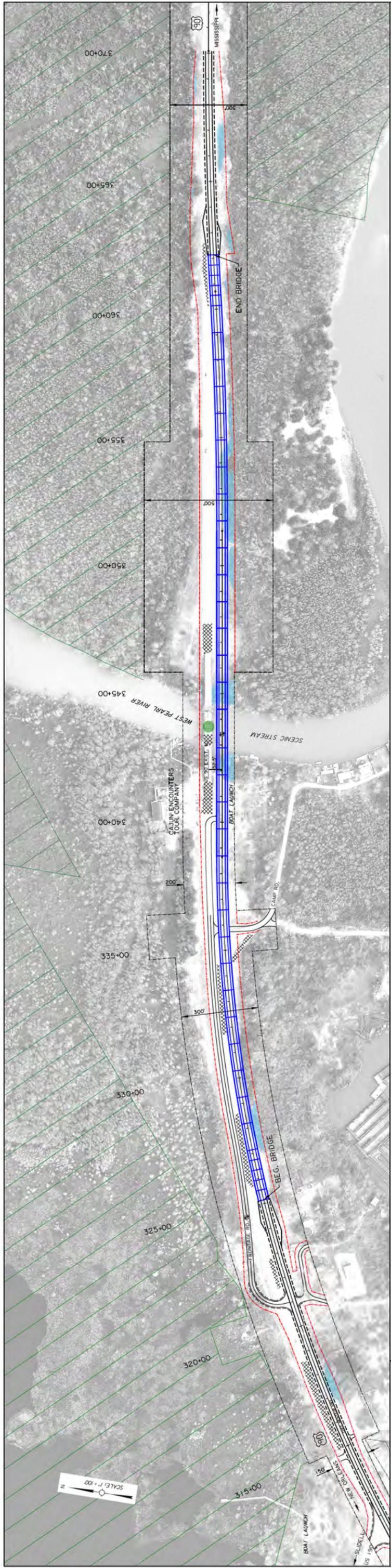
I VOTE FOR FIG. #4 MOVABLE BRIDGE.

NAME: Joseph W. Pancano  
ADDRESS:

DATE: 08-25-2019

# MEETING EXHIBITS

# **WEST PEARL RIVER BRIDGE**

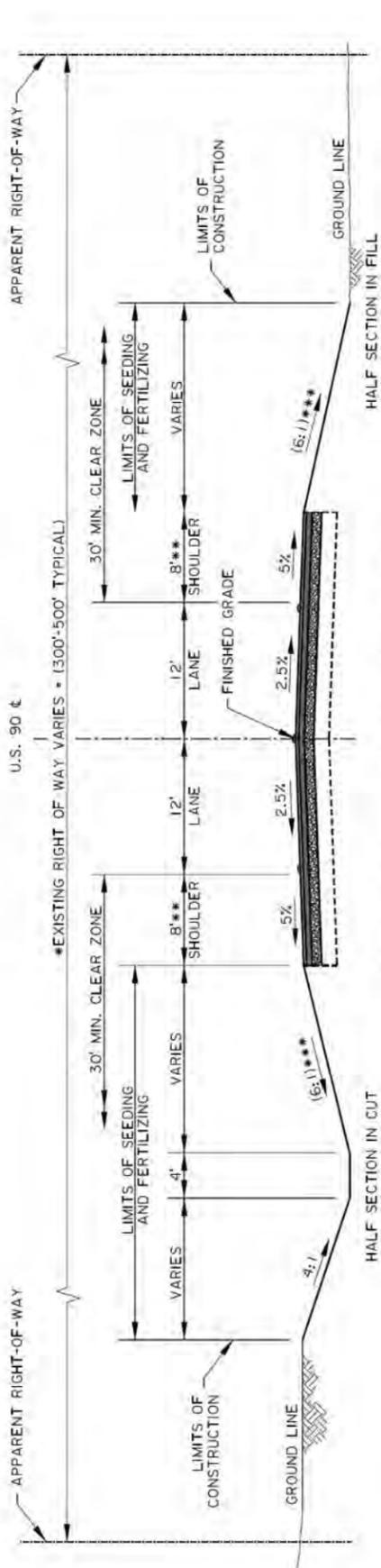


**FIXED SPAN BRIDGE ALTERNATIVE F-1**  
 STATE PROJECT NO. H-000284  
 US 90 PEARL RIVER BRIDGES  
 ST. TAMMANY PARISH

**LEGEND**

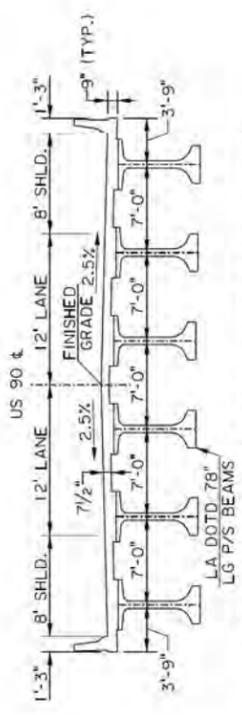
- HISTORIC STRUCTURE
- WILDLIFE MANAGEMENT AREA
- WATER OF THE DISTRICT
- PROPOSED BRIDGE
- REMOVAL
- EXIST. R/W
- RECD. R/W

SCALE: HORIZONTAL 1" = 100'  
 VERTICAL 1" = 5'

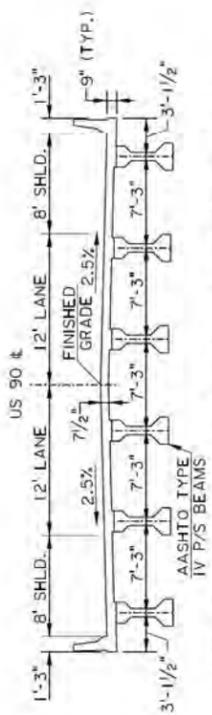


**ROAD TYPICAL FINISHED SECTION (N.T.S.)**

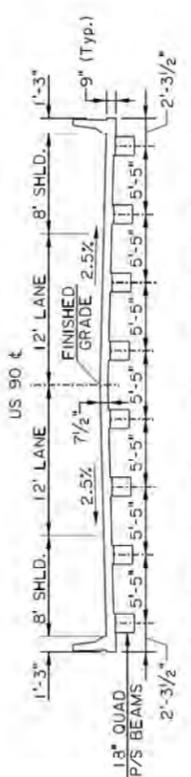
- EXISTING RIGHT-OF-WAY TAKEN FROM AS-BUILTS
- FULL WIDTH PAVED SHOULDER
- 20:1 FORESLOPE WAS USED ON AS-BUILTS



**150' MAIN SPAN TYPICAL SECTION (N.T.S.)**

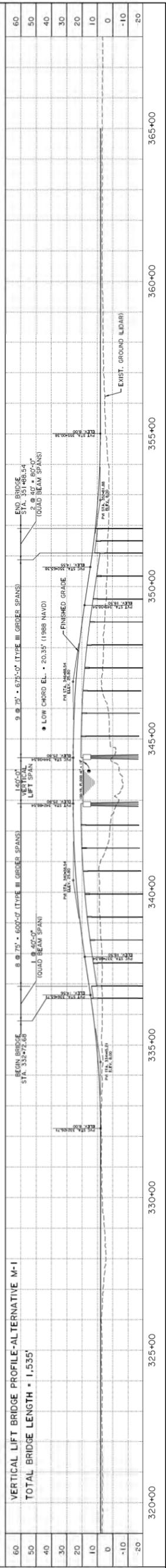
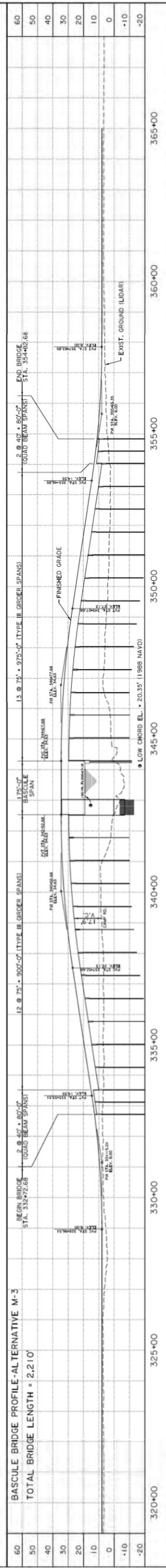
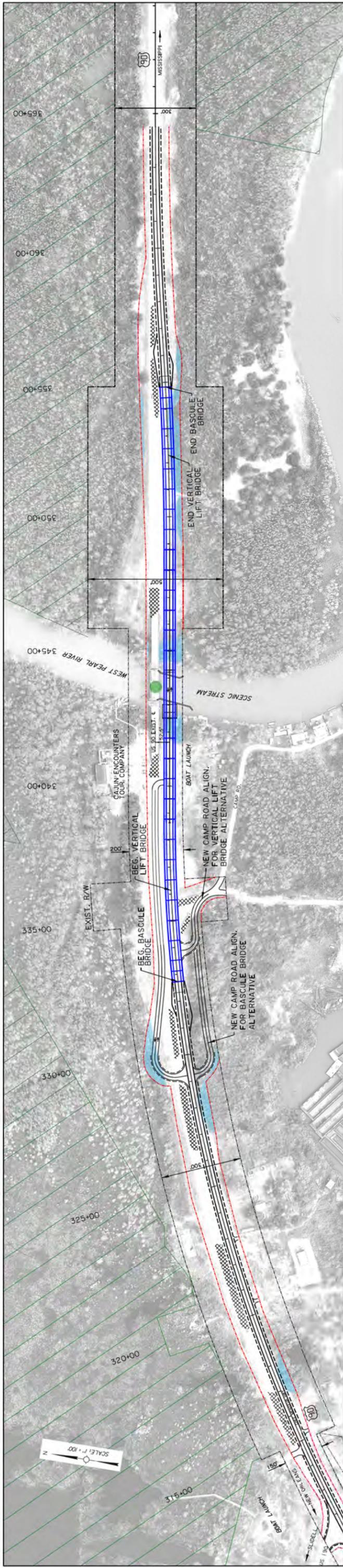


**105' APPROACH SPAN TYPICAL SECTION (N.T.S.)**



**40' END SPAN TYPICAL SECTION (N.T.S.)**

**ALTERNATIVE F-1 TYPICAL SECTIONS  
FIXED SPAN BRIDGE AT WEST PEARL RIVER**



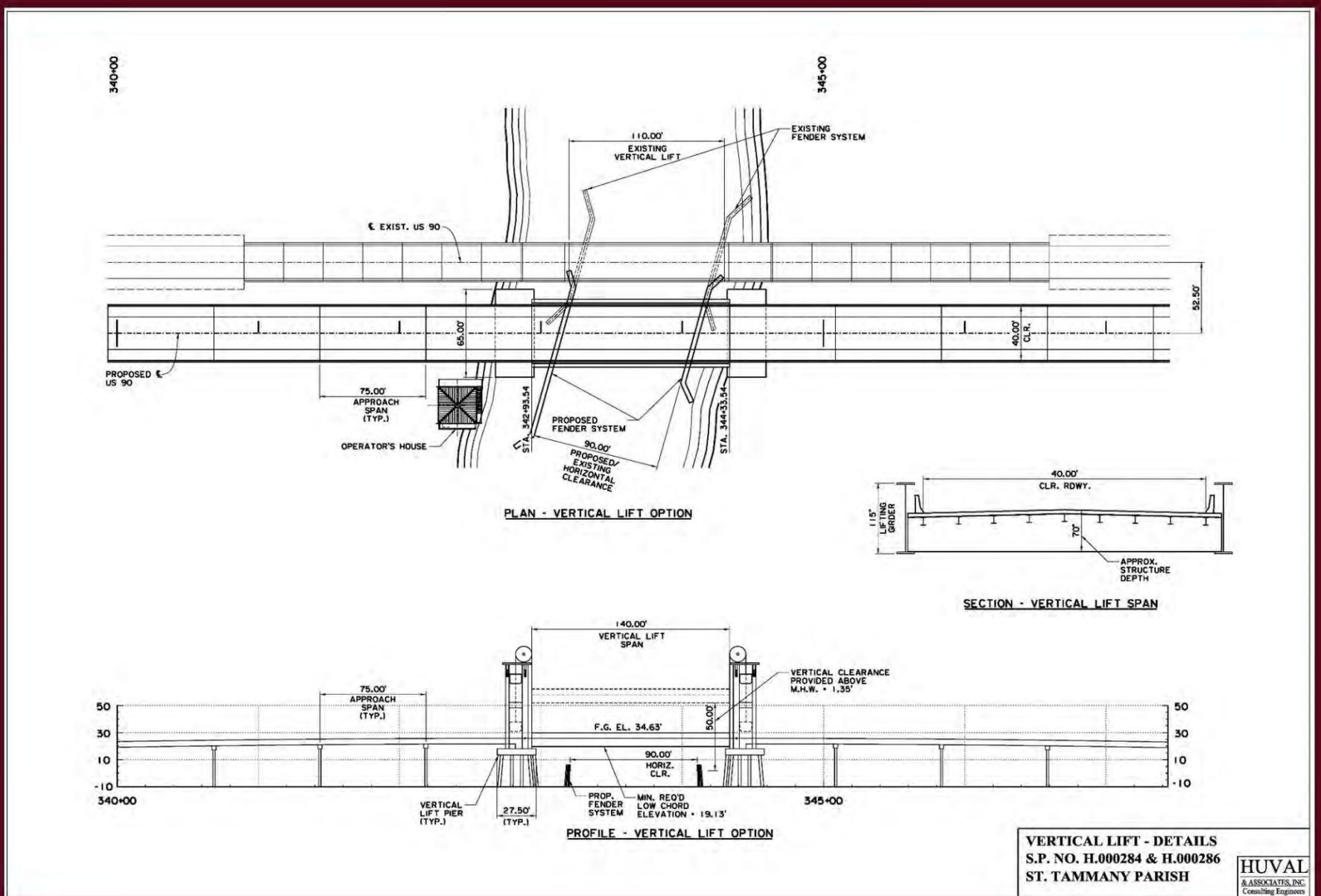
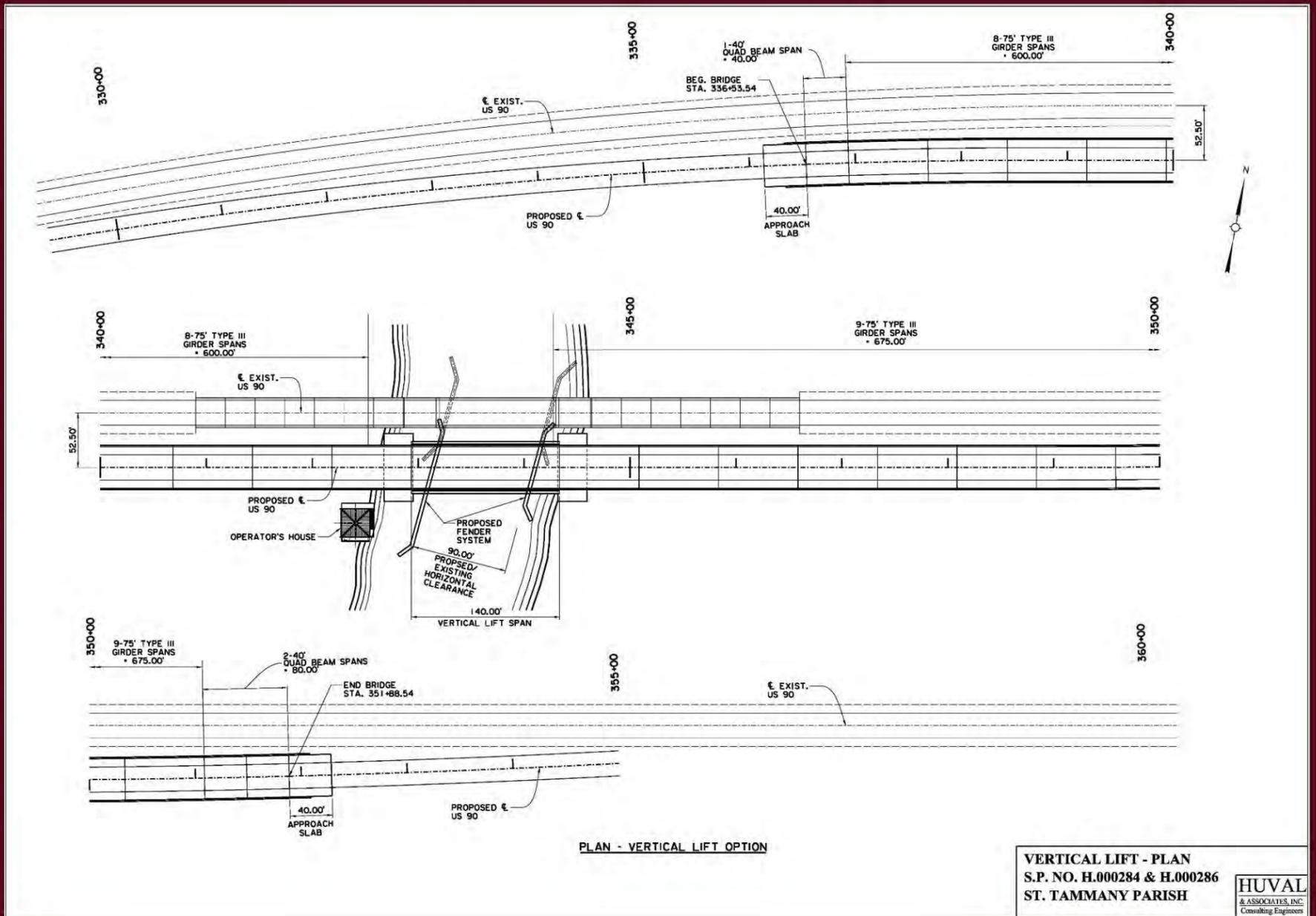
**MOVABLE BRIDGE ALTERNATIVES**  
 STATE PROJECT NO. H400286  
 US 90 PEARL RIVER BRIDGE  
 ST. TAMMANY PARISH

**LEGEND**

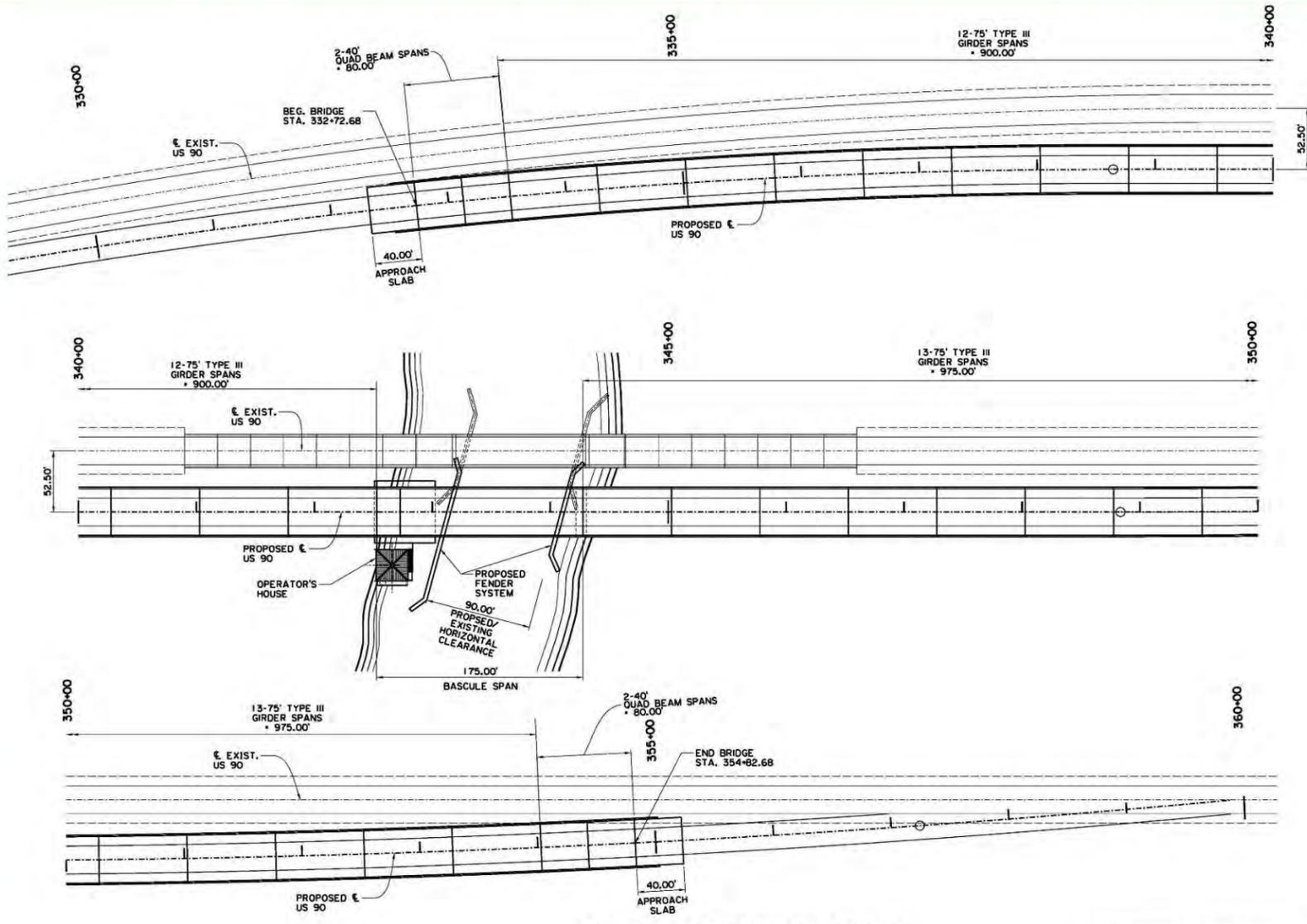
- HISTORIC STRUCTURE
- WILDLIFE MANAGEMENT AREA
- WETLANDS EXCLUDED WATER OF THE U.S.
- PROPOSED BRIDGE
- REMOVAL
- RECD RIGHT-OF-WAY
- EXIST. R/W

SCALES:  
 HORIZONTAL 1" = 100'  
 VERTICAL 1" = 5'

# West Pearl - Vertical Lift Option

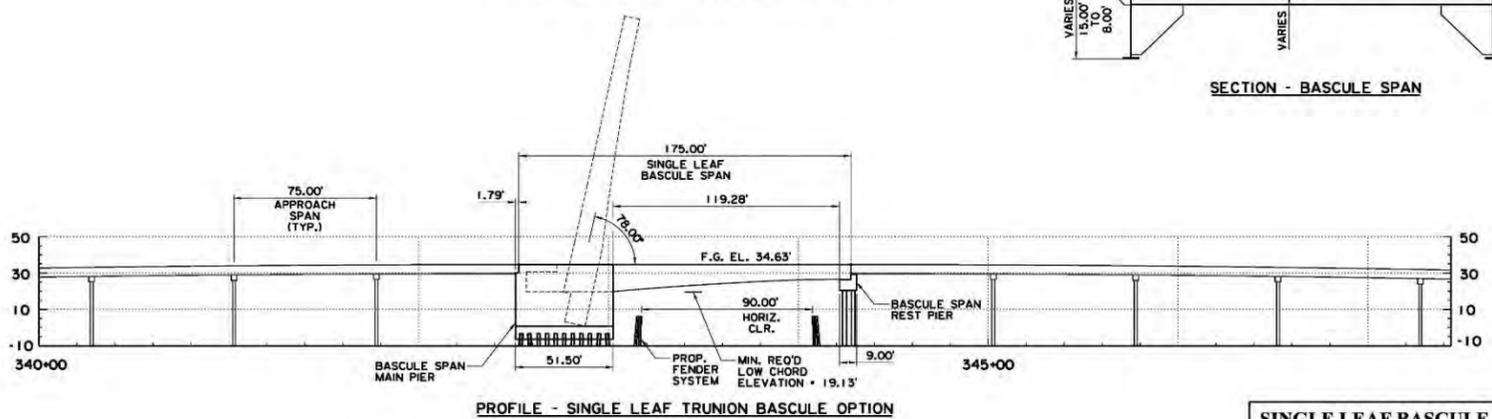
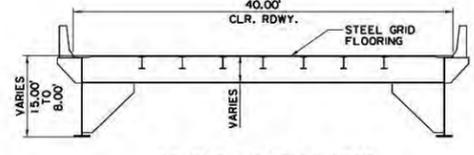
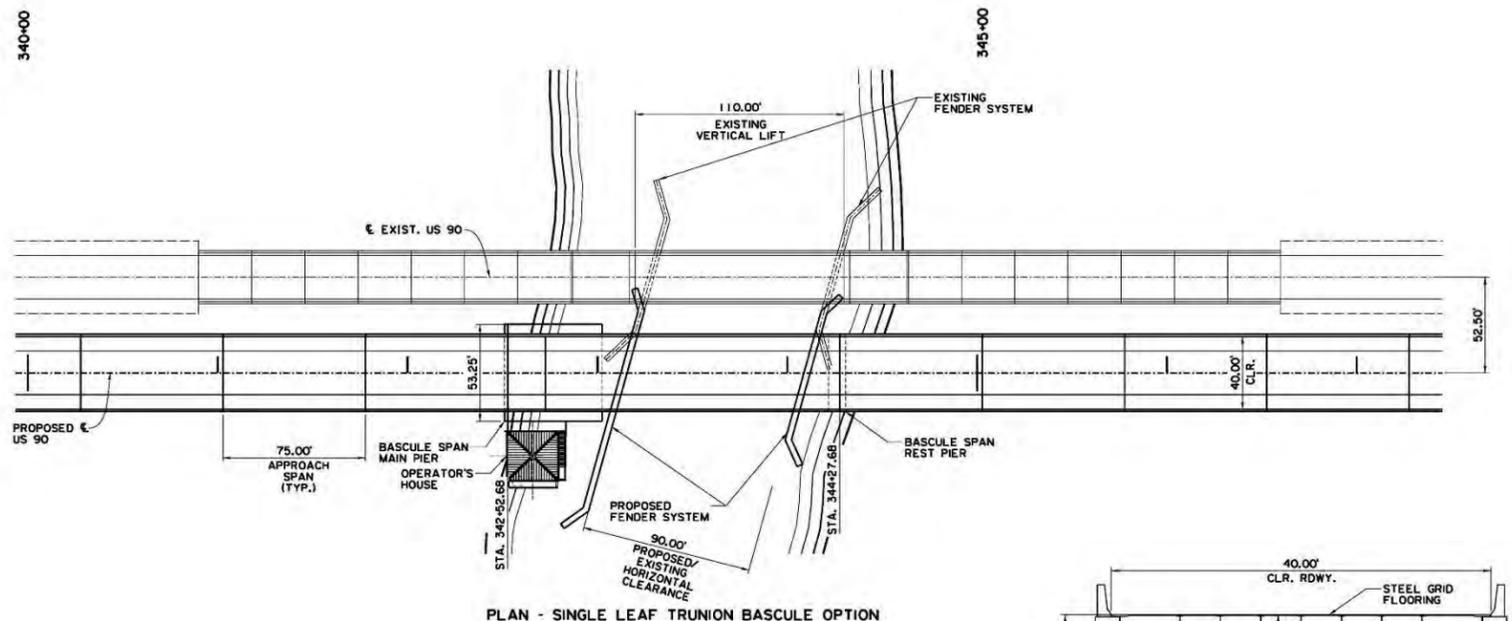


# West Pearl - Single Leaf Trunion Bascule Option



PLAN - SINGLE LEAF TRUNION BASCULE OPTION

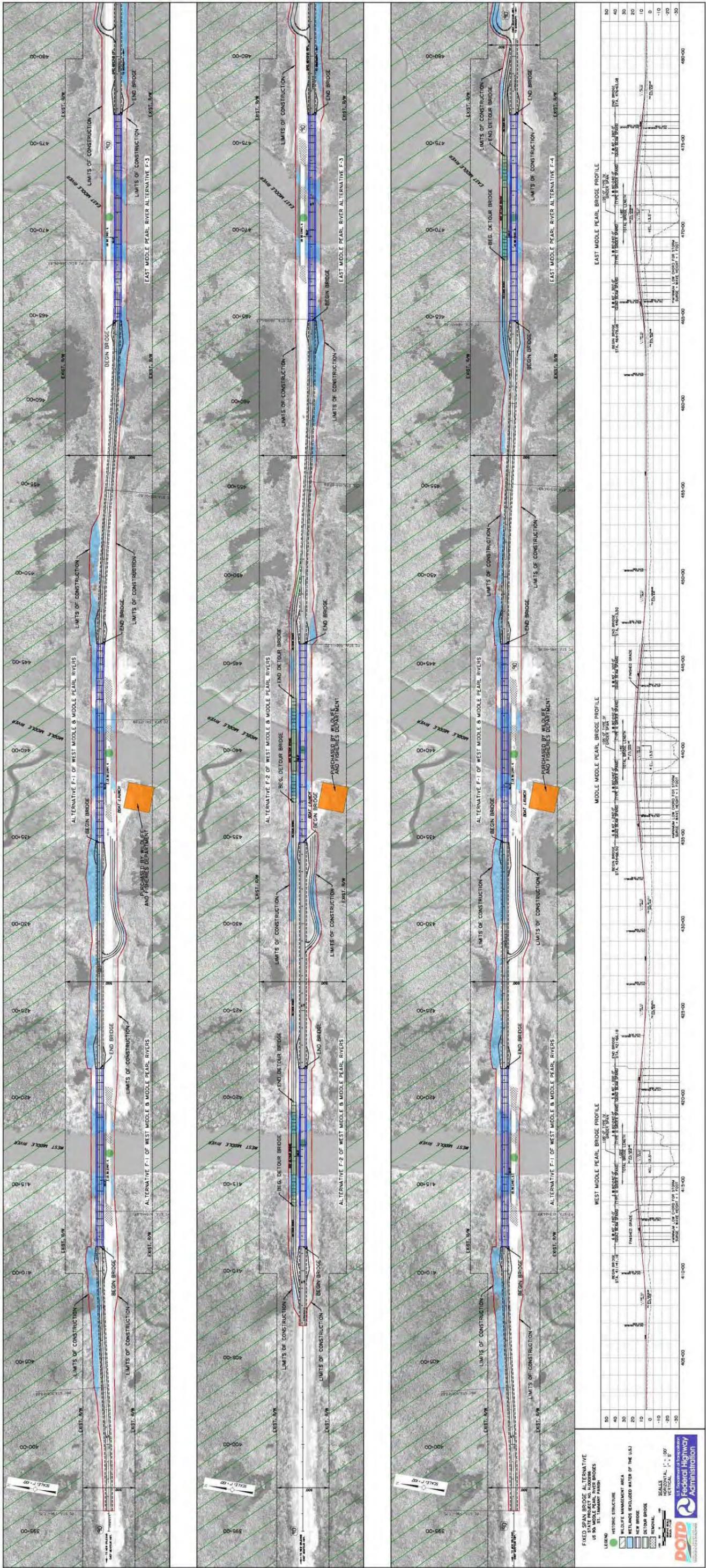
SINGLE LEAF BASCULE - PLAN  
 S.P. NO. H.000284 & H.000286  
 ST. TAMMANY PARISH

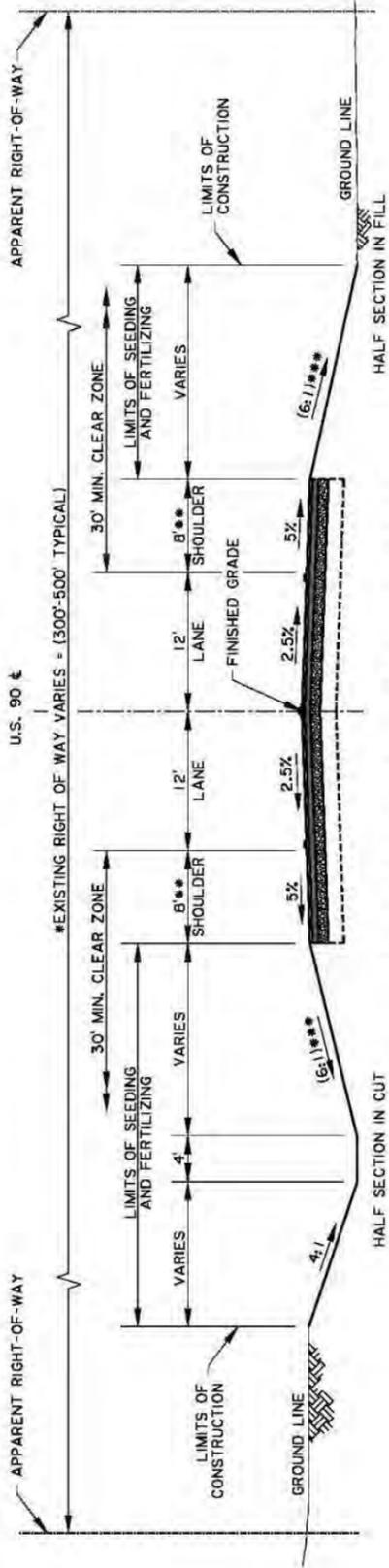


SINGLE LEAF BASCULE - DETAILS  
 S.P. NO. H.000284 & H.000286  
 ST. TAMMANY PARISH



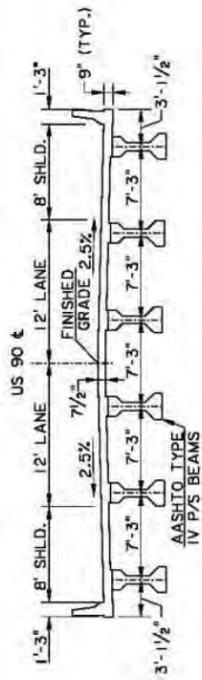
## Middle Pearl River Bridges



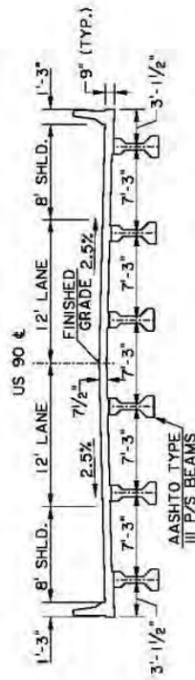


- \* EXISTING RIGHT-OF-WAY TAKEN FROM AS-BUILTS
- \*\* FULL WIDTH PAVED SHOULDER
- \*\*\* 20:1 FORESLOPE WAS USED ON AS-BUILTS

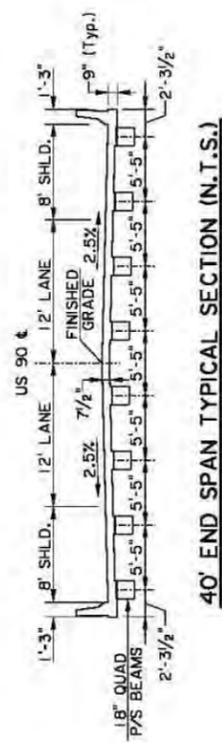
**ROAD TYPICAL FINISHED SECTION (N.T.S.)**



**105' MAIN SPAN TYPICAL SECTION (N.T.S.)**



**80' APPROACH SPAN TYPICAL SECTION (N.T.S.)**

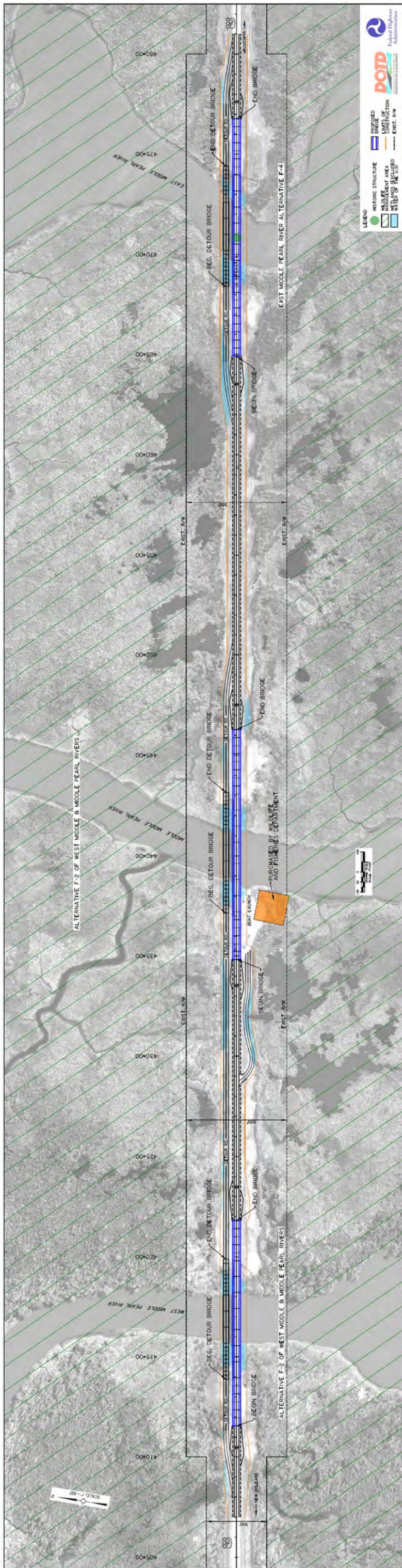


**40' END SPAN TYPICAL SECTION (N.T.S.)**

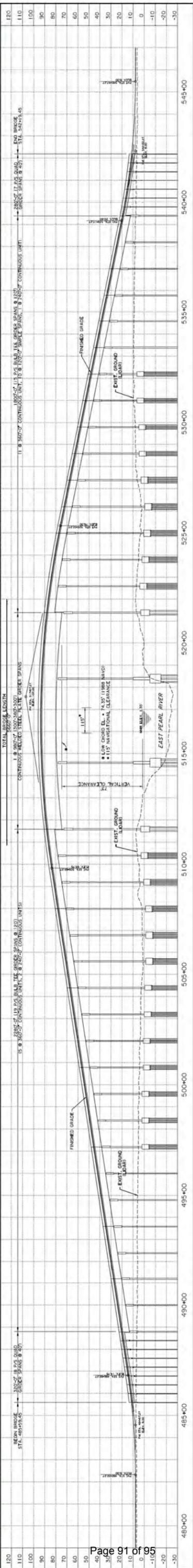
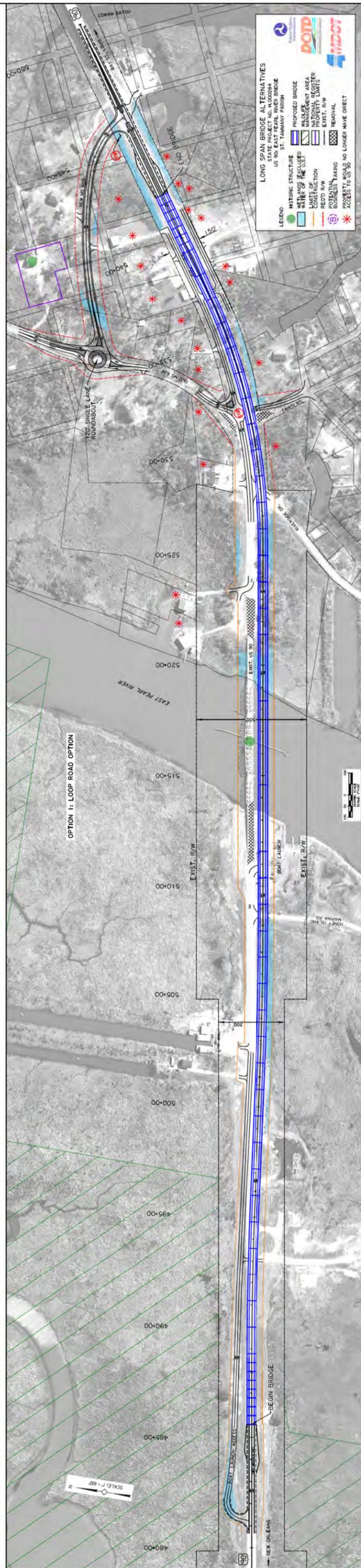
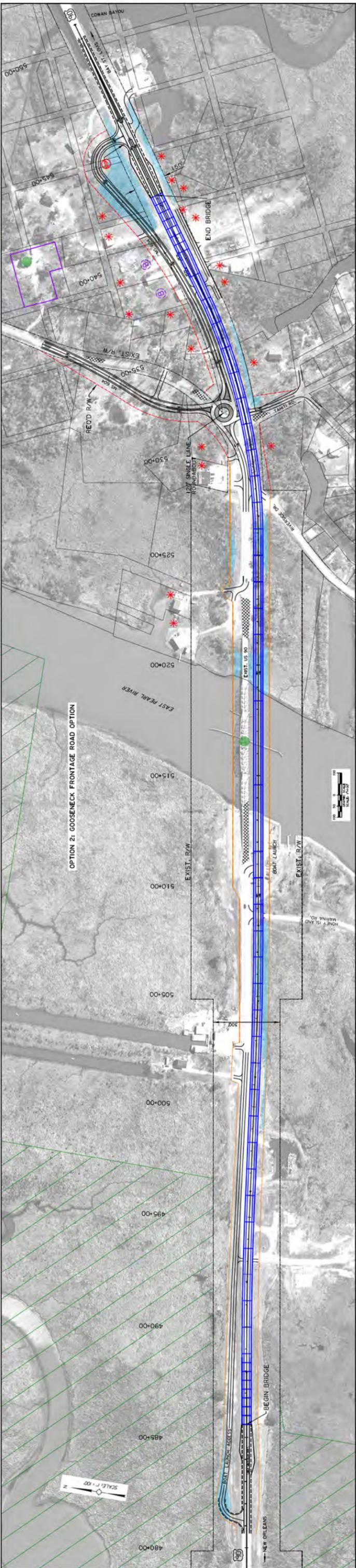


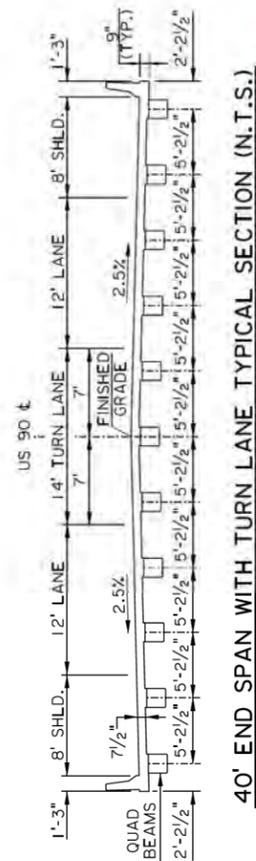
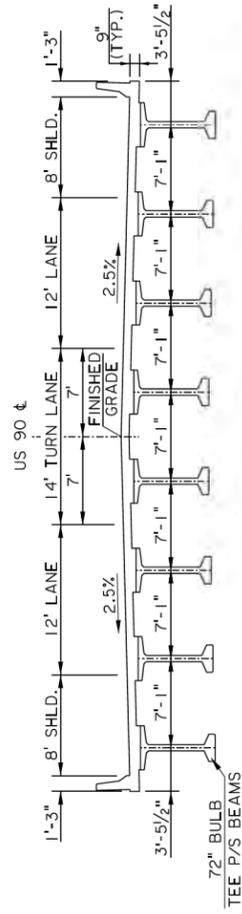
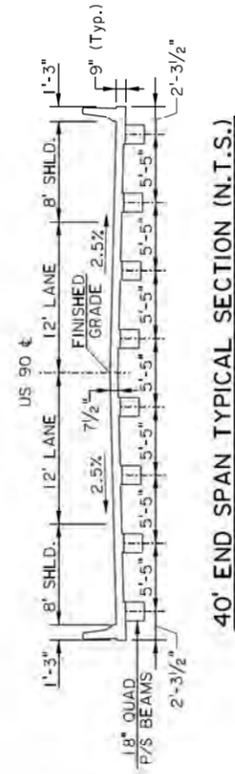
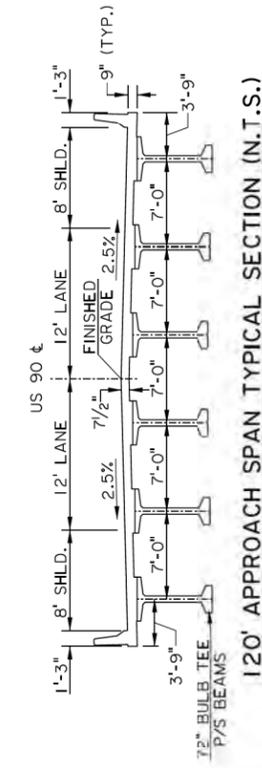
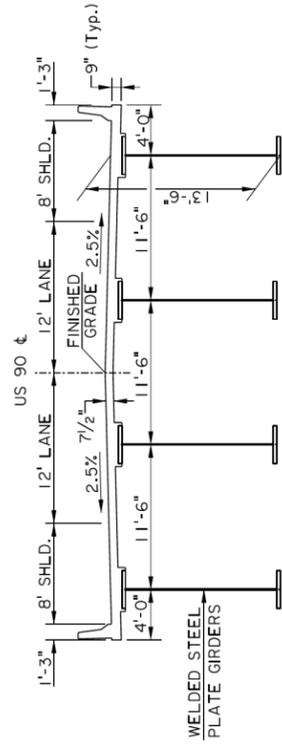
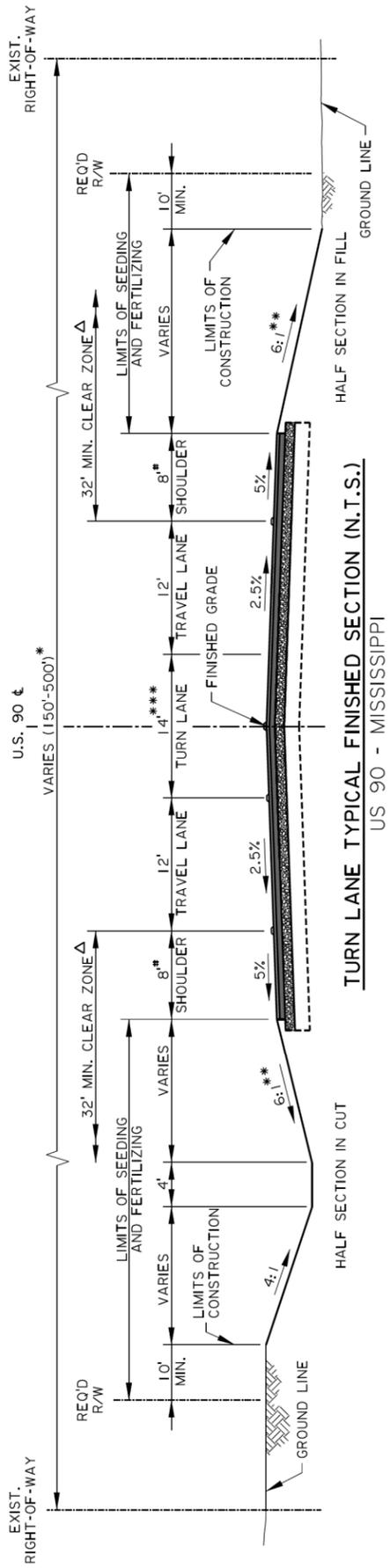
**TYPICAL SECTIONS  
FIXED SPAN BRIDGE AT 3 MIDDLE PEARL RIVERS**





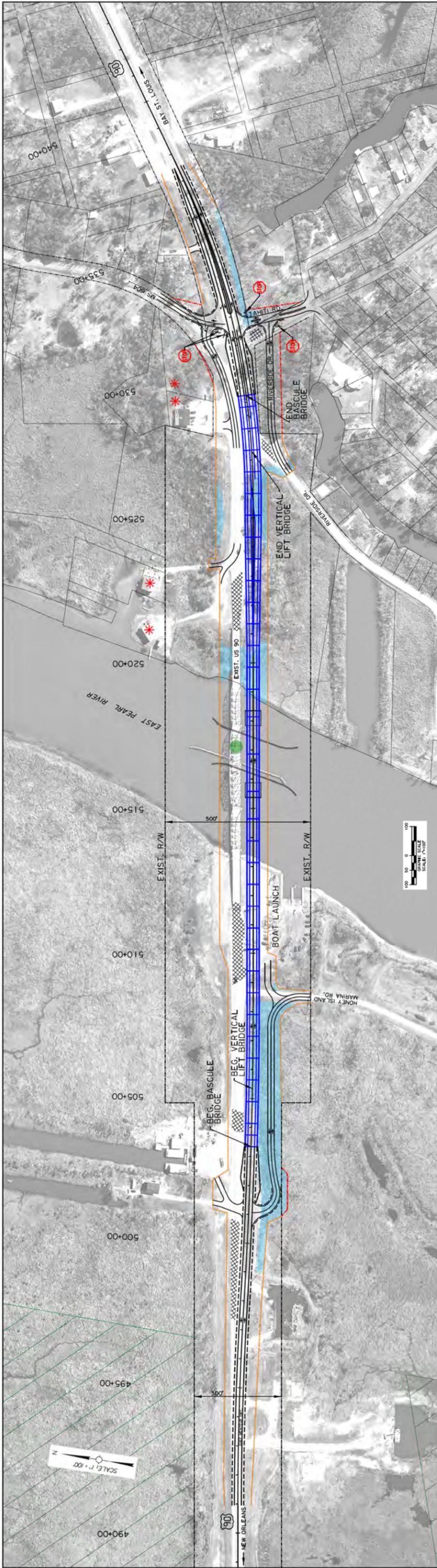
# East Pearl River Bridge





ALTERNATIVE F-4 TYPICAL SECTIONS  
LONG SPAN BRIDGE AT EAST PEARL RIVER





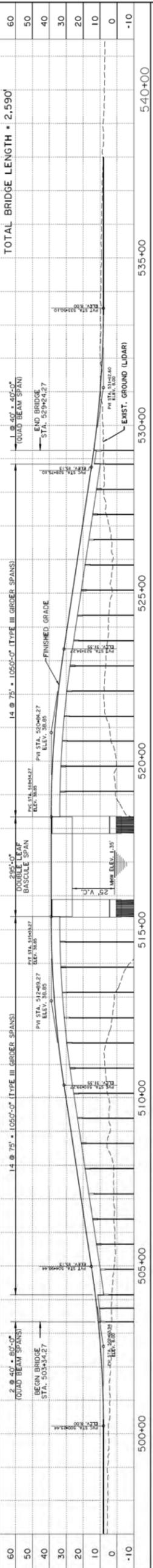
**MOVABLE BRIDGE ALTERNATIVES**  
 STATE PROJECT NO. H-000284  
 US 90: MIDDLE PEARL RIVER BRIDGES  
 ST. TAMMANY PARISH

- LEGEND**
- HISTORIC STRUCTURE
  - WILDLIFE MANAGEMENT AREA WETLANDS (EXCLUDED WATER OF THE U.S.)
  - PROPOSED BRIDGE REMOVAL
  - RECD R/W
  - LIMITS OF CONSTRUCTION
  - EXIST. R/W

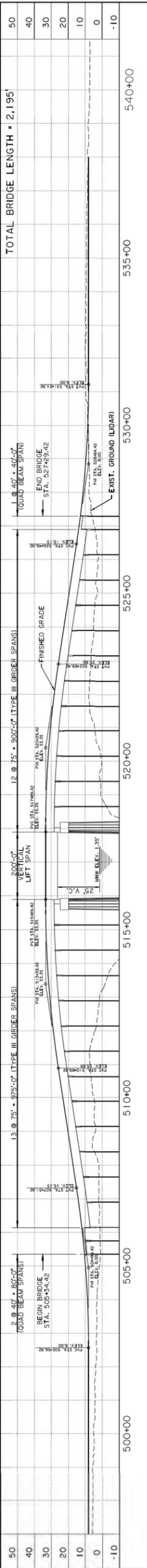
PROPERTY WOULD NO LONGER HAVE DIRECT ACCESS TO US 90



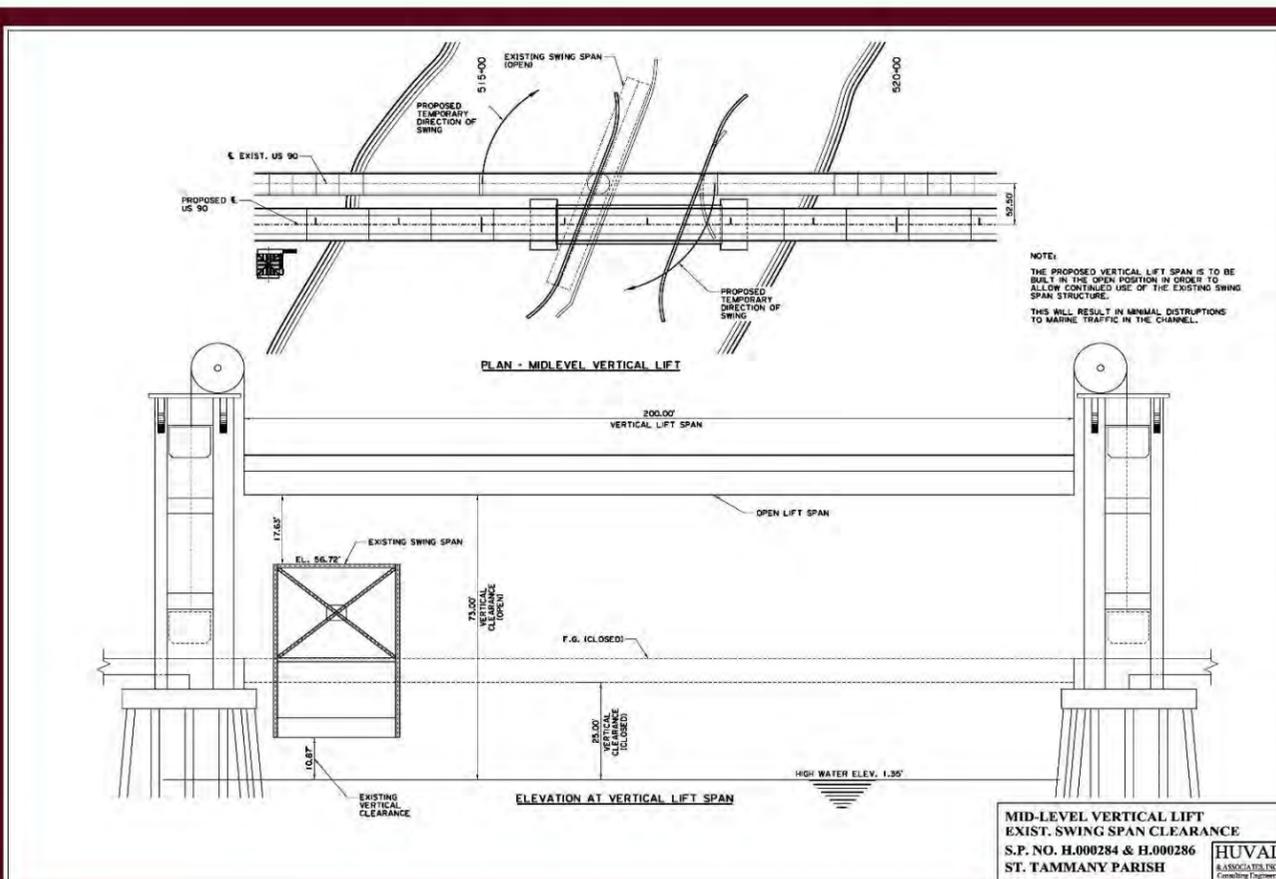
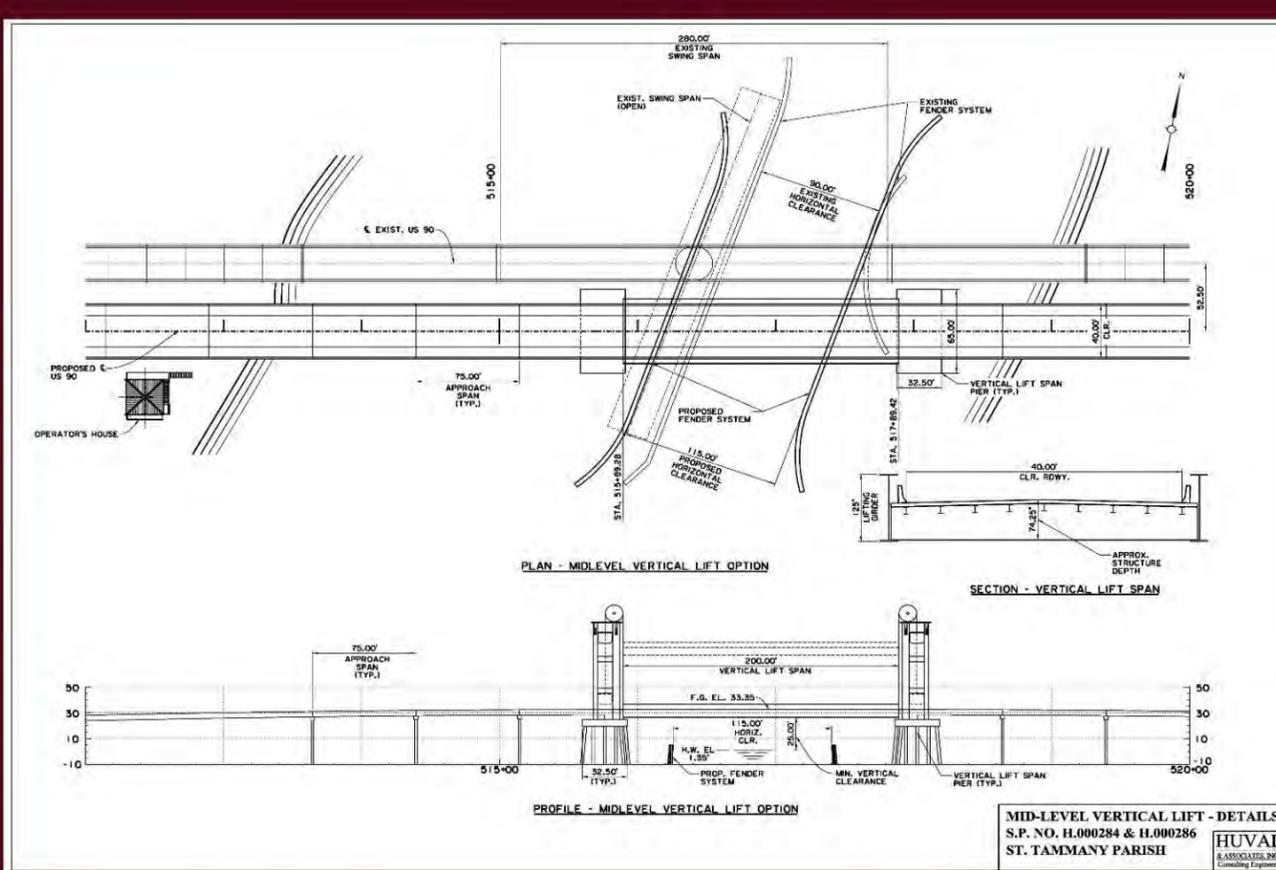
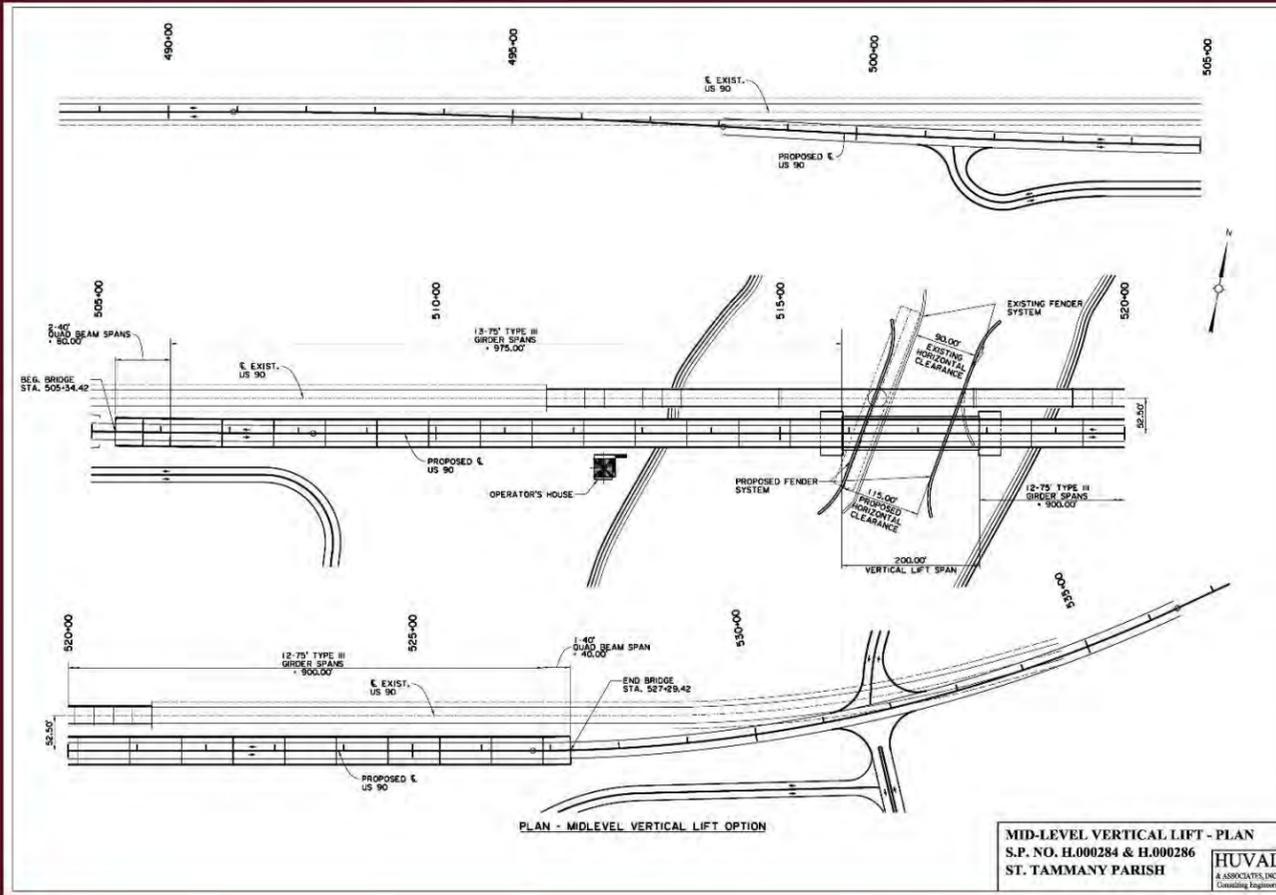
**BASCULE BRIDGE PROFILE-ALTERNATIVE M-4D**



**VERTICAL LIFT BRIDGE PROFILE-ALTERNATIVE M-4B**



# East Pearl - Mid-Level Vertical Lift Option



# East Pearl - Mid-Level Bascule Option

