



US 11 Norfolk Southern Railroad Environmental Assessment Route US 11

State Project No. H.000688.2
Federal Aid Project No. H000688
St. Tammany Parish, Louisiana

November 2015



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EXECUTIVE SUMMARY

Description of the Proposed Project

The Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA), in cooperation with the New Orleans Regional Planning Commission (RPC), propose to replace the US Highway 11 (US 11) bridge over the Norfolk Southern (NS) railroad and widen US 11 from two lanes to four lanes from U.S. Highway 190 (Gause Boulevard; US 190) north to Interstate 12 (I-12) in Slidell, St. Tammany Parish, Louisiana. The RPC is the designated Metropolitan Planning Organization for the New Orleans urbanized area including St. Tammany Parish.

The proposed action is identified as a Tier II – On System Unfunded Project for fiscal year 2015 – 2024 in the *Metropolitan Transportation Plan, St. Tammany Parish Urbanized Areas, Fiscal Years 2011-2040* and in the Transportation Improvement Plan for the parish. The proposed project was included as a financially constrained priority project in the *Transportation Improvement Plan, St. Tammany Parish Urbanized Areas, Fiscal Years 2012-2016*.

The study of the alternatives developed in this environmental assessment and the associated environmental consequences were evaluated according to the National Environmental Policy Act (NEPA); LADOTD’s Stage 1 Planning/Environmental Manual of Standard Practice; and FHWA Guidance for Preparing and Processing Environmental and Section 4(f) Documents.

Study Area

The Study Area is located in the southeastern portion of St. Tammany Parish and extends south along US 11 from Powell Drive to the intersection

of US 11 at Florida Avenue, a distance of approximately 1.7 miles, and includes the logical termini and the area that may be impacted by the indirect and cumulative impacts from the proposed project (Figure ES-1).

Project Purpose

The purpose of the project is to address safety features of the bridge crossing the NS railroad and system reliability of the roadway segment from US 190 north to Powell Drive.

Project Need

The project is needed in order to upgrade the bridge and roadway segment to current design standards for travel lane and shoulder widths, bridge width, stopping sight distances, and design speed, as well as system reliability. Roadway segment and bridge improvements will also provide better accommodations for bicycle users and will serve the surrounding community and the larger metropolitan area. More specifically, needs for the proposed project include:

- Address the safety features of the US 11 bridge crossing over the NS railroad;
- Improve capacity;
- Improve travel time;
- Relieve congestion on US 11 by removing the bottleneck between US 190 and I-12;
- Relieve future congestion on area roadways; and
- Improve area-wide mobility and system reliability.

ENVIRONMENTAL ASSESSMENT

Alternatives Development

Early coordination with federal, state, and local agencies as well as NS solicited comments and responses that were combined with available environmental data. This information was used to help determine if the preliminary alternatives impact certain human, natural, or cultural resources that would result in the decision to dismiss an alternative from further evaluation. This coordination resulted in the dismissal of an at-grade crossing as a viable alternative.

To minimize impacts and reduce the amount of right-of-way to be acquired, alignments were located as close to the existing US 11 roadway as LADOTD design standards and project construction limits would allow. Policies such as roadway and bridge design, intersection configuration, traffic, noise, and minimization of social and environmental impacts were also considered in the alternatives development. Requirements for horizontal and vertical clearances for the railroad crossing were evaluated, and cost and constructability factors were also considered. The roadway segments north and south of the bridge follow the existing roadway alignment providing widening and improvements to accommodate a four-lane divided highway. This resulted in one alternative for the roadway segments, identified as Alternative 1. Additional bridge and intersection configurations were also developed.

Potential alignment revisions were reviewed and incorporated into the preliminary alignments following the officials and public meetings held on August 22, 2013. Suggestions were considered to shift the roadway segment east on the north side of the bridge in order to reduce impacts to residential and commercial properties.

The public expressed a need for bicycle and/or pedestrian facilities within the Study Area. Currently, the City of Slidell does not have a master plan that includes bicycle and/or pedestrian improvements along US 11. Bicycle and pedestrian improvements were evaluated in accordance with LADOTD's Complete Streets

Policy and in coordination with the City of Slidell and St. Tammany Parish.

A revision to Alternative 1, Bridge 1 resulted in Alternative 1 Revised (Alternative 1R) and includes a roadway centerline shift to the east on the north side of the US 11 bridge, which continues along this new alignment toward North Boulevard. A right-turn lane was added from the northbound lanes at North Boulevard — with two through lanes continuing north that shift back to the west — in order to meet recent roadway widening improvements at US 11 and Powell Drive.

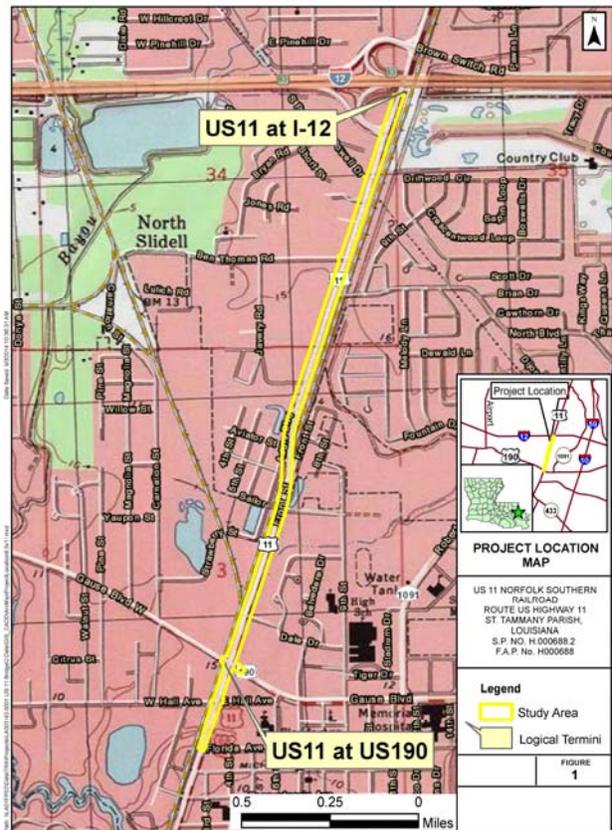


Figure ES-1: Study Area and Location Map

Bridge Rehabilitation

Following early agency coordination, the bridge over the NS railroad was identified as eligible for the National Register of Historic Places (NRHP). The LADOTD in cooperation with the FHWA and the State Historic Preservation Office completed a statewide historic bridge inventory for bridges constructed prior to 1971. A *National Register*

Eligibility Documentation Report (September 2013) was prepared by Mead & Hunt. The FHWA made final NRHP eligibility determinations, which are presented in the Mead & Hunt report, and the SHPO has concurred with those determinations.

The NRHP employs four criteria for evaluation in determining eligibility: A, B, C, and D. This bridge was determined eligible under Criterion A Events, which applies to structures that have an important association with single events, a pattern of events, repeated activities, or historic trends that are significant within the context of Louisiana's transportation and bridge-building history.

The bridge is also potentially eligible for protection under Section 4(f) of the *Department of Transportation Act of 1966* (Section 4(f)) and Section 106 of the National Historic Preservation Act (Section 106).

Bridge rehabilitation was included in the list of preliminary alternatives as the only potential means of preserving the bridge. Rehabilitation to the original condition without changing the 1930s bridge design features such as lane widths and lack of shoulders was one form of rehabilitation considered. However, to remain in service, the bridge deficiencies related to its age and design must be addressed.

Rehabilitation that maintains the existing bridge would not sufficiently address structural and functional deficiencies of the bridge. Current LADOTD roadway design criteria cannot be met. Therefore, rehabilitation of the existing bridge was dismissed from further consideration.

Alternatives Comparison

Resource Impact Analysis

Alternatives were evaluated with respect to environmental resources and engineering factors. No impacts were identified for churches, public facilities, cemeteries, farmlands, archaeological resources, or oil and gas wells. Table ES-1 provides a summary of resources that will be

impacted and preliminary cost analyses for the alternatives.

Preferred Alternative

As a result of the comprehensive resources evaluation, traffic studies, and coordination with public, local, state, and federal officials or agencies, sufficient information and public opinion exist to identify Alternative 1R, Bridge 1, Intersection Configuration Option 3 as the Preferred Alternative (Figures ES-2A and ES-2B). This alternative includes the widening of US 11 from Hall Avenue to Powell Avenue and replacement of the existing NS bridge with one situated along the existing bridge alignment. The Preferred Alternative provides for a shared use path as part of the widened roadway connecting to the local roadway network.

The identification of the Preferred Alternative addresses the stated purpose and need and satisfies, to the fullest extent possible, the objectives of NEPA. Impacts from the Preferred Alternative were avoided where possible and minimized to the greatest extent practicable.

Project implementation will include permits, mitigation, and commitments that will be incorporated into the design and construction phases of the project:

- Louisiana Department of Natural Resources Coastal Use Permit
- U.S. Army Corps of Engineers Wetland Permit
- Louisiana Department of Environmental Quality National Pollutant Discharge Elimination System Permit for Construction-Related Activities
- Traffic Control
- Property Access
- Section 401 Water Quality
- Bicycle/Pedestrian Facilities
- Relocation

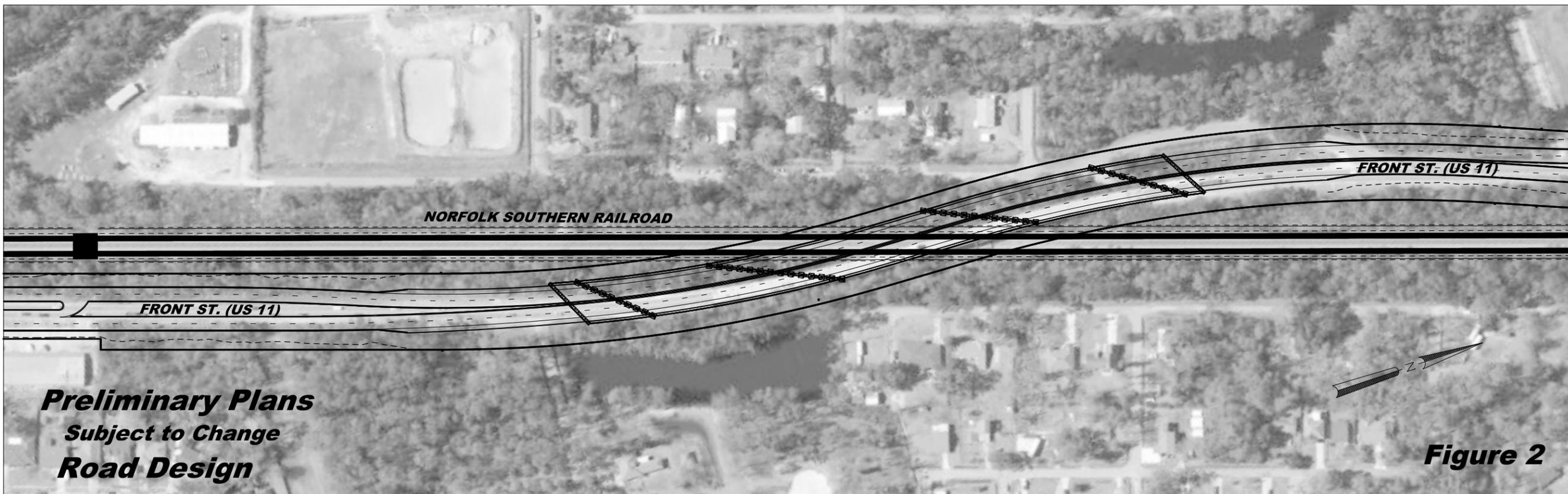
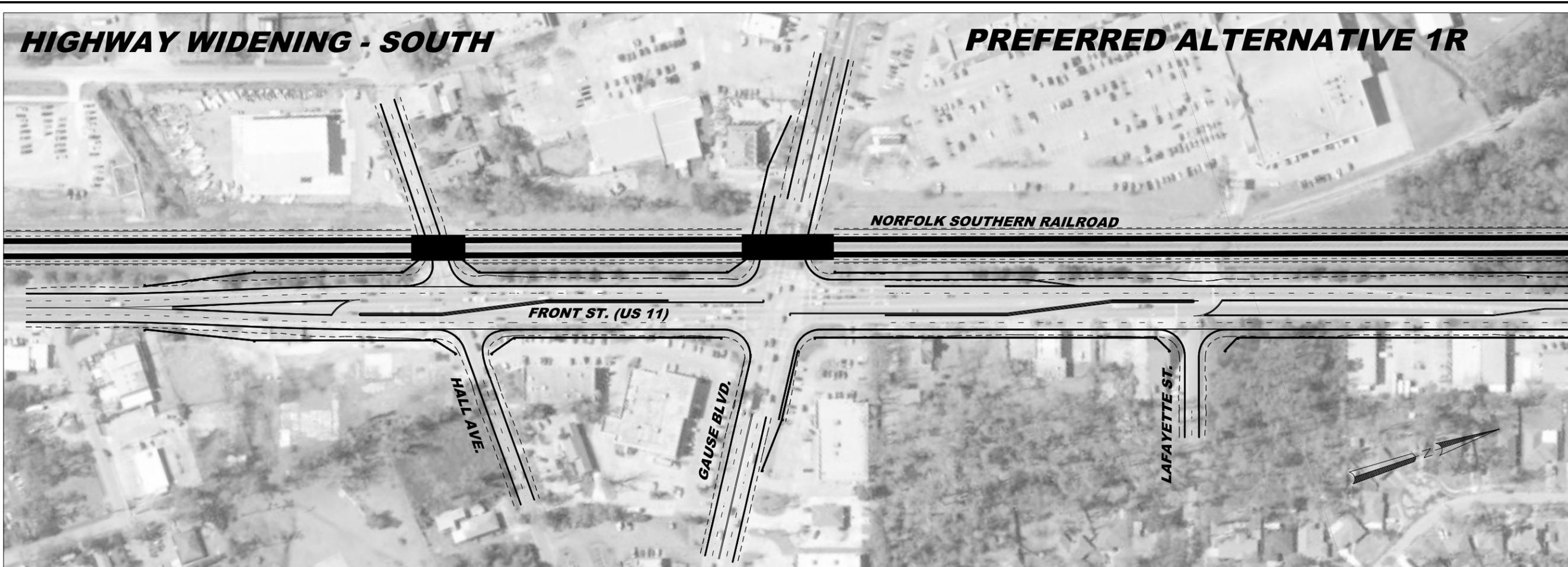
ENVIRONMENTAL ASSESSMENT

Table ES-1: Alternatives Evaluation Matrix

Evaluation Factors	Alternative 1, Bridge 1			Alternative 1R, Bridge 1			No Build
	Intersection Configuration Option			Intersection Configuration Option			
	1	2	3	1	2	3	
Structure Impacts							
Residence	0	0	0	0	0	0	0
Business	13	13	13	13	13	13	0
Underground Storage Tanks/Pumps/Piping	6	6	6	6	6	6	0
Signage	11	11	11	11	11	11	0
Parking Spaces	107	107	107	107	107	107	0
Potential Relocations	13	13	13	13	13	13	0
Noise	10	10	10	10	10	10	4
Natural Resources Impacts							
100-Year Floodplain (acres)	7	7	7	6	6	6	0
Surface Waters (acres)	<1	<1	<1	<1	<1	<1	0
Wetland (acres)	<1	<1	<1	<1	<1	<1	0
Historic Resources > 50 Years Old	23	23	23	23	23	23	0
Historic Resources—Potentially Eligible	2	2	2	2	2	2	0
Known Underground Storage Tank Sites	3	3	3	3	3	3	0
Water Wells	3	3	0	3	3	3	0
Traffic Impacts							
Permanent Road Closures	2	2	2	2	2	2	0
Temporary Detour	0	0	0	1	1	1	0
COST ANALYSIS							
Total Length (miles)	1.75	1.75	1.75	1.75	1.75	1.75	0
Cost (million dollars)							
Roadway Construction	13.1	13.1	13.1	13.1	13.1	13.1	0
Bridge Construction	35.8	35.8	35.8	35.8	35.8	35.8	0
Utility Relocation	0.51	0.51	0.51	0.51	0.51	0.51	0
Right-of-Way Acquisition	7.1	7.1	7.1	6.8	6.8	6.8	0
Wetland Mitigation	0.16	0.16	0.16	0.16	0.16	0.16	0
Surveying, Engineering, Construction Supervision & Inspection	3.8	3.8	3.8	3.8	3.8	3.8	0
TOTAL	60.5	60.5	60.5	60.2	60.2	60.2	0

Includes 20% Roadway and 10% Bridge Contingency for Estimating Purposes. Costs Rounded.

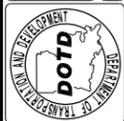
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REVISION DESCRIPTION		STATE PROJECT	H.000688.2
NO.		DATE	
BY			

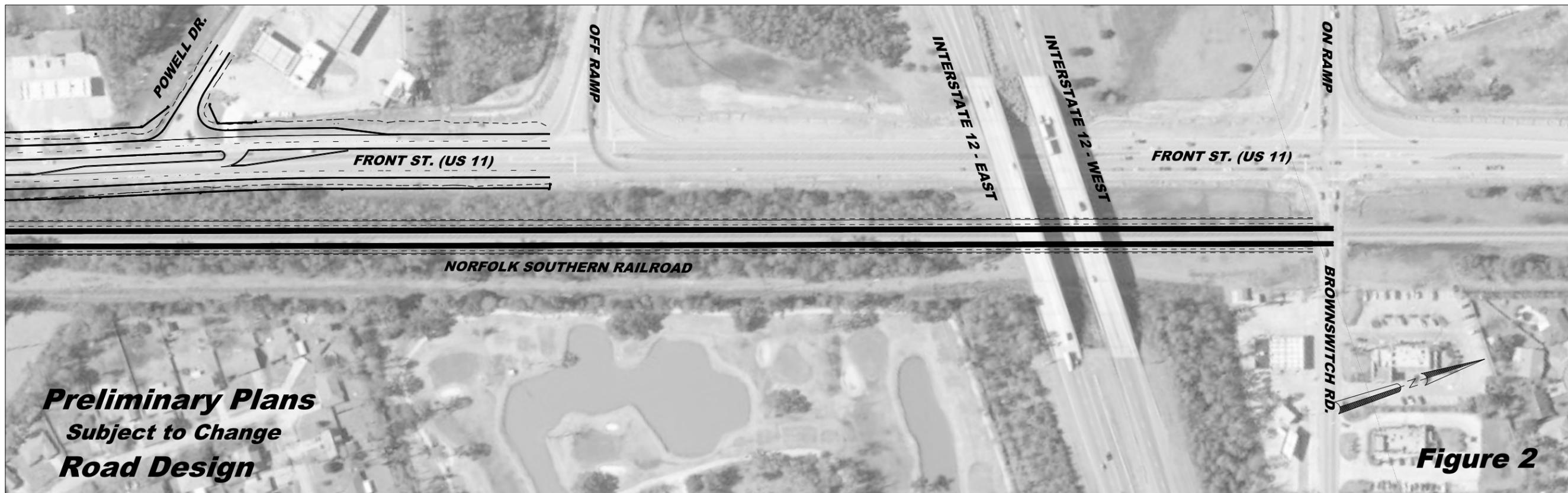
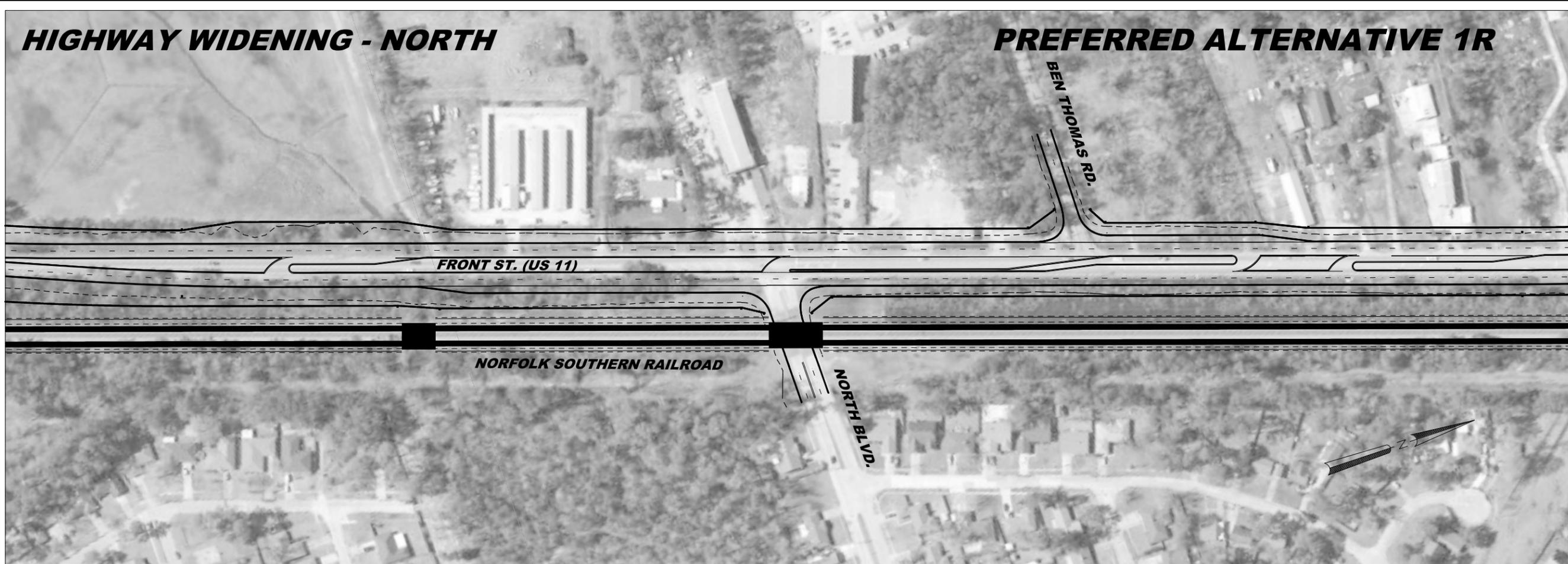


US 11 NORFOLK SOUTHERN RAILROAD BRIDGE



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Preliminary Plans
 Subject to Change
 Road Design

Figure 2

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US 11 NORFOLK SOUTHERN RAILROAD BRIDGE					
					
					

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ENVIRONMENTAL DETERMINATION CHECKLIST

State Project No. H.000688.2
Federal Aid No. 000688
Name: US 11 Norfolk Southern Railroad
Route: US 11
Parish: St. Tammany

1. General Information

- Status: () Conceptual Layout () Plan-in-Hand
(X) Line and Grade () Preliminary Plans
() Survey () Final Design

2. Class of Action

- () Environmental Impact Statement (E.I.S.)
(X) Environmental Assessment (E.A.)
() Categorical Exclusion (C.E.)
() Programmatic C.E. (as defined in letter of agreement dated 03/15/95, does not require FHWA approval)

3. Project Description (use attachment if necessary)

See Sections 1, 2, and 3.

4. Public Involvement

- (X) Views were solicited on May 3, 2013. Responses are attached.
() No adverse comments were received.
() Comments are addressed in attachment.
() Views were not solicited.
() A public hearing (P/H)/Opportunity is not required.
() An opportunity for requesting a P/H will be afforded upon your concurrence.
() Opportunity was afforded, with no requests for P/H.
(X) A Public Hearing was held on
(X) A Public Meeting was held on August 22, 2013.

5. Real Estate

Table with 2 columns: Question, NO, YES. Rows include: Will additional right-of-way be required?, Will any relocations be required?, Are construction or drainage servitudes required?, Will right-of-way be required from a Wetland Reserve Program (WRP) property?.....

6. Cultural and 106 Impacts

	NO	YES
a. Section 4(f) or 6(f) lands		
Are any impacted by the project? (If so, list below).....	()	(X)
Are any adjacent to the project? (If so, list below).....	(X)	()
b. Known Historic sites/structures		
Are any impacted by the project? (If so, list below).....	(X)	()
Are any adjacent to the project? (If so, list below).....	()	(X)
c. Known Archaeological sites		
Are any impacted by the project? (If so, list site # below).....	(X)	()
Are any adjacent to the project? (If so, list site # below).....	(X)	()
d. Cemeteries		
Are any impacted by the project? (If so, list below).....	(X)	()
Are any adjacent to the project? (If so, list below).....	(X)	()
e. Historic Bridges	()	(X)

7. Wetlands

	NO	YES
a. Are wetlands being affected?.....	()	(X)
b. Are other waters of the U.S. being affected?.....	()	(X)
c. Can C.O.E. Nationwide Permit be used?.....	()	(X)

8. Natural Environment

	NO	YES
a. Endangered/Threatened Species/Habitat.....	(X)	()
b. Within 100 Year Floodplain?.....	()	(X)
Is project a significant encroachment in Floodplain?.....	(X)	()
c. In Coastal Zone Management Area?.....	()	(X)
Is the project consistent with the Coastal Management Program?.....	()	(X)
Will a Coastal Use Permit be required?.....	()	(X)
d. Coastal Barrier Island (Grand Isle only).....	(X)	()
e. Farmlands (use form AD 1006 if necessary).....	(X)	()
f. Is project on Sole Source Aquifer?.....	()	(X)
Is coordination with EPA necessary?.....	(X)	()
g. Natural & Scenic Stream Permit required.....	(X)	()
h. Is project impacting a waterway?.....	(X)	()
Has navigability determination been made?.....	(X)	()
Will a US Coast Guard permit or amended permit be required?.....	(X)	()

9. Physical Impacts

	NO	YES
a. Is a noise analysis warranted (Type I project).....	()	(X)
Are there noise impacts based on violation of the (NAC)?.....	()	(X)
Are there noise impacts based on the 10 dBA increase?.....	(X)	()
Are noise abatement measures reasonable and feasible?.....	(X)	()
b. Is an air quality study warranted?.....	(X)	()
Do project level air quality levels exceed the NAAQS for CO?.....	(X)	()
c. Is project in a non-attainment area for Carbon monoxide (CO), Ozone (O ₃), Nitrogen dioxide (NO ₂), or Particulates (PM-10)?	(X)	()
d. Is project in an approved Transportation Plan, Transportation Improvement Program (TIP) and State Transportation Improvement Program (STIP)?.....	()	(X)
e. Are construction air, noise, & water impacts major?.....	(X)	()
f. Are there any known waste sites or U.S.T.s?.....	()	(X)
Will these sites require further investigation prior to purchase?	()	(X)

10. Social Impacts

	NO	YES
a. Land use changes	()	(X)
b. Churches and Schools		
Are any impacted by the project? (If so, list below).....	()	(X)
Are any adjacent to the project? (If so, list below).....	(X)	()
c. Title VI Considerations	(X)	()
d. Will any specific groups be adversely affected (i.e., minorities, low-income, elderly, disabled, etc.)?	(X)	()
e. Hospitals, medical facilities, fire police		
Are any impacted by the project? (If so, list below).....	(X)	()
Are any adjacent to the project? (If so, list below).....	(X)	()
f. Transportation pattern changes	()	(X)
g. Community cohesion	(X)	()
h. Are short-term social/economic impacts due to construction considered major?	(X)	()
i. Do conditions warrant special construction times (i.e., school in session, congestion, tourist season, harvest)?	(X)	()
j. Were Context Sensitive Solutions considered? (If so explain below).....	(X)	()
k. Will the roadway/bridge be closed? (If yes, answer questions below)	(X)	()
Will a detour bridge be provided?.....	(X)	()
Will a detour route be signed?.....	(X)	()

11. Other (Use this space to explain or expand answers to questions above.)

6(a) The US 11 bridge over the Norfolk Southern Railroad was recommended as eligible for the National Register of Historic Places as part of the statewide historic bridge inventory for bridges constructed prior to 1971 and subject to 4(f) evaluation. This determination signified that the adverse effect from the potential demolition of the bridge would require compliance with the Statewide Programmatic Agreement Regarding Management of Historic Bridges in Louisiana (Sept 21, 2015) to demonstrate that the Federal Highway Administration and LADOTD have complied with Section 106 of the National Historic Preservation Act.

6(b) In addition to the US 11 bridge over the Norfolk Southern Railroad that was recommended as eligible for the NRHP, two structures, the Old Slidell Icehouse (52-02387) and a 1920s residence (52-02398) are located along US 11. The Old Ice House has a modern wooden handicap ramp on the west and north sides that will be avoided as a result of right-of-way requirements. The Old Ice structure will not be impacted. The 1920s residence will also be avoided.

6(e) The US 11 bridge over the NS railroad, built in 1937, is constructed with a concrete deck and bents, steel I-beams, and eight spans with two lanes and no shoulders crossing one railroad track.

It was determined eligible for the National Register of Historic Places under Criterion A for its association with important trends or events that have made a significant contribution to the broad patterns of Louisiana history and for its association with federal New Deal-era funding through the U.S. Works Grade Crossing Program.

Steel beam and girder bridges often lack engineering distinction and typically exhibit little aesthetic treatment. This bridge type was built in substantial numbers, typically following standard plans, both nationally and in Louisiana. During the eligibility determination process, no evidence was found to indicate the bridge is an important example of bridge design, engineering, or construction and, therefore, the bridge does not demonstrate significance under Criterion C.

7(c) Wetland impacts are estimated to be less than 1 acre.

8(c) The Preferred Alternative will require a Coastal Use Permit (CUP) in compliance with the Louisiana Coastal Resources Program and for consistency with the federal Coastal Zone Management Act. The

proposed improvements are a use of state concern. The permit process was initiated through the Solicitation of Views process and CUP Permit No. P20130672 has been assigned by the Louisiana Department of Natural Resources, Office of Coastal Management with a copy to the U.S. Army Corps of Engineers (USACE), New Orleans District.

- 9(f) Required right-of-way for lane widening along US 11 and intersection improvements associated with the Preferred Alternative at US 11 north of Lafayette Road is likely to impact the Circle K requiring removal of one pump island, appurtenant piping, and part of the canopy. Additional impacts may occur at US 11 immediately south of Indiana Avenue near the former McMurray filling station. Further investigation may be required during final design and ROW acquisition. Potential, but unlikely, impacts may occur at the Shell station and former Jubilee Express located at the intersection of US 11 and Powell Drive.
- 10(a) To meet roadway widening design criteria, land use changes include some developed and undeveloped residential/commercial lands to transportation use.
- 10(b) Potential impacts to driveway entrances and parking area at Community Baptist Church located on the west side of US 11 north of the existing bridge.
- 10(f) Roadway closures are anticipated at Addis Boulevard and Indiana Avenue with Addis Boulevard neighborhood access connecting through Strawberry Street via a required local roadway improvement. Right-in/right-out travel at US 11 and Indiana Avenue will be eliminated with all traffic utilizing 9th Avenue. Additional transportation pattern changes will be required during bridge construction for the Preferred Alternative. These changes will not require closure or detours.

Preparer: ARCADIS U.S., Inc.
Title: Scott L. Hoffeld, Sr. Project Manager
Date: November 2015

Attachments

- S.O.V. and Responses **Appendix A-1, A-2**
 - Wetlands Finding **Section 3.2; Appendix CD-1**
 - Project Description Sheet **Sections 1, 2, and 3**
 - Conceptual Stage Relocation Plan
 - Noise Analysis **Section 3.7, Appendix CD-3**
 - Air Analysis
 - Exhibits and/or Maps
 - Form AD 1006 (Farmlands)
 - 106 Documentation **Appendix A-3 – Agency Coordination – Section 106**
 - Other
- The Public Meeting Summary is on file with LADOTD and was submitted on September 25, 2014.**

The Permits, Mitigation & Commitments document follows the Environmental Checklist.

Appendix CD-1 – Traffic Analysis Report

Appendix CD-2 – Biological Resources and Wetland Findings Report

Appendix CD-3 – Noise Analysis Technical Report

Appendix CD-4 – Phase I Environmental Site Assessment Report



SUMMARY

Permits, Mitigation & Commitments

The Preferred Alternative would involve the preparation and submittal of several federal and state permits as well as mitigation requirements.

◆ LDNR Coastal Use Permit	◆ Traffic Control	◆ Relocation
◆ USACE Wetland Permit	◆ Bicycle Facilities	◆ Property Access
◆ Section 401 Water Quality	◆ LDEQ NPDES for Construction-Related Activities	

U.S. Army Corps of Engineers Permit

A Jurisdictional Determination by the U.S. Army Corps of Engineers (USACE), New Orleans District is required.

A programmatic general permit is required to satisfy Section 404 of the Clean Water Act. This permit is required for temporary and permanent construction-related impacts to wetlands and other waters of the U.S. determined to be jurisdictional. This permit will be jointly applied for in conjunction with a Coastal Use Permit (CUP). The joint permit process was initiated as part of the Solicitation of Views (SOV). Coordination with the Louisiana Department of Natural Resources (LDNR), Office of Coastal Management is required. All correspondence or inquiries shall reference CUP No. P20130672.

In order to comply with the federal policy of ensuring that there is no net loss of wetlands acres, unavoidable wetlands impacts along the project would be compensated according to an approved mitigation plan as part of the wetland permitting process.

Section 401 Water Quality Certification

A Section 401 Water Quality Certification is required in conjunction with the Section 404 permit per Louisiana’s Water Quality Regulations (Louisiana Administrative Code 33:IX Chapter 15).

This certification would be coordinated with the Louisiana Department of Environmental Quality (LDEQ).

Louisiana Department of Natural Resources, Coastal Use Permit

The Preferred Alternative will require a CUP in compliance with the Louisiana Coastal Resources Program and for consistency with the federal Coastal Zone Management Act. The proposed improvements are a use of state concern. The permit process was initiated through the SOV process and CUP No. P20130672 has been assigned by LDNR, Office of Coastal Management, with a copy to the USACE, New Orleans District. This permit will be jointly applied for in conjunction with a USACE Section 404 permit.

Louisiana Pollutant Discharge Elimination System (LPDES) Permit and Storm Water Pollution Prevention Plan (SWPPP)

Short-term impacts associated with construction of the Preferred Alternative are anticipated including erosion of areas cleared for construction, temporary increases in noise levels, and fugitive dust from use of heavy construction equipment. Temporary impacts to traffic flow and travel patterns are anticipated with construction of the Preferred Alternative.

ENVIRONMENTAL ASSESSMENT

Adverse construction impacts to water quality would be reduced by implementation of Best Management Practices as outlined in a project-specific SWPPP and Erosion and Sedimentation Control Plan. Measures to reduce erosion and nonpoint source pollution from runoff into surface waters (e.g., properly store materials and equipment, properly store and dispose of waste materials, maintain equipment, and avoid accidental discharges of fuels or other chemicals) will be outlined in the SWPPP. The Preferred Alternative would require an LPDES Notice of Intent (NOI) for construction-related activities. The SWPPP will be required to be prepared and kept at the construction site in addition to the LPDES NOI application. LDEQ monitors these practices through its Water Quality Certification program, which is integrated into the Section 404 process.

Commercial and Residential Relocations

Commercial and residential relocations associated with the Preferred Alternative will be addressed through the Uniform Relocation Act of 1970. Measures to reduce relocation impacts will be incorporated during the design stage.

Traffic Control

Construction-related traffic delays will be minimized through signing plans that inform the drivers of work zones, road closures, detours, and other temporary changes. All traffic maintenance plans will be prepared by qualified traffic engineers in accordance with Louisiana Department of Transportation and Development (LADOTD) standards and will be monitored for effectiveness throughout the construction process.

Bicycle and Pedestrian Facilities

Currently, the City of Slidell and St. Tammany Parish Master Plans do not include bicycle and pedestrian improvements along the Study Area. However, understanding that future planning within the City of Slidell and St. Tammany Parish

may include bicycle and pedestrian improvements along or near the Study Area, the roadway sections will allow for incorporation of a shared use path on the east and west sides of US 11 north of the bridge and along the west side of US 11 south of the bridge. The shared use path will provide an opportunity for future local bicycle and/or pedestrian linkages to Heritage Park and the Tammany Trace. As a safety measure, pedestrian access across the bridge is not provided.

Eight foot paved shoulders are provided in the bridge typical section to accommodate bicycles. Less experienced riders can utilize the local roadway network west of US 11 and south of the overpass to connect to the proposed shared use path that begins at the north end of Addis Boulevard.

Bicycle and pedestrian improvements for the proposed project have been evaluated in accordance with the LADOTD Complete Streets Policy and in coordination with the City of Slidell and St. Tammany Parish.

Historic and 4(f) Resources

As part of the Section 106 process, documentation was submitted to the State Historic Preservation Office (SHPO) who concurred that the existing US 11 bridge over the Norfolk Southern railroad is eligible for the National Register of Historic Places (NRHP). This determination signified that the adverse effect from the potential demolition of the bridge would require compliance with the statewide *Programmatic Agreement Regarding Management of Historic Bridges in Louisiana* (Sept 21, 2015). Compliance with this programmatic agreement, along with marketing the bridge, will be completed in order to demonstrate that the Federal Highway Administration (FHWA) and LADOTD have complied with Section 106 of the National Historic Preservation Act (NHPA).

Requirements to assess whether all possible planning has been considered and whether there is a feasible and prudent avoidance alternative is specified in the Programmatic Section 4(f) Evaluation and Approval for the US 11 bridge. The proposed project meets all the applicability criteria specified in the programmatic agreement. Prior to demolition of the US 11 bridge, the LADOTD shall contact the SHPO to determine the appropriate form of documentation and the appropriate state or local depository for the documentation. Unless otherwise agreed to by the SHPO, the FHWA shall ensure that all documentation is completed and accepted by the SHPO prior to the relocation or demolition of the bridge.

Hazardous Materials Sites, Underground Storage Tanks

No HRECs were identified but several RECs were identified within the Study Area. Site No. 1, Jubilee Express Store #4815, and Site No. 9, Circle K #2896, are located within the Study Area. Site No. 8, Western International Gas & Cylinder, is located adjacent to the Study Area.

Required ROW for lane widening along, and intersection improvements associated with, the Preferred Alternative may impact Site Nos. 1, 7, and 9. It is anticipated that UST facilities at Site Nos. 1 and 9 will not be impacted by required ROW although appurtenant piping may be impacted. Due to limited historic information regarding Site No. 7, direct impacts are not known. Caution should be taken during construction-related activities.

Property Access

Access will be maintained to properties and all residences and businesses adjacent to the project during and following construction.

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LIST OF ACRONYMS

AASHTO	American Association of State Highway and Transportation Officials
ACHP	Advisory Council on Historic Preservation
ADT	Average Daily Traffic
APE	Area of Potential Effect
CAAA	Clean Air Act Amendments
CERCLA	Comprehensive Environmental Response, Compensation, and Liability Act
CESQG	Conditionally Exempt Small Quantity Generator
CFR	Code of Federal Regulations
CUP	Coastal Use Permit
CZMA	Coastal Zone Management Area
dBA	A-Weighted Decibel
DHP	Division of Historic Preservation
DOA	Division of Archaeology
DOT	U.S. Department of Transportation
EA	Environmental Assessment
EDMS	Electronic Document Management System
EDR	Environmental Data Resources
ERNS	Emergency Response Notification System
ESI	Earth Search, Inc.
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
FINDS	Facility Index Systems/Facility Registration System
FIRM	Flood Insurance Rate Maps
FONSI	Finding of No Significant Impact
FPPA	Farmland Protection Policy Act
GIS	Geographic Information System
HAER	Historic American Engineering Record
HREC	Historical Recognized Environmental Condition
I-10/12	Interstate 10/12
LADOTD	Louisiana Department of Transportation and Development
LCRP	Louisiana Coastal Resources Program
LDCRT	Louisiana Department of Culture, Recreation & Tourism
LDEQ	Louisiana Department of Environmental Quality
LDNR	Louisiana Department of Natural Resources

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LEP	Limited English Proficiency
LHRI	Louisiana Historic Resource Inventory
LNHP	Louisiana Natural Heritage Program
LOS	Level of Service
LPDES	Louisiana Pollutant Discharge Elimination System
LUST	Leaking Underground Storage Tank
MOA	Memorandum of Agreement
mph	Miles per hour
MPO	Metropolitan Planning Organization
MTP	Metropolitan Transportation Plan
NAAQS	National Ambient Air Quality Standards
NAC	Noise Abatement Criteria
NEPA	National Environmental Policy Act
NHPA	National Historic Preservation Act
NOI	Notice of Intent
NPDES	National Pollutant Discharge Elimination System
NRCS	Natural Resources Conservation Service
NRHP	National Register of Historic Places
NS	Norfolk Southern
RCRA	Resource Conservation and Recovery Act
REC	Recognized Environmental Condition
ROW	Right-of-Way
RPC	New Orleans Regional Planning Commission
SELA	Southeast Louisiana
SHPO	State Historic Preservation Office/Officer
SOV	Solicitation of Views
SPILLS	Emergency Response Section Incidents
SQG	Small Quantity Generator
SWPPP	Storm Water Pollution Prevention Plan
TDM	Travel Demand Model
TIP	Transportation Improvement Plan
TNM	Traffic Noise Model
UA	Urban Arterial Design Criteria
US 11/190	U.S. Highway 11/190
US Hist Auto	Historical Gas Station
USACE	U.S. Army Corps of Engineers
USDA	U.S. Department of Agriculture
USEPA	U.S. Environmental Protection Agency
USFWS	U.S. Fish and Wildlife Service
USGS	U.S. Geological Survey
UST	Underground Storage Tank

VOC Volatile Organic Compound

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SECTION 1

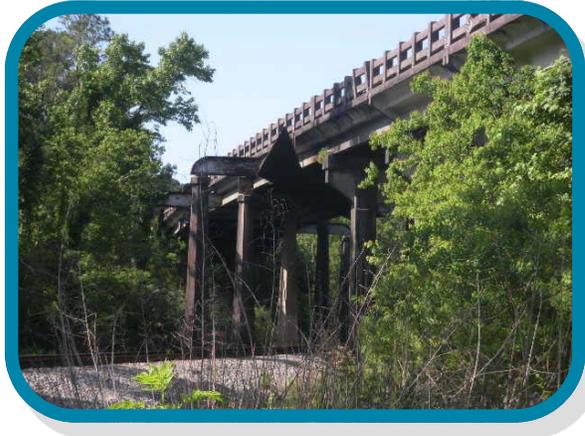
Proposed Project

*In This Section....*The Louisiana Department of Transportation and Development and the Federal Highway Administration, in cooperation with the New Orleans Regional Planning Commission, propose to replace the US 11 bridge over the Norfolk Southern Railroad and widen US 11 from two lanes to four lanes from US 190 (Gause Boulevard) north to Interstate 12 in Slidell, St. Tammany Parish, Louisiana.

The Study Area is located in the southeastern portion of St. Tammany Parish and extends south along US 11 from Powell Drive to the intersection of US 11 at Florida Avenue, in Slidell, a distance of approximately 1.7 miles.

1.1 Description of the Proposed Project

The Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA), in cooperation with the New Orleans Regional Planning Commission (RPC), propose to replace the US Highway 11 (US 11) bridge over the Norfolk Southern (NS) railroad and widen US 11 from two lanes to four lanes from US 190 (Gause Boulevard; US 190) north to Interstate 12 (I-12) in Slidell, St. Tammany Parish, Louisiana. The RPC is the designated Metropolitan Planning Organization (MPO) for the New Orleans urbanized area including St. Tammany Parish.



US 11 Bridge over the Norfolk Southern railroad Slidell, Louisiana.

The proposed action is identified as a Tier II – On System Unfunded Project for fiscal year 2015 – 2024 in the *Metropolitan Transportation Plan*,

St. Tammany Urbanized Areas, Fiscal Years 2011 – 2040 (MTP November 2010) and in the Transportation Improvement Plan (TIP) for the parish. The proposed project was included as a financially constrained priority project in the *Transportation Improvement Plan, St. Tammany Urbanized Areas, Fiscal Years 2012 – 2016* (March 2012).

The study of the alternatives developed in this environmental assessment (EA) and the associated environmental consequences were evaluated according to the National Environmental Policy Act (NEPA); LADOTD’s Stage 1 Planning/Environmental Manual of Standard Practice; and FHWA Guidance for Preparing and Processing Environmental and Section 4(f) Documents.

1.2 Study Area

The Study Area is located in the southeastern portion of St. Tammany Parish and extends south along US 11 from Powell Drive to the intersection of US 11 at Florida Avenue, a distance of approximately 1.7 miles. The Study Area is located to the east of Airport Road and

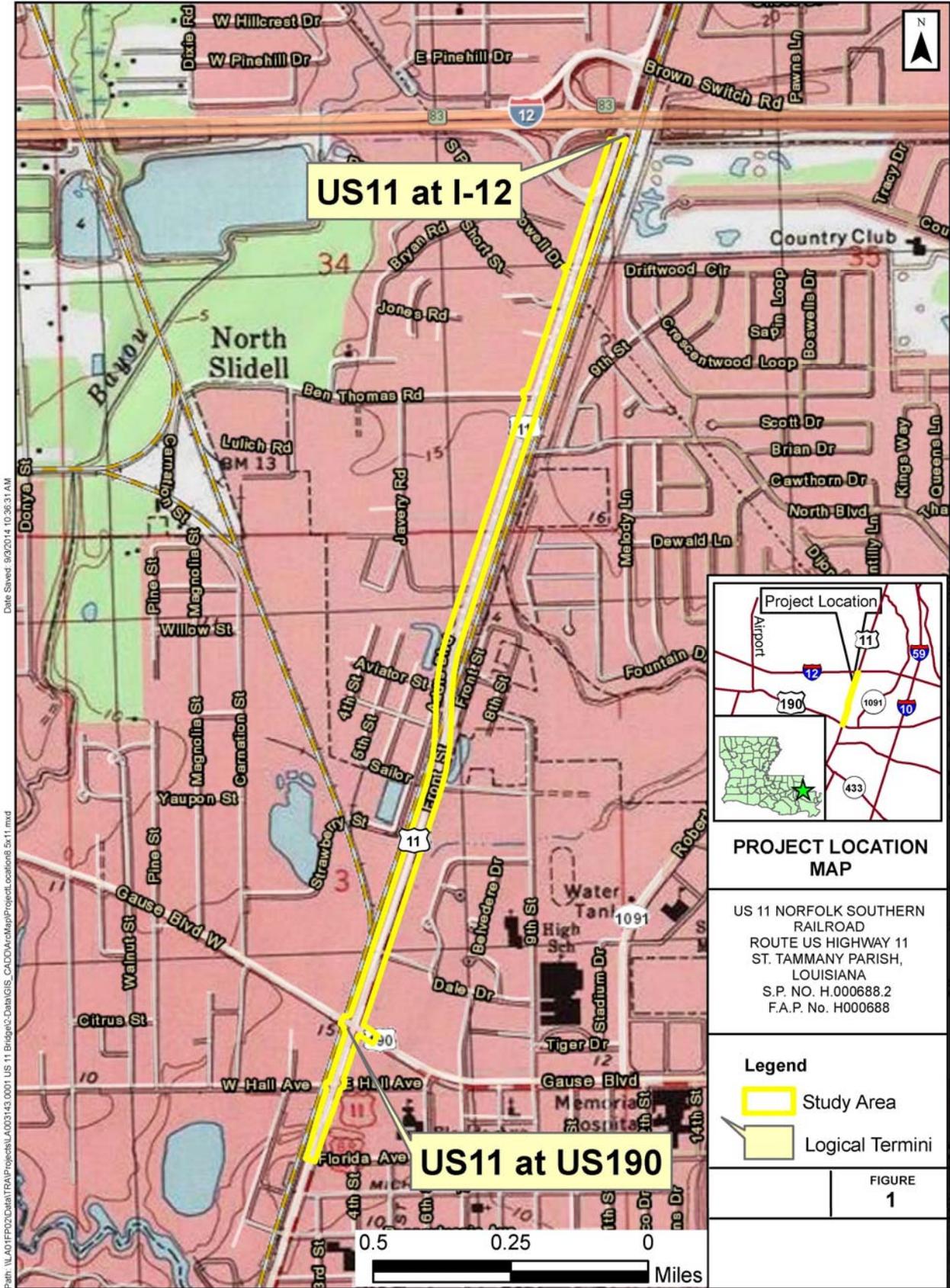
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west of Interstate 10. North Slidell is located to the north/northwest and Lake Pontchartrain to the south. The Study Area includes the logical termini and the area that may be impacted by the indirect and cumulative impacts from the proposed project (Figure 1).

1.3 Logical Termini

The FHWA defines logical termini for project development as (1) rational end points for a transportation improvement, and (2) rational end points for a review of the environmental impacts. The environmental impact review frequently covers a broader geographic area than the strict limits of the transportation improvements. In the past, the most common termini have been points of major traffic generation, especially intersecting roadways. This is due to the fact that in most cases traffic generators determine the size and type of facility being proposed. Choosing a corridor of sufficient length to look at all impacts need not preclude staged construction.

The logical termini identified on Figure 1 are a result of previous studies completed for the project and surrounding areas, completed improvements, and identified future improvements. The logical termini for the proposed project are US 11 at I-12 and US 11 at US 190. Widening improvements were recently completed at US 11 and I-12 and continue south along US 11 to just north of Powell Drive.





SECTION 2

Purpose and Need

In This Section..... The purpose of the proposed project is to replace the existing structurally deficient and functionally obsolete bridge crossing the Norfolk Southern railroad and widen US 11 from two lanes to four lanes from US 190 north to Powell Drive. The project is needed in order to upgrade the bridge and roadway segment to current design standards for travel lane and shoulder widths, bridge width, stopping sight distances, design speed, and structural capacity. The proposed roadway segment and bridge improvements will also provide better accommodations for bicycle users and will serve the surrounding community and the larger metropolitan area.

2.1 Introduction

The City of Slidell and St. Tammany Parish are located in southeast Louisiana approximately 35 miles northeast of New Orleans and the Mississippi River, 6 miles north of Lake Pontchartrain, and 6 miles west of the state of Mississippi. The City of Slidell is located within the New Orleans urbanized area which includes the city of New Orleans and surrounding suburban areas located in Jefferson, Orleans, Plaquemines, St. Bernard, and St. Tammany Parishes. Due to the proximity of the Gulf of Mexico, Mississippi River, and the Gulf Intracoastal Waterway, this urbanized area has developed into a major port for the region and country. As a result, the New Orleans region supports an extensive multimodal transportation system and is the origination point for six Class I railroads. The region has also experienced an increase in the energy, tourism, and healthcare industries.

As of the 2010 U.S. Census, the greater New Orleans region had a total estimated population of 1,172,734. The City of Slidell had a population of 27,197 and St. Tammany Parish had 233,740. Between 2000 and 2010, the greater New Orleans region experienced a decline in population of 143,766 while St. Tammany Parish experienced an estimated increase of 42,482. The shift in population is due, in part, to the impacts and lingering effects sustained as a result of

Hurricane Katrina in 2005 and has led to more complex travel patterns and lengthier trips.

The LADOTD proposes to replace the existing bridge crossing the NS railroad and widen US 11 from two lanes to four lanes from US 190 north to Powell Drive in Slidell, St. Tammany Parish.

2.2 Project Purpose

The purpose of the project is to address safety features of the bridge crossing the NS railroad and system reliability of the roadway segment from US 190 north to Powell Drive.

2.3 Project Need

The project is needed in order to upgrade the bridge and roadway segment to current design



Aerial view of the US 11 bridge over the Norfolk Southern railroad, Slidell, Louisiana.

Source: ©2014 Google Maps

standards for travel lane and shoulder widths, bridge width, stopping sight distances, and design speed, as well as system reliability. Roadway segment and bridge improvements will also provide better accommodations for bicycle users and will serve the surrounding community and the larger metropolitan area. More specifically, needs for the proposed project include:

- Address the safety features of the US 11 bridge crossing over the NS railroad;
- Improve capacity;
- Improve travel time;
- Relieve congestion on US 11 by removing the bottleneck between US 190 and I-12;
- Relieve future congestion on area roadways; and
- Improve area-wide mobility and system reliability.

2.3.1 Bridge Replacement

The US 11 bridge that crosses the NS railroad was built in 1937 to bridge and roadway standards in place at that time. Even though the US 11 Bridge over the Norfolk Southern railroad is eligible for inclusion on the National Register of Historic Places (NHRP), it must perform as an integral part of the modern transportation system. When unable to maintain system continuity and integrity, the result is bridge rehabilitation or replacement. When a proposed action will “use” a bridge that is eligible for inclusion on the NHRP and the historic integrity of the bridge will be impaired by demolition a programmatic Section 4(f) evaluation is completed.

Constructed with a concrete deck and steel I-beams, the bridge spans 828 feet. The substructure is composed of concrete piers and bents, and the approaches are constructed on embankment. Because the bridge has two 12-foot travel lanes, has no shoulders, and is undivided, it is rated as functionally obsolete (Appendix A). The classification of functional obsolescence does not mean that the bridge is inherently unsafe. It is a

term that identifies a bridge as one that does not perform adequately for its current use, which indicates that measures should be taken to improve functionality. Functional obsolescence is also a term that assigns priority status for federal funding for bridge replacement and rehabilitation.

A further indication of the inadequacy of the US 11 bridge to remain in service is its bridge sufficiency rating. This rating takes many factors into account including structural adequacy and safety, serviceability, functional obsolescence, and essentiality for public use. A sufficiency rating of 80 or below qualifies the bridge for rehabilitation funding. A sufficiency rating of 50 or below qualifies it for replacement funding (FHWA 2006). The most recent bridge sufficiency rating for the US 11 bridge is 16.4, a clear signal that the bridge is ready for replacement.

The most recent LADOTD Bridge Inspection Report prepared in March 2010 provides an evaluation of the superstructure and substructure according to National Bridge Inspection condition ratings. The general condition ratings are an overall assessment of the physical condition of the deck, superstructure, substructure, and culverts and range from 0 (failed condition) to 9 (excellent condition). The superstructure includes the load-carrying members such as beams or girders that support the driving surface (deck). The substructure includes abutments, piers, and bents.

The bridge inspection shows that the bridge is structurally deficient. The inspection recorded an overall rating of 4, which indicates poor condition with advanced section loss, deterioration, or spalling (concrete pitting). A rating of 4 was recorded for the superstructure and substructure, and a rating of 5 was recorded for the deck because it was recently resurfaced. A bridge is classified as “structurally deficient” with a general condition rating of 4 or less for the deck, superstructure, and substructure.

The US 11 bridge was built to standards that no longer meet minimum American Association of State Highway and Transportation Officials (AASHTO) and LADOTD guidelines. In order to

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determine geometric adequacy, consideration must be given to the number of travel lanes, roadway width, approach roadway, vertical and horizontal clearances, sight distances, and functional classifications of the roadway. Because historic bridges are often geometrically inadequate, the functional classification of the existing bridge and average daily traffic (ADT) count are utilized to evaluate geometric adequacy. The existing bridge is classified as urban arterial. For 2013, the ADT for the project length was 19,381 with a projected ADT of 26,475 for design year 2042, based on a 1.0 percent annual growth rate. Truck traffic in 2042 is projected to be 6.5 percent of the ADT for 2013.

The roadway classification also determines which standards will be applied to the new design. Current standards and guidelines for this functional classification require, at a minimum, 12-foot travel lanes and 8-foot shoulders, as well as adequate sight distances. The recommended design speed for the roadway classification is 45 miles per hour (mph). The existing bridge guardrails are concrete posts and rails. Additional guardrails were installed to the interior of the existing guardrail as part of the recent overlay project. The vertical clearance of the existing bridge cannot accommodate widening and meet NS's vertical and horizontal clearance requirements.

2.3.2 Roadway Widening

In 1930, the population of St. Tammany Parish was 23,624, growing to 233,540 in 2010. Likewise, the City of Slidell grew from a station on the New Orleans and Northeastern

Railroad to a city of 27,068 persons today. The New Orleans urbanized area, which includes Slidell and St. Tammany Parish, had a total population of approximately 1,316,500 in 2000. St. Tammany Parish experienced dramatic population growth between 1990 and 2000, growing by nearly 47,000. Growth between 2000 and 2010 increased by 42,482 in St. Tammany Parish while Jefferson, Plaquemines, and St. Bernard Parishes all experienced population declines. The City of Slidell also experienced a population increase of 1,373 during this time period. Population projections estimate growth within St. Tammany Parish to increase 51 percent by 2030 to a total population of 459,160.

The Study Area is comprised of five signalized intersections and four un-signalized intersections. Existing year traffic volumes were determined through data obtained from LADOTD and traffic count data collected in May 2013. The RPC is responsible for long- and short-range roadway and transportation plans for the New Orleans urbanized area including St. Tammany Parish and the City of Slidell. The RPC maintains a regional travel demand model (TDM) to forecast traffic conditions. The TDM was utilized to evaluate opening-year (2021) and design-year (2042) traffic volumes for the No Build alternatives.

Traffic volumes are projected to increase along US 11 as shown in Table 1. Travel demand projections for the opening-year and design-year projections applied an estimated annual growth rate of 1 percent and include impacts from proposed developments in the Study Area.



Aerial view looking north along US 11, north of Lafayette Street at transition from recent roadway widening back to two lanes.

Source: Google Maps ©2014 Google

Table 1: Average Daily Traffic Volumes

Location US 11 Between:	2013 Existing	2021 No-Build	2042 No-Build
I-12 Westbound and Eastbound Ramps	22,090	24,980	30,480
1-12 Eastbound Ramps and Powell Drive	21,000	24,340	29,650
Powell Drive and Ben Thomas Road	20,850	23,850	29,090
Ben Thomas Road and North Boulevard	20,470	23,120	28,290
North Boulevard and Addis Boulevard	18,000	20,270	24,780
Addis Boulevard and Indiana Avenue	17,890	20,140	24,600
Indiana Avenue and Lafayette Street	17,810	20,050	24,530
Lafayette Street and Gause Boulevard	16,940	18,980	23,210
Gause Boulevard and Hall Avenue	17,420	19,230	23,650
Hall Avenue and Florida Avenue	21,710	23,850	29,310

Safety is not included in the measures that establish service levels. LADOTD design standards specify an acceptable level of service (LOS) based on roadway classifications. Because of its urban location, LOS C is acceptable for proposed improvements along US 11 although LOS D is allowable in heavily developed urban areas.

LOS analysis locations included signalized and un-signalized intersections, eastbound and westbound interstate ramps, and two-lane and four-lane roadway segments. Capacity analyses were performed for a.m. and p.m. peak periods for existing, open year, and design year No Build conditions. The capacity analysis results for existing conditions provided in Table 2 indicate that most intersections perform well with LOS C or better. However, the eastbound and westbound approaches of the signalized intersection of US 11 at the I-12 westbound ramps currently operate at LOS E for the a.m. and p.m. peak periods. In addition, the intersection average is LOS D and shows the highest delay for all peak periods analyzed.

The capacity analysis results for design year No Build conditions presented in Table 3 indicate that several intersection approaches will fail (LOS F) without capacity improvements. By the design year, the critical approaches at all of the un-signalized intersections, except for the intersection of US 11 at Indiana Avenue, would fail to operate (LOS F) due to insufficient capacity to accommodate design year traffic volumes. The signalized intersections of US 11 at the I-12 westbound ramps and US 11 at US 190 (Gause Boulevard) are expected to operate at LOS E during the p.m. peak hour. In addition, several of the signalized intersection approaches are projected to fail (LOS F) during design year peak periods.

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Table 2: LOS Results for Existing (2013) Conditions

Intersection with US 11	S/U	EB		WB		NB		SB		Intersection Avg		
		Delay (sec)	LOS	v/c	Delay (sec)	LOS						
AM Peak Period												
I-12 WB Ramps	S	55.8	E	56.5	E	44.1	D	37.5	D	0.65	47.0	D
I-12 EB Ramps	S	24.4	C	-	-	12.6	B	17.3	B	0.46	18.4	B
Powell Drive	U	32.5	C	-	-							
Ben Thomas Road	U	19.6	C	-	-							
North Boulevard	S	-	-	31.2	C	30.5	C	13.7	B	0.57	22.4	C
Addis Boulevard	U	23.5	C	-	-							
Indiana Avenue	U	-	-	15.8	C							
Lafayette Street	S	39.5	D	41.7	D	4.3	A	4.9	A	0.31	7.2	A
US 190 (Gause Blvd)	S	25.6	C	24.1	C	25.6	C	34.1	C	0.63	28.4	C
Hall Avenue	S	38.9	C	35.9	D	11.1	B	11.0	B	0.49	16.6	B
Noon Peak Period												
I-12 WB Ramps	S	45.0	D	45.4	D	31.9	C	30.3	C	0.53	35.7	D
I-12 EB Ramps	S	22.1	C	-	-	9.8	A	6.7	A	0.33	20.0	B
Powell Drive	U	15.7	C	-	-							
Ben Thomas Road	U	19.7	C	-	-							
North Boulevard	S	-	-	44.4	D	8.1	A	3.0	A	0.45	8.9	A
Addis Boulevard	U	17.0	C	-	-							
Indiana Avenue	U	-	-	11.7	B							
Lafayette Street	S	50.5	D	49.9	D	5.4	A	4.8	A	0.27	10.6	B
US 190 (Gause Blvd)	S	26.9	C	26.9	C	36.8	D	37.3	D	0.65	31.7	C
Hall Avenue	S	33.4	C	42.0	D	12.0	B	13.0	B	0.47	18.9	B
PM Peak Period												
I-12 WB Ramps	S	58.9	E	56.0	E	51.6	D	35.7	D	0.7	50.4	D
I-12 EB Ramps	S	30.6	C	-	-	13.6	B	6.9	A	0.49	18.4	B
Powell Drive	U	21.6	C	-	-							
Ben Thomas Road	U	33.2	D	-	-							
North Boulevard	S	-	-	52.2	D	14.8	B	10.0	B	0.66	17.6	B
Addis Boulevard	U	31.0	D	-	-							
Indiana Avenue	U	-	-	15.1	C							
Lafayette Street	S	51.1	D	49.1	D	5.9	A	6.1	A	0.34	12.6	B
US 190 (Gause Blvd)	S	29.5	C	32.6	C	33.6	C	42.8	D	0.73	34.3	C
Hall Avenue	S	32.5	C	43.4	D	13.6	B	16.8	B	0.66	19.9	B

- = No approach. EB = Eastbound. SB = Southbound. WB = Westbound.
 = Not applicable. LOS = Level of service. sec = Second.
 = LOS E. NB = Northbound. U = Un-signalized.
S = Signalized. v/c = Volume/capacity.

Table 3: LOS Results for Design Year (2042) No Build Conditions

Intersection with US 11	S/U	EB		WB		NB		SB		Intersection Avg		
		Delay (sec)	LOS	v/c	Delay (sec)	LOS						
AM Peak Period												
I-12 WB Ramps	S	66.3	E	98.0	F	56.0	E	46.8	D	0.87	64.9	E
I-12 EB Ramps	S	30.4	C	-	-	14.3	B	21.7	C	0.64	22.6	C
Powell Drive	U	59.0	F	-	-							
Ben Thomas Road	U	41.4	E	-	-							
North Boulevard	S	-	-	44.3	D	25.9	C	15.0	B	0.78	24.1	C
Addis Boulevard	U	49.5	E	-	-							
Indiana Avenue	U	-	-	23.0	C							
Lafayette Street	S	39.1	D	42.3	D	4.9	A	6.2	A	0.43	8.1	A
US 190 (Gause Blvd)	S	36.2	C	30.8	C	45.6	D	82.3	F	0.95	49.9	D
Hall Avenue	S	24.1	C	31.1	C	19.8	B	18.5	B	0.75	21.1	C
Noon Peak Period												
I-12 WB Ramps	S	50.2	D	54.5	D	38.7	D	36.9	D	0.72	42.6	D
I-12 EB Ramps	S	41.4	D	-	-	10.2	B	8.4	A	0.44	20.8	C
Powell Drive	U	35.8	E	-	-							
Ben Thomas Road	U	57.0	F	-	-							
North Boulevard	S	-	-	43.1	D	12.0	B	5.4	A	0.62	11.6	B
Addis Boulevard	U	27.8	D	-	-							
Indiana Avenue	U	-	-	13.8	B							
Lafayette Street	S	53.1	D	48.1	D	7.0	A	6.5	A	0.38	12.2	B
US 190 (Gause Blvd)	S	35.6	D	42.9	D	64.8	E	49.2	D	0.96	47.9	D
Hall Avenue	S	30.9	D	54.8	D	16.2	B	16.8	B	0.68	23.2	C
PM Peak Period												
I-12 WB Ramps	S	86.8	F	83.8	F	85.2	F	40.4	D	0.94	76.5	E
I-12 EB Ramps	S	95.5	F	-	-	15.3	B	8.1	A	0.67	43.0	D
Powell Drive	U	139.1	F	-	-							
Ben Thomas Road	U	391.5	F	-	-							
North Boulevard	S	-	-	56.3	E	79.5	E	28.5	C	1.02	53.2	D*
Addis Boulevard	U	92.0	F	-	-							
Indiana Avenue	U	-	-	20.7	C							
Lafayette Street	S	69.0	E	59.0	E	7.2	A	8.4	A	0.49	16.4	B
US 190 (Gause Blvd)	S	40.4	D	55.6	E	50.3	D	136.0	F	1	68.5	E
Hall Avenue	S	31.6	C	69.5	E	24.1	C	21.0	C	0.98	29.0	C

- = No approach. EB = Eastbound. SB = Southbound. WB = Westbound.
 = Not applicable. LOS = Level of service. sec = Second.
 = LOS E. NB = Northbound. U = Un-signalized.
 = LOS F. S = Signalized. v/c = Volume/capacity.

Note: Intersections with v/c greater than 1.0 are considered LOS F per Highway Capacity Manual 2010.



SECTION 3

Alternatives

In This Section..... NEPA directs federal agencies to conduct environmental reviews to consider the potential impacts from proposed federal undertakings. The study of alternatives and the associated environmental consequences were evaluated according to NEPA and consists of three primary tasks:



3.1 Introduction

NEPA directs federal agencies to conduct environmental reviews to consider the potential impacts from proposed federal undertakings. The NEPA process requires coordination with local, state, and federal agencies and stakeholders throughout planning and project development decision-making.

FHWA and LADOTD are committed to the practicable avoidance and minimization of potential impacts to the social and natural environment when considering approval of proposed transportation projects. NEPA project development must consider a range of alternatives that would serve the purpose of the project while balancing the impacts and benefits of the project.

The alternatives and the associated environmental consequences were evaluated according to NEPA, LADOTD’s Stage 1 Planning/Environmental Manual of Standard Practice, and FHWA’s Guidance for Preparing and Processing Environmental and Section 4(f) Documents.

This study consists of three primary tasks (Figure 2):

- Scoping & Purpose and Need;
- Alignment Studies and Development; and
- EA Documentation with FONSI.

This study process satisfies regulatory and coordination requirements as directed by NEPA allowing for coordination during the alternatives development process and thorough consideration of alternatives developed.

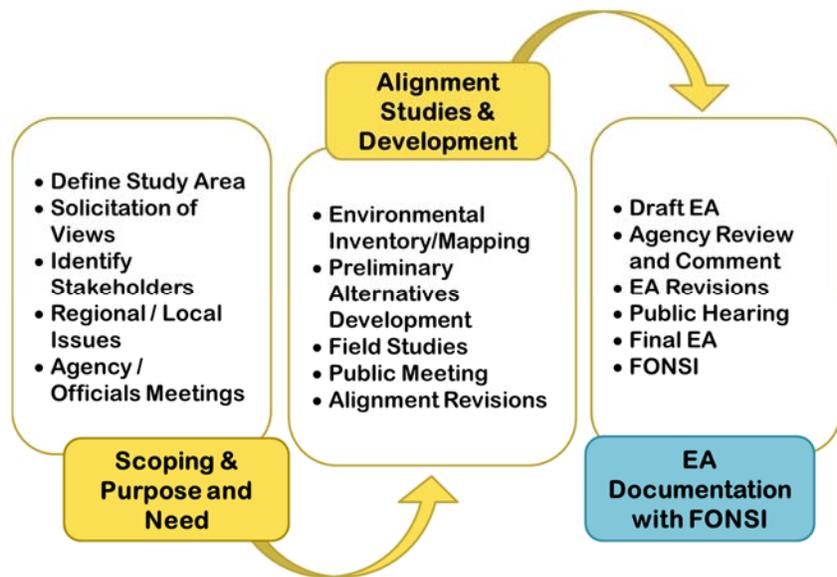


Figure 2: The NEPA/LADOTD Stage 1 Study Process

3.2 Design Criteria

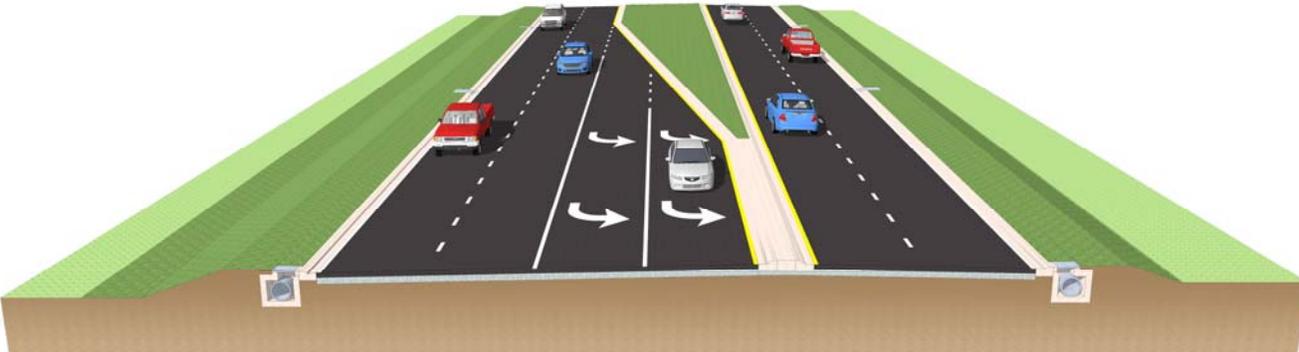
The proposed project includes replacement of the bridge over the NS railroad designed to LADOTD Bridge Design Standards and widening of the roadway from a two-lane undivided roadway to a four-lane divided roadway designed

to LADOTD urban arterial design criteria (UA-2). The UA-2 design criteria, bridge design criteria, and typical roadway sections for the bridge replacement and roadway sections are in Appendix B. A graphic representation of the roadway and bridge sections is shown in Figures 3 and 4.



Not to Scale

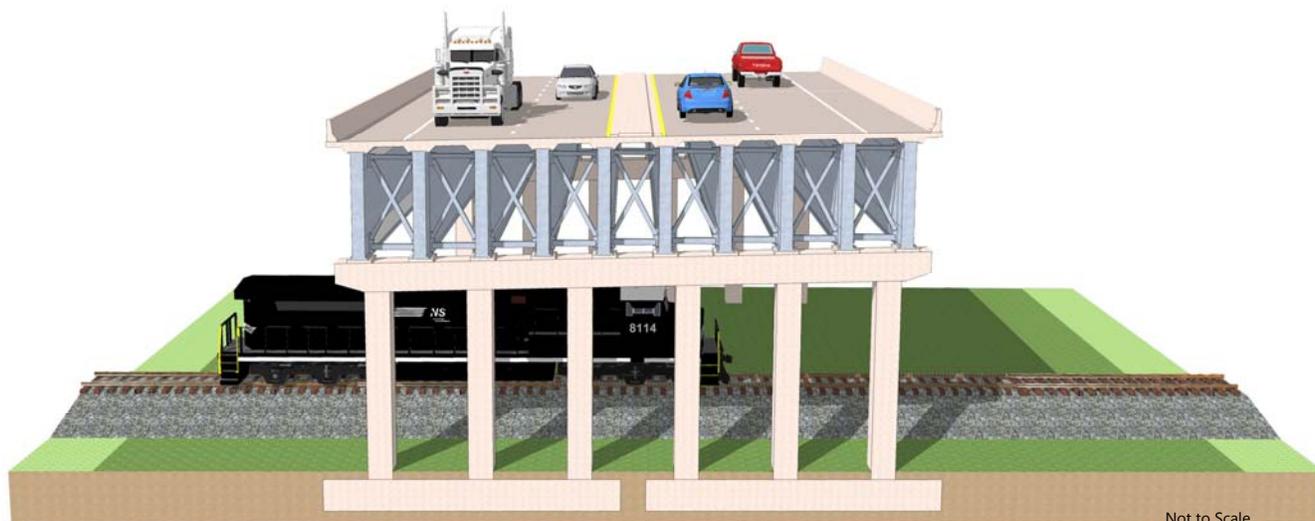
Typical Roadway Section, Urban Arterial - 2 (UA-2) Representation of roadway section for the roadway segment north of the of Norfolk Southern Overpass to Powell Drive (left turn lanes where required). Proposed underground storm drainage on east and west sides of US 11 with connecting yard drains on the west side.



Not to Scale

Typical Roadway Section, Urban Arterial - 2 (UA-2) Representation of roadway section for the roadway segment south of the of Norfolk Southern Overpass to Hall Avenue (left turn lanes where required). Proposed underground storm drainage on east and west sides of US 11.

FIGURE 3



Typical Bridge Section, Norfolk Southern Railroad Overpass.

FIGURE 4

3.3 GIS Environmental Inventory

An environmental inventory of existing social, natural, and cultural resource (secondary-source) data was collected within the Study Area. This information was supplemented with field-collected (primary-source) data for the Study Area and proposed alternatives. A Geographical Information System (GIS) was developed for the project and utilized to map and analyze the human, natural, and cultural resources and the proposed preliminary alternatives.

3.4 Alternatives Development

3.4.1 Stage 0 Alternatives

The Study Area was initially evaluated in a Stage 0 Feasibility Study completed for the RPC and is available at the RPC and LADOTD. The *US 11 Corridor Improvements Stage 0 Feasibility Study* (Krebs et al. 2008) developed a preliminary purpose and need statement, initial project concepts to address the needs, and potential alternatives to the initial concepts. Two

alternatives were identified in the Stage 0 study:

- Widening of US 11 from I-12 south to US 190 and replacement of the bridge crossing the NS railroad; and
- Widening of US 11 from I-12 south to US 190, removal of the bridge crossing the NS railroad, and replacement with an at-grade crossing of the NS railroad.

Early coordination with federal, state, and local agencies solicited comments and responses that were combined with available environmental data. This information was used to help determine if the preliminary alternatives impact certain human, natural, or cultural resources that would result in the decision to dismiss an alternative from further evaluation. This coordination resulted in the dismissal of an at-grade crossing as a viable alternative.

3.4.2 Preliminary Alternatives

To minimize impacts and reduce the amount of right-of-way (ROW) to be acquired, alignments

were located as close to the existing US 11 roadway as design standards and construction limits would allow. LADOTD policies such as roadway and bridge design, intersection configuration, traffic, noise, and minimization of social and environmental impacts were also considered in the alternatives development. Requirements for horizontal and vertical clearances for the railroad crossing were evaluated and cost and constructability factors were also used. The roadway segments north and south of the bridge follow the existing roadway alignment providing widening and improvements to accommodate a four-lane divided highway. This resulted in one alternative for the roadway segments, identified as Alternative 1. Additional bridge and intersection configurations were also developed.

Two alternatives were considered for replacement of the existing two-lane bridge with a four-lane bridge. The existing bridge crossing angle and NS's overhead grade separation design requirements dictated vertical and horizontal clearance requirements for bridge replacement options. Two bridge alignment alternatives were developed: Bridge 1 follows the existing angle of the bridge; and Bridge 2 follows a slightly realigned bridge crossing angle.

Alternative 1, Bridge 1 crosses the railroad at a 74-degree angle requiring a 390-foot main span length with bridge approaches partially located on embankment. This bridge alternative will allow for maintenance of traffic during construction. With implementation of phased construction, traffic will be allowed to continue to use the existing bridge while part of the new bridge is built. Once completed, the existing bridge will be removed, allowing for bridge construction completion.

Alternative 1, Bridge 2 crosses the railroad at a 65-degree angle requiring a 250-foot main span length with bridge approaches located on embankment. This bridge option will not allow for maintenance of traffic during construction due to the location of the new bridge situated over a portion of the existing bridge. Complete removal

of the existing bridge would be required prior to construction of the new bridge, resulting in roadway closures and a prolonged detour. In addition, US 11 is designated as a secondary hurricane evacuation route (U.S. Geological Survey [USGS] Multi-Hazards Demonstration Project).



The US 11 bridge over the Norfolk Southern railroad, Slidell, Louisiana.

Source: Google Maps ©2014 Google

For any bridge replacement alternative, the closing of Addis Boulevard and Indiana Avenue will result due to the increased horizontal extents of a new bridge. Alternative ingress/egress to the Addis Boulevard neighborhood includes local roadway improvements to connect 4th Street to Javery Road, which intersects with Ben Thomas Road, leading to US 11. A second option includes local roadway improvements to connect Addis Boulevard to Strawberry Street south of the Western Tampa facility. This option then connects to Carnation Street, which intersects with US 190.

Continued coordination with federal, state, and local agencies solicited comments and responses regarding Bridges 1 and 2. This information was used to help determine if bridge options impact certain human, natural, or cultural resources that would result in the decision to dismiss an alternative from further evaluation. This coordination resulted in the dismissal of Bridge 2 as a viable component of Alternative 1 leaving Bridge 1 to be further evaluated (Appendix B).

3.4.3 Alignment Revisions

Potential alignment revisions were reviewed and incorporated into the preliminary alignments following the officials and public meetings held on August 22, 2013. Suggestions were considered to shift the roadway segment east on the north side of the bridge in order to reduce impacts to residential and commercial properties.

The public expressed a need for bicycle and/or pedestrian facilities within the Study Area. Currently, the City of Slidell does not have a master plan that includes bicycle and/or pedestrian improvements along US 11. Bicycle and pedestrian improvements were evaluated in accordance with LADOTD's Complete Streets Policy and in coordination with the City of Slidell and St. Tammany Parish. Anecdotal evidence also presented a need for bicycle and/or pedestrian facilities to allow safe crossing of the NS railroad.

A revision to Alternative 1, Bridge 1 resulted in Alternative 1 Revised (Alternative 1R) and includes a roadway centerline shift to the east on the north side of the US 11 bridge, which continues along this new alignment toward North Boulevard (Appendix B). A right-turn lane was added from the northbound lanes at North Boulevard — with two through lanes continuing north that shift back to the west — in order to meet recent roadway widening improvements at US 11 and Powell Drive.

3.4.4 Complete Streets

Currently, the City of Slidell and St. Tammany Parish Master Plans do not include bicycle and pedestrian improvements along US 11 within the Study Area. LADOTD's Complete Streets Policy recommends that appropriate pedestrian facilities be determined by the context of the roadway. Currently there is no pedestrian access across the NS bridge. Following the Public Meeting, Alternatives 1 and 1R were revised to incorporate a shared-use path for bicycles and pedestrians. Future planning within the City of Slidell and St. Tammany Parish may include bicycle and pedestrian improvements along or near the Study

Area. The roadway sections will allow for incorporation of a shared use path beginning at North Boulevard continuing south and connecting with Addis Boulevard on the west side of US 11. South of the bridge, the roadway section allows for a shared use path on the east side of US 11.



Bicyclists and pedestrians can then connect to areas south of the bridge utilizing the local neighborhood bicycle and/or pedestrian network providing an opportunity for future linkages to Heritage Park and the Tammany Trace. As a safety measure, pedestrian access across the bridge is not provided.

LADOTD Complete Streets Policy recommends new bridges accommodate bicycles where bicycling is not specifically prohibited. Shoulder improvements, such as paved shoulders, are recommended for roadways with greater than 10,000 vehicles per day and an average speed over 30 mph. Following the recommended improvements, 8 foot paved shoulders will be provided to accommodate bicycles. Less experienced riders can utilize the local roadway network west of US 11 and south of the overpass to connect to the shared use path that begins at the north end of Addis Boulevard.

Alternatives 1 and 1R will be subject to review by the public, local officials, resource agencies, and Native American tribes during the public hearing comment period for the Draft EA.

3.4.5 Bridge Rehabilitation

Following early agency coordination, the bridge over the NS railroad was identified as eligible for the NRHP. The LADOTD in cooperation with the FHWA and the State Historic Preservation Office (SHPO) completed a statewide historic bridge inventory for bridges constructed prior to 1971. A *National Register Eligibility Documentation Report* (September 2013) was prepared by Mead & Hunt. The FHWA made final NRHP eligibility

determinations, which are presented in the Mead & Hunt report, and the SHPO has concurred with those determinations. The NRHP employs four criteria for evaluation in determining eligibility: A, B, C, and D. As a result of the Louisiana historic bridge inventory, the US 11 bridge crossing over the NS railroad was identified as eligible under Criterion A for its association with important “events that have made a significant contribution to the broad patterns of our history”.

Important New Deal agencies that funded road and bridge construction included the Civilian Conservation Corps, Civil Works Administration, Public Works Administration, and Works Progress Administration. During the New Deal era, increased attention was given to creating grade separation between railway lines and roadways. Specific legislation was passed which resulted in funding from the U.S. Works Grade Crossing Program through the National Industrial Recovery Act (1933), Hayden Cartwright Act (1934), and Emergency Relief Appropriation Act (1935). The US 11 bridge over the NS railroad was constructed utilizing funds from this program.



Because the US 11 Bridge crossing the NS railroad was found eligible for the NRHP, it is also potentially eligible for protection under Section 4(f) of the *Department of Transportation Act of 1966* (Section 4(f)) and Section 106 of the National Historic Preservation Act (NHPA; Section 106).

The bridge is identified as Recall Number 059090, Structure Number 62520180400531, and SHPO Number 52-08393. The bridge is constructed with a concrete deck and bents, steel I-beams, and eight spans with two lanes and no shoulders. Steel beam and girder bridges often lack engineering distinction and typically exhibit little aesthetic treatment. This bridge type was built in substantial numbers, typically following standard plans, both nationally and in Louisiana.

While eligible for listing in the NRHP, no evidence was found to indicate the bridge is an important example of bridge design, engineering, or construction and, therefore, the bridge does not demonstrate significance under Criterion C (distinctive design or construction characteristics).

Bridge rehabilitation was included in the list of preliminary alternatives as the only potential means of preserving the bridge. Rehabilitation to the original condition without changing the 1930s bridge design features such as lane widths and lack of shoulders was one form of rehabilitation considered. Keeping the existing bridge in service as a part of the transportation network could avoid any adverse effects. However, to remain in service, the bridge deficiencies related to its age and design must be addressed. Bridge rehabilitation can be considered an avoidance alternative if modifications sufficient enough to address bridge deficiencies are implemented. The bridge rehabilitation can be considered an avoidance alternative that satisfies Section 4(f) requirements only if both of the following conditions can be met:

1. The elements that make the bridge historically significant are preserved; and
2. Structural and functional deficiencies are addressed.

Rehabilitation that maintains the existing bridge would not sufficiently address structural and functional deficiencies of the bridge. Correction of structural and functional deficiencies would entail removal or replacement of the existing bridge components in order to meet current LADOTD roadway design criteria, while meeting railroad clearance requirements. Therefore, rehabilitation of the existing bridge was dismissed from further consideration.

3.4.6 No Build Alternative

NEPA requires that doing nothing to address the project need be considered during the environmental review process. This alternative was designated as the No Build Alternative. Although this alternative does not meet the

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purpose and need for the project because it would not address bridge deficiencies related to its age and design, it is considered in the EA as a baseline for comparison.

3.5 Traffic

A capacity analysis was performed to address future capacity issues along US 11 from I-12 to Hall Avenue (Appendix CD-1). Traffic forecasts were performed for the design year (2042) and capacity analyses were performed for a.m., noon, and p.m. peak periods for existing conditions and future year (2042) No Build and build conditions. In addition, existing operating speeds were analyzed for the corridor and alternative intersection configurations were developed that

would improve future traffic conditions. The alternative configurations analyzed for each intersection included Option 1, standard (signalized) median openings; Option 2, restricted crossing median U-turns; and Option 3, closed medians with U-turns (Appendix B). Table 4 provides a description of the intersection changes and configurations for each of the three options.

For all future build scenarios, the following conditions were assumed:

- The current intersection of Addis Boulevard and Indiana Avenue with US 11 would be closed to accommodate the new bridge structure. (As part of the EA and public

Table 4: Intersection Configurations for Build Options

Intersection with US 11	Option 1	Option 2	Option 3
I-12 WB Ramps	No changes		
I-12 EB Ramps	No changes		
U-Turn (1)	Provided for US 11 NB traffic to access Powell Drive		
Powell Drive	Right out only; closed median		
U-Turn (2)	Separate U-turns. One for Powell Drive (SB) and one for Ben Thomas Road (NB)	Separate U-turns. One for Powell Drive (SB) and one for Ben Thomas Road and North Boulevard (NB)	
Ben Thomas Road	Right out only; closed median		
North Boulevard	Full-access opening	Right out only; SB left allowed from US 11	
U-Turn North of Bridge	Provided for Ben Thomas Road traffic and businesses on the western side of US 11; traffic will use U-turn to travel northbound		
Addis Boulevard	Closed		
Indiana Avenue	Closed		
U-Turn South of Bridge	Provided for businesses on the eastern side of US 11; traffic will use U-turn to travel SB		
Lafayette Street	Full-access opening	Right out only; SB left allowed from US 11	Right out only; SB left allowed from US 11; dual U-turn lanes provided for Gause Boulevard
US 190 (Gause Boulevard)	Full-access opening	Full-access opening	Left turns restricted on Gause Boulevard; traffic must turn right onto US 11 and make U-turn
Hall Avenue	Full-access opening	Right out only; SB and NB lefts allowed from US 11	Right out only; closed median
U-Turn (1)	N/A		Provided for Gause Boulevard traffic

outreach, local access for the Addis Boulevard neighborhood has been identified.)

- US 190 would be widened on the western side of US 11 from a two-lane undivided to a four-lane divided roadway.
- US 11 at I-12 (both eastbound and westbound ramps) would not be modified because they were recently improved (2012) under a separate project.

Several new median U-turns were required in the build condition due to the closed median and restricted crossings planned for the corridor. The U-turn north of the bridge is warranted in Option 1 under open year volumes. The U-turn between Powell Drive and Ben Thomas Road is only slightly above the threshold value and shows acceptable LOS values. Therefore, it is not deemed warranted. For Options 2 and 3, the U-turns between Powell Drive and Ben Thomas Road are warranted for signalization for the open year volumes along with the U-turn north of the bridge during p.m. peak periods. For Option 3, dual U-turn lanes are also needed for Gause Boulevard traffic at Lafayette Street and the U-turn south of Hall Avenue.

3.5.1 Build Conditions for Intersections

Build improvements will provide safer and more efficient operating conditions along the study corridor as compared to the No Build scenario.

The capacity analysis results for Build Options 1, 2, and 3 are summarized in Table 5 and illustrated on figures in Appendix B, respectively. All three options show a reduction in delay at several intersections as compared to No Build conditions in the design year. For Option 1, the eastbound approaches of Ben Thomas and Hall Roads show high delay during p.m. peak periods. The intersection of US 11 and I-12 westbound ramps show similar delay and low LOS for the No Build scenario. For Option 2, the intersection of US 11 and US 190 remains at LOS E during a.m. and p.m. peak periods. This intersection average is LOS D during the p.m. peak period for Option 3. In addition, the westbound approach at Lafayette Street operates at LOS D during the a.m. and noon peak periods.

Build Option 3 shows the greatest reduction in delay and improvement in travel speed over the No Build condition. Also, as previously noted, the intersection of US 190 and US 11 shows the greatest operation improvement under Option 3.

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Table 5: LOS Results for Intersection Build Option 1, 2, and 3 (2042)

Intersection with US 11	S/U	Eastbound			Westbound			Northbound			Southbound		
		Option			Option			Option			Option		
		1	2	3	1	2	3	1	2	3	1	2	3
AM Peak Period													
I-12 WB Ramps	S	F	F	F	E	E	E	E	E	E	E	E	E
I-12 EB Ramps	S	B	B	B	-	-	-	B	B	B	C	C	C
U-turn 1	U	-	-	-	-	-	-	C	C	C			
Powell Drive	U	C	C	C	-	-	-						
U-turn 2	U	-	-	-	-	-	-	D	B	C	A	D	C
Ben Thomas Road	U	C	B	B	-	-	-						
North Boulevard	S	-	-	-	B	C	C	B	A	B	A	A	A
U-turn North of Bridge	S	-	-	-	-	-	-	A	A	A	C	C	C
U-turn South of Bridge	U	-	-	-	-	-	-	C	B	B	-	-	-
Lafayette Street	S	D	-	C	D	B	D	A	B	C	A	A	A
US 190 (Gause Blvd)	S	D	D	C	C	D	C	D	F	D	D	E	C
Hall Avenue	S	D	B	-	C	B	B	C	A		B	B	
U-turn 3	S	-	-	-	-	-	-	-	-	A	-	-	C
Noon Peak Period													
I-12 WB Ramps	S	E	E	E	D	D	E	D	D	C	D	D	D
I-12 EB Ramps	S	B	B	B	-	-	-	A	B	B	A	A	A
U-turn 1	U	-	-	-	-	-	-	B	B	B			
Powell Drive	U	C	C	C	-	-	-						
U-turn 2	U	-	-	-	-	-	-	C	B	C	A	C	C
Ben Thomas Road	U	C	B	B	-	-	-						
North Boulevard	S	-	-	-	B	C	C	A	A	A	A	A	A
U-turn North of Bridge	S	-	-	-	-	-	-	A	A	A	C	B	C
U-turn South of Bridge	U	-	-	-	-	-	-	A	A	A	-	-	-
Lafayette Street	S	C	-	-	C	B	D	A	B	C	A	B	A
US 190 (Gause Blvd)	S	D	D	B	C	D	C	D	E	C	E	E	C
Hall Avenue	S	D	C	B	C	B	B	B	B		A	B	
U-turn 3	S	-	-	-	-	-	-	-	-	A	-	-	C
PM Peak Period													
I-12 WB Ramps	S	F	F	E	F	F	F	D	D	D	E	E	E
I-12 EB Ramps	S	B	B	B	-	-	-	B	B	B	C	A	B
U-turn 1	U	-	-	-	-	-	-	C	C	C			
Powell Drive	U	C	C	C	-	-	-						
U-turn 2	U	-	-	-	-	-	-	C	B	C	B	C	C
Ben Thomas Road	U	E	C	C	-	-	-						
North Boulevard	S	-	-	-	C	C	C	B	C	B	B	B	B
U-turn North of Bridge	S	-	-	-	-	-	-	A	A	A	C	B	C
U-turn South of Bridge	U	-	-	-	-	-	-	A	C	A	-	-	-
Lafayette Street	S	D	-	-	D	B	B	A	B	C	B	A	B
US 190 (Gause Blvd)	S	D	E	C	C	E	D	D	E	D	D	E	D
Hall Avenue	S	E	B	C	C	B	B	C	B		C	B	
U-turn 3	S	-	-	-	-	-	-	-	-	A	-	-	B

- = No approach. EB = Eastbound. S = Signalized.
 = Not applicable. NB = Northbound. U = Un-signalized.
 = LOS E. SB = Southbound. LOS = Level of service.
 = LOS F. WB = Westbound.
 = U-turn delay.

3.5.2 Build Option Comparisons and Recommendations

The intersection capacity analysis for the No Build conditions shows that many intersections in the Study Area will operate near or over capacity at LOS E or F by the design year. The analysis reveals that all signalized intersections will operate at LOS D or better in the design year under Build Option 3.

Table 6 presents a summary and comparison of the No Build and Build Options performance results from the analysis for both the network as a whole and for the US 11 corridor. Performance indicators included average delay per vehicle and average speed.

Table 6: Comparison of No Build and Build Options

Design Year (2042) - AM Peak Hour				
Options	Network Level		US 11 Corridor	
	Delay/Vet (sec)	Avg Speed (mph)	Delay/Vet (sec)	Avg Speed (mph)
No Build	26	17	15	23
Option 1	16	20	11	24
No Build vs. Option 1	-38%	18%	-27%	4%
Option 2	17	20	11	24
No Build vs. Option 2	-35%	18%	-27%	4%
Option 3	14	21	10	25
No Build vs. Option 3	-46%	24%	-33%	9%
Design Year (2042) - NOON Peak Hour				
No Build	18	18	13	23
Option 1	13	23	9	27
No Build vs. Option 1	-28%	28%	-31%	17%
Option 2	14	22	9	27
No Build vs. Option 2	-22%	22%	-31%	17%
Option 3	11	23	8	27
No Build vs. Option 3	-39%	28%	-38%	17%
Design Year (2042) - PM Peak Hour				
No Build	101	6	24	17
Option 1	19	19	13	23
No Build vs. Option 1	-81%	217%	-46%	35%
Option 2	18	19	12	24
No Build vs. Option 2	-82%	217%	-50%	41%
Option 3	15	20	11	24
No Build vs. Option 3	-85%	233%	-54%	41%

Avg = average mph = mile per hour veh = vehicle EB = Eastbound sec = second

3.6 Alternatives Comparison

3.6.1 Resource Impact Analysis

A number of resources and issues were used to compare each alternative chosen for detailed evaluation. The resources used to compare the alternatives are compiled in the GIS for the pro-

ject or detailed in a series of technical documents that are incorporated by reference into the EA. Alternatives were evaluated with respect to the environmental and engineering factors. Table 7 summarizes these effects.

Table 7: Alternatives Evaluation Matrix

Evaluation Factors	Alternative 1, Bridge 1			Alternative 1R, Bridge 1			No Build
	Intersection Configuration Option			Intersection Configuration Option			
	1	2	3	1	2	3	
Structure Impacts							
Residence	0	0	0	0	0	0	0
Business	13	13	13	13	13	13	0
Church	0	0	0	0	0	0	0
Public Facility	0	0	0	0	0	0	0
Underground Storage Tanks/Pumps/Piping	6	6	6	6	6	6	0
Signage	11	11	11	11	11	11	0
Parking Spaces	107	107	107	107	107	107	0
Potential Relocations	13	13	13	13	13	13	0
Noise	10	10	10	10	10	10	4
Natural Resources Impacts							
Cemetery	0	0	0	0	0	0	0
100-Year Floodplain (acres)	7	7	7	6	6	6	0
Surface Waters (acres)	<1	<1	<1	<1	<1	<1	0
Wetland (acres)	<1	<1	<1	<1	<1	<1	0
Prime Farmland (acres)	0	0	0	0	0	0	0
Archaeological	0	0	0	0	0	0	0
Historic Resources > 50 Years Old	23	23	23	23	23	23	0
Historic Resources—Potentially Eligible	2	2	2	2	2	2	0
Known Underground Storage Tank Sites	3	3	3	3	3	3	0
Water Wells	3	3	0	3	3	3	0
Oil and Gas Wells	0	0	0	0	0	0	0
Traffic Impacts							
Permanent Road Closures	2	2	2	2	2	2	0
Temporary Detour	0	0	0	1	1	1	0

3.6.2 Preliminary Costs Analysis

Preliminary costs analysis for the alternatives includes roadway construction, bridge construction, utility relocation, ROW, wetland

mitigation and surveying, engineering, and construction supervision/inspection. These costs are presented in Table 8.

Table 8: Alternatives Cost Analysis

Evaluation Factors	Alternative 1, Bridge 1 Intersection Configuration Option			Alternative 1R, Bridge 1 Intersection Configuration Option			No Build
	1	2	3	1	2	3	
	Total Length (miles)	1.75	1.75	1.75	1.75	1.75	
Cost (million dollars)							
Roadway Construction	13.1	13.1	13.1	13.1	13.1	13.1	0
Bridge Construction	35.8	35.8	35.8	35.8	35.8	35.8	0
Utility Relocation	0.505	0.505	0.505	0.505	0.505	0.505	0
Right-of-Way Acquisition	7.1	7.1	7.1	6.8	6.8	6.8	0
Wetland Mitigation	0.16	0.16	0.16	0.16	0.16	0.16	0
Surveying, Engineering, Construction Supervision & Inspection	3.8	3.8	3.8	3.8	3.8	3.8	0
TOTAL	60.5	60.5	60.5	60.2	60.2	60.2	0

Includes 20% Roadway and 10% Bridge Contingency for Estimating Purposes. Costs Rounded.

3.6.3 Preferred Alternative

As a result of the comprehensive resources evaluation, traffic studies, and public, local officials, and federal and state resource agency coordination, sufficient information and public opinion exist to identify Alternative 1R, Bridge 1, Intersection Configuration Option 3 as the Preferred Alternative. This Alternative includes the widening of US 11 from Gause Boulevard north connecting with Bridge 1, which is situated along the existing bridge alignment, and roadway widening from the north side of the bridge north to Powell Avenue. The Preferred Alternative includes a shared use path of a segment of the widened roadway connecting to a local roadway network.

The identification of the Preferred Alternative addresses the stated purpose and need and satisfies, to the fullest extent possible, the objectives of NEPA. Impacts from the Preferred Alternative were avoided where possible and minimized to the greatest extent practicable.



SECTION 4

Existing Conditions & Impacts

*In This Section....*Key resources are evaluated to determine the potential beneficial or adverse impacts of the project's Preferred Alternative and No Build Alternative.

Evaluation of social, economic, and environmental resources indicates minor or no impacts to populations including minority and low-income, farmlands, biological resources, and air quality. Impacts are noted for noise, historic resources, underground storage tank sites, wetlands, and temporary construction related impacts.

4.1 Introduction

This section provides an analysis of the potential beneficial or adverse impacts of the project's Preferred Alternative and No Build Alternative. The project is evaluated with respect to transportation, social, economic, cultural, physical, natural, and biological resources. This section discusses direct impacts (loss of a resources), indirect impacts (changes in function or quality of a resource), and cumulative impacts (historical, project related, and foreseeable impacts).

4.2 Land Use and Community Resources

4.2.1 Land Use

Land use classifications within the Study Area are shown on Figure 5. The Study Area comprises approximately 35 acres. Land use within the Study Area is predominantly transportation including roadway and rail line uses. Commercial land uses are located along state routes while single-family residential and undeveloped lands are located along existing local roadways. North of the bridge that crosses the NS railroad, US 11 is bounded to the east by the railroad and to the west by a large flood control basin and commercial and light industrial uses. Interstate 12 is adjacent to the Study Area to the north. Single family residential is the primary use adjacent to the bridge to the east and west. Some light industrial is located to

the southwest of the bridge. South of the bridge, US 11 is bounded on the east by commercial land uses including retail, office, and a gasoline station and on the west by the railroad. A shopping center is located to the west beyond the railroad. Approximately 52 percent of the Study Area is transportation land use with 26 percent undeveloped lands including woodlands, wetlands, and surface waters. Commercial and residential land uses comprise approximately 10 and 2 percent of the Study Area respectively.

For all alternatives, existing roadway ROW or previously disturbed lands will be converted from its present use to transportation use.

The No Build Alternative would not impact land use.

4.2.2 Residential and Business Relocations

Potential structure impacts within the Study Area include one residential and six commercial structures. Figure 5 shows and Table 9 lists the number of displacements the proposed project may have on residential and commercial structures within the Study Area.

The No Build Alternative does not require any relocations.

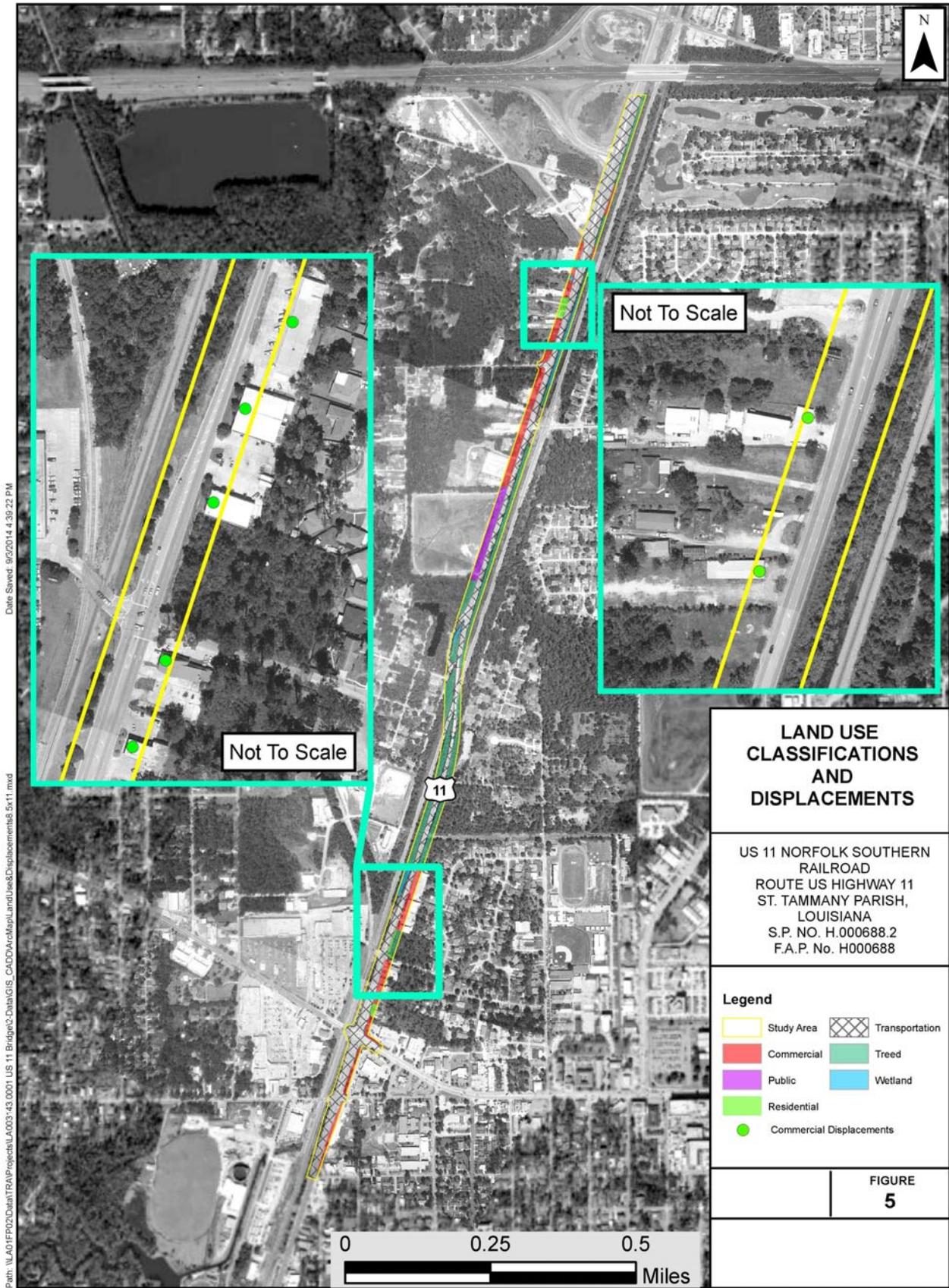


Table 9: Number of Residential and Commercial Displacements by Alternative

Type of Displacement	Number of Displacements Alternative 1, Bridge 1		
	Option 1	Option 2	Option 3
Residential	0	0	0
Owner-Occupied Business	2	2	2
Tenant Business	11	11	11
Personal Property Only Moves	1	1	1

Source: Quest Acquisitions, LLC 2014

4.3 Economic Environment

Early Slidell and St. Tammany Parish residents enjoyed a robust tourist industry while many depended on the land for their economic welfare. Along with farming and trapping, an aggressive timber industry supported lumber mills, such as the Salmen Brick and Lumber Company, into the 20th century. Brickmaking also remained an important industry in the Parish until their decline following World War I. Today, industry sectors that contribute the highest employment opportunities include a combination of educational services, health care, and social assistance followed by retail trade, government, light manufacturing, and professional, services (St. Tammany Economic Development November 2013).

Economic impacts associated with construction of the proposed project will include a temporary increase in construction-related employment. Benefits from the proposed project such as reduced congestion, increased traffic flow, and increased accessibility may improve the economic environment within and adjacent to the Study Area.

The No Build Alternative would lead to continued and worsened congestion within the Study Area

and surrounding area and may have a negative economic impact on employment.

4.4 Socioeconomic Resources

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority and Low-Income Populations* (59 Federal Register 7629 1994), requires federal agencies to determine whether a proposed action would have an adverse and disproportionately high impact on minority and/or low-income populations.

4.4.1 Population

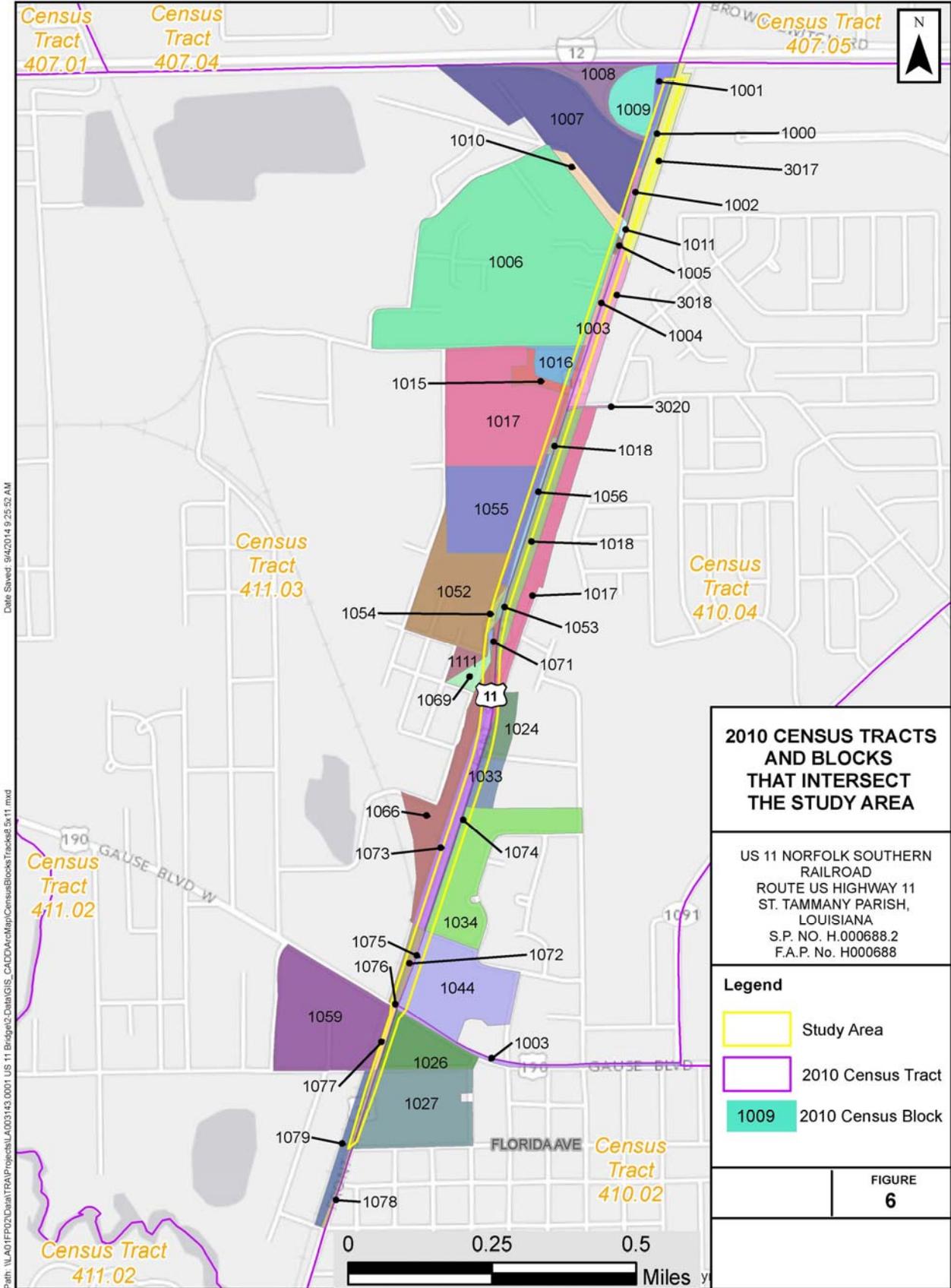
The 2010 U.S. Census identified three Census Tracts, 410.02, 410.04, and 411.03, comprised of 46 Census Blocks that intersect the Study Area (Figure 6). The population within the Census Blocks was examined to determine total population and minority and/or low-income populations associated with improvements related to all alternatives. Census Block data were compared with Census Tract level data in order to identify potential disproportionate impacts.

Only 9 of the 46 Census Blocks that intersect the Study Area represent the population within the Study Area. The estimated population within the Study Area is 241. The Census Block population is 2 percent of the Census Tract population, 0.8 percent of the Slidell population, and 0.01 percent of the St. Tammany Parish population (Table 10).

Table 10: Total Population Data

Geographic Area	Population
Louisiana	4,533,372
St. Tammany Parish	233,740
City of Slidell	27,068
2010 Census Tracts Within Study Area	12,709
2010 Blocks Within Study Area	241

Source: U.S. Census Bureau, Census 2010 (www.census.gov)
 Note: Geographic Area was determined to be the Census Blocks within Census Tracts 410.02, 410.04, and 411.03 that intersect the Study Area.



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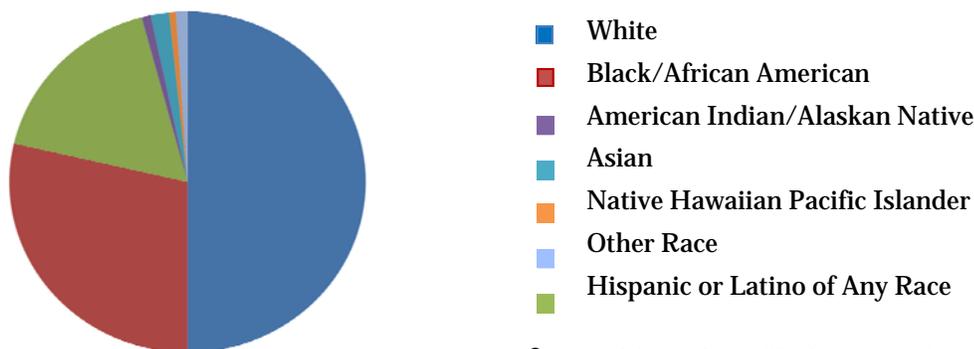
4.4.2 Minority Populations

The racial and ethnic composition of the population within the Study Area was examined in order to identify the presence or absence of minority populations. Within the Census Blocks that intersect the Study Area, 57 percent of the population is identified as white alone and

43 percent as minority. Total and minority population data are depicted on Figures 7 and 8 and presented in Table 11.

The Preferred Alternative and No Build Alternative would have no effect on the minority population within the Study Area.

Figure 7: Total and Minority Populations



Source: US Census Bureau, 2010 Summary File 1; Tables QT-P5 and P5, B03002 2008-2012 America Community Survey 5-Year Estimates

Table 11: Total and Minority Populations

Geographic Area	Total Pop.	Not Hispanic or Latino						Hispanic or Latino of Any Race	Total Minority Pop (%)
		White	Black / African American	AIAN *	Asian	NHPI*	Other Race		
All Blocks TOTAL	256	136	83	4	8	3	5	17	40.2
TOTAL Percent	100	53.1	32.4	1.6	3.1	1.2	1.9	6.7	40.2
Census Tract 410.02	2,988	2,764	72	14	0	0	76	62	5.4
Census Tract 410.04	4,267	3,751	179	0	53	0	59	225	6.8
Census Tract 411.03	3,454	1,571	1,403	0	35	0	82	363	44.0
Census Tracts TOTAL	10,709	8,086	1,654	14	88	0	217	650	18.4
TOTAL Percent	100	75.5	15.4	0.1	0.8	0	2.1	6.1	18.4

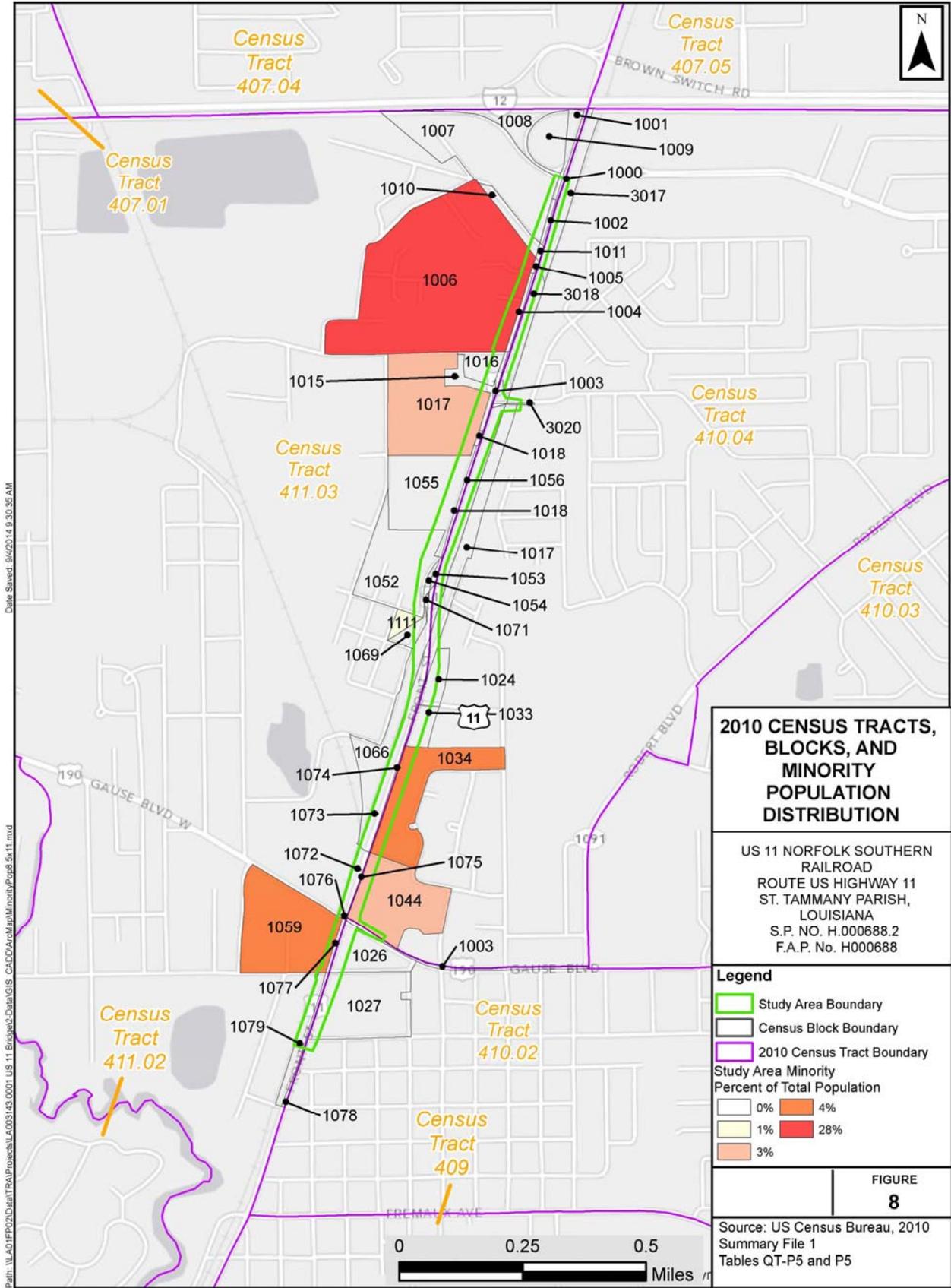
Source: US Census Bureau, 2010 Summary File 1; Tables QT-P5 and P5, B03002 2008-2012 America Community Survey 5-Year Estimates

*AIAN - American Indian and Alaskan Native, NHPI - Native Hawaiian and Other Pacific Islander.

Excludes Hispanic or Latino of Any Race to avoid double counting. See Note Below.

Notes: Geographic Area was determined to be blocks within Census Tracts 410.02, 410.04, and 411.03 that intersect the Study Area.

The race concept "alone or in combination" includes people who reported a single race alone (e.g., Asian) and people who reported that race in combination with one or more of the other race groups. The sum of the six individual race "alone or in combination" categories may add to more than the total population because people who reported more than one race are tallied in each race category. Likewise the "alone" categories may add to less than the total population.



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ENVIRONMENTAL ASSESSMENT

4.4.3 Low-Income Populations

The Census Tracts that intersect the Study Area represent the demographic area evaluated for low-income populations. The median household income and persons of poverty status were examined in order to identify the presence or absence of low-income populations within the Study Area and determine if the proposed project would impact low-income persons. The poverty level was determined based on the 2013 U.S. Department of Health and Human Services poverty threshold of \$23,550 for a family of four.

Table 12 presents the estimated number of households, median household income, and households below the poverty level within Census Tracts 410.02, 410.04, and 411.03. Slightly more than 11 percent of households within the Census Tracts that intersect the Study Area are below the poverty level.

Neither the Preferred Alternative nor the No Build Alternative would have a disproportionate impact on low-income populations.

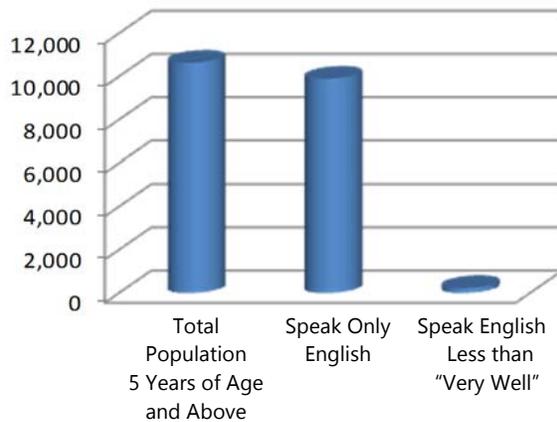
4.4.4 Limited English-Speaking Proficiency

Executive Order 13166, *Improving Access to Services for Persons with Limited English Proficiency* (LEP) (2001), requires federal agencies to work to provide meaningful access to LEP applicants and beneficiaries. 2010 Census data were reviewed for Language Spoken at Home by Ability to Speak English for the Population 5 Years

and Over in the Study Area. Less than 3 percent of the Study Area population speaks English “less than very well”. Other languages spoken by a small percentage of the Study Area population are Spanish or Spanish Creole and French. Figure 9 shows LEP for the population within the Study Area.

The No Build Alternative would not have an impact on LEP populations within or adjacent to the Study Area.

Figure 9: Limited English Proficiency Populations



Source: U.S. Census Bureau, 2006-2010 American Community Survey 5-Year Estimates. Table B16001.

4.4.5 Environmental Justice

Per Executive Order 12989, the Study Area was examined to determine if the proposed project would disproportionately affect minority and low

Table 12: Median Household Income and Poverty Status

Geographic Area	2012 Households ⁽¹⁾	Median Household Income	Households Below Poverty Level ⁽²⁾	
			Number	Percent of Census Tract
Census Tract 410.02	1,104	\$49,423	210	19.0
Census Tract 410.04	1,590	\$61,333	85	18.7
Census Tract 411.03	738	\$49,636	10	1.4
Total	3,432		305	11.3

Source: U.S. Census Bureau, 2008-2012 American Community Survey 5-Year Estimates S1903 (www.census.gov).

Note: Geographic Area was determined to be the Census Tracts that intersect the Study Area.

⁽¹⁾ Total Households within Census Tracts 410.02, 410.04, and 410.03.

⁽²⁾ Households below the poverty level were determined based on 2008-2012 American Community Survey 5-Year Estimates, B25121 and 2013 U.S. Department of Health and Human Services poverty threshold of \$23,550 for a family of four.

income populations. Concentrations of minority populations were identified within the Study Area by mapping the census block populations of individuals self-identified as Black/African American, American Indian, Alaska Native, Native Hawaiian, Other Pacific Islander, Other Race and/ or Two or More Races for the 2010 U.S. Census. Two census blocks have minority populations of 48 and 21 percent, respectively. The remaining census blocks within the Study Area have minority populations of 12 percent or less. Because improvements associated with the Preferred Alternative are located along the existing US 11 roadway, the proposed project will not have disproportionately high adverse effects on minority populations.

Slightly more than 11 percent of households within the Census Tracts that intersect the Study Area are below the poverty level.

It is expected that the proposed project will not raise environmental justice issues. The No Build Alternative would not raise environmental justice issues.

4.5 Natural and Physical Environment

4.5.1 Geology and Soils

The Study Area is located within the Gulf Coastal Plain of Louisiana and lies on the Mississippi Embayment, a sedimentary sequence thousands of meters thick, which includes mostly unconsolidated clays, silts, sands (Earth Search, Inc. [ESI] 2014). Surface exposures consist of Quaternary sediment occupied by Pleistocene terraces. According to the USGS, Slidell, Louisiana, 7.5-minute quadrangle map (2012), topography in the Study Area is associated with the broad flats and low ridges characteristic of the southeastern part of the parish. Elevations across the Study Area rise from 10 feet at the US 11/US 190 intersection to 20 feet at the US 11/I-12 intersection.

Soils within the Study Area are primarily composed of the Myatt-Stough-Prentiss map unit classified by the U.S. Department of Agriculture, *Soil Survey of St. Tammany Parish,*

Louisiana (March 1990). These soils are a mix of level and very gently sloping, poorly to moderately well drained soils that are loamy throughout. This association is located on broad terraces in the southern part of the parish. Myatt soils are level, poorly drained sandy loam soil located on flats and in depressional areas and drainageways. Stough soils are level and somewhat poorly drained sandy loam soil and occur in higher positions relative to Myatt soils. Prentiss soils also are level, very gently sloping, and moderately well drained sandy loam soils located on low ridges.

Figure 10 shows the soil series, or groups, mapped within the Study Area as well as the estimated percent coverage of each soil within the Study Area based on information provided by the Natural Resources Conservation Service (NRCS).

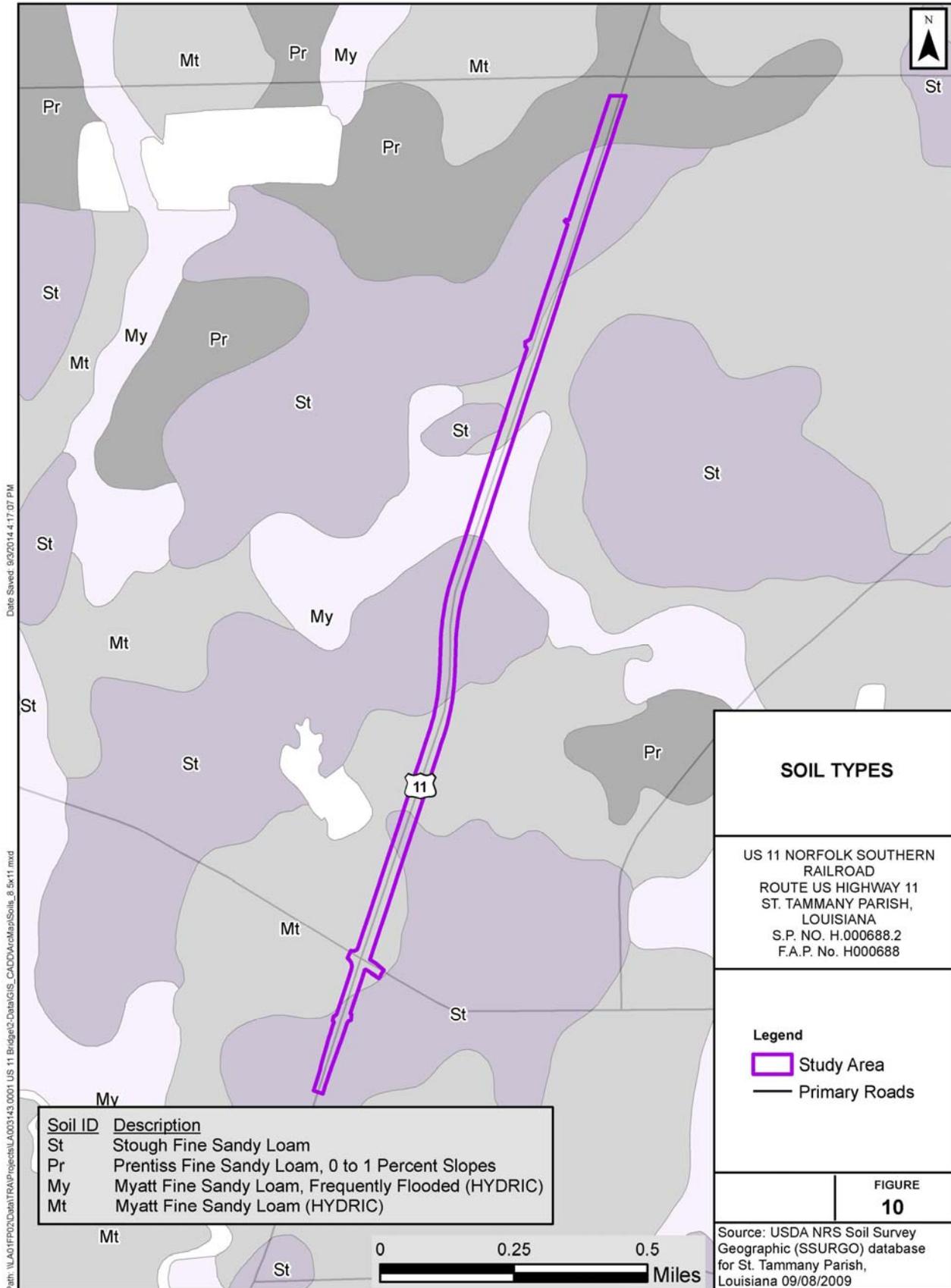
The Myatt soils are categorized within Hydrologic Group D which has very slow infiltration rates, a permanent high water table, and a clay layer near the surface and are considered hydric. The Stough and Prentiss soils are categorized within Hydrologic Group C, which have somewhat restricted infiltration rates, and a water table within 2 feet of the surface.

4.5.2 Farmland Protection Policy Act

The U. S. Department of Agriculture (USDA), through the NRCS, administers the *Farmland Protection Policy Act 1983 Subtitle I of Title XV, Section 1539 – 1549* (FFPA). The purpose of the FFPA is to “minimize the extent to which federal programs contribute to the unnecessary and irreversible conversion of farmland to nonagricultural uses.” The NRCS defines prime farmland and soils as those that have the best combination of physical and chemical characteristics to economically produce high yields of agricultural crops when treated and managed according to acceptable farming practices.

To ensure compliance with the FFPA, agency coordination with the NRCS, Alexandria, Louisiana, was initiated May 3, 2013 (Appendix C).

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In a letter dated May 6, 2013, the NRCS determined that the proposed project will not impact soils that are classified as prime, unique, or of statewide or local importance. Therefore, the proposed project is exempt from FPPA rules and regulations due to its location within an urban area.

The Preferred Alternative would result in minimal disturbance to soils and geologic resources and is primarily located within existing roadway ROW. As such, these areas have been previously disturbed and no impacts are anticipated.

The No Build Alternative would not impact the geology, soils, or farmlands.

4.5.3 Water Resources

The Study Area is located within the Pontchartrain Basin of Louisiana, which is bounded by the state of Mississippi to the north, the Gulf of Mexico to the south, the Pearl River to the east, and the Mississippi River to the west. Surface waters identified within the Study Area include two southeast Louisiana (SELA) Urban Flood Control drainage canals, which flow into Bayou Vincent and Bayou Bonfouca and discharge into Lake Pontchartrain south of the Study Area (Figure 11).

The existing roadway spans two drainage canals with concrete box culverts. The culvert for the SELA stream south of the US 11 bridge will be extended to accommodate the additional lanes. The box culvert north of the US 11 bridge will be replaced to accommodate the widened roadway and proposed bridge.

The No Build Alternative would not impact surface waters located within the Study Area.

The Louisiana Scenic Rivers Act of 1988 established the Louisiana Natural and Scenic Rivers System which is intended to protect, conserve, and replenish the natural resources of the state including certain free-flowing streams or segments.

Neither the Preferred Alternative nor the No Build Alternative would impact natural and scenic rivers.

A floodplain evaluation was conducted in accordance with Executive Order 11988,

Floodplain Management (1977), 23 Code of Federal Regulations (CFR) 650, Subpart A "Location and Hydraulic Design of Encroachments on Floodplains" and U.S. Department of Transportation (DOT) 5650.2 "Floodplain Management and Protection".

The location of the 100-year floodplain for the Study Area was identified from Federal Emergency Management Agency (FEMA) Digital Flood Insurance Rate Maps (FIRM) and is shown on Figure 11. Special Flood Hazard Area designations are indicated as Zone A and Zone X within the Study Area. Zone A designates the flood expected to occur on average once every 100 years. Zone X designates the flood expected to occur once every 500 years.

The Preferred Alternative impacts approximately 6 acres of floodplain area (Table 13).

There is no practicable alternative to the proposed location of the Preferred Alternative

Table 13: Floodplain Impact by Alternative

Alternative	Flood Zone A (100-year flood) (acres)
Alternative 1	
Option 1	7
Option 2	7
Option 3	7
Alternative 1R	
Option 1	6
Option 2	6
Option 3	6
No Build	0

Sources: *Flood Insurance Rate Map, City of Slidell, Louisiana, St. Tammany Parish*. Community Panel Number 220204 0010 C, Revised April 21, 1999 and *St. Tammany Parish, Louisiana* Community Panel No. 225205 0410D, Revised April 21, 1999.

that does not cross floodplains. The Preferred Alternative includes all practicable measures to minimize floodplain impacts.

The No Build Alternative would not further impact floodplains within the Study Area.

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4.5.4 Wetlands

All wetlands identified within the Study Area were evaluated in accordance with Executive Order 11990, *Protection of Wetlands* (1977), and the technical guidelines and methods for wetland delineations as set forth in the U.S. Army Corps of Engineers (USACE) *Wetland Delineation Manual* (1987) and the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Atlantic and Gulf Coastal Plain Region Wetland Delineation Manual* (2010).

An initial site visit was conducted on May 9, 2013, to visually assess the Study Area and note the location of probable wetlands. A formal delineation followed on June 18, 2013.

Each wetland site was documented with photographs and field notes, and boundaries were delineated and mapped using a sub-meter global positioning system unit. Observations of vegetation, hydrology, soils, and other visible wetland indicators were recorded on *Wetland Determination Forms – Atlantic and Gulf Coastal Plain Region*.

Table 14 lists wetland impacts by alternative. A detailed analysis and description of wetlands and other waters identified within the Study Area can be found in the Biological Resources and Wetland Findings Report (Appendix CD-2).

Wetlands lost from construction of the proposed project would be replaced through mitigation. Mitigation comprises measures which avoid, minimize, and/or compensate for unavoidable losses to resources that cannot be further minimized. The assessment of mitigation measures (avoidance, minimization, and compensation) is an integral part of the NEPA/Section 404 process. For those impacts that cannot be avoided, other mitigation efforts must be considered. These efforts include minimization of potentially adverse impacts and compensation for those remaining adverse impacts that cannot be reduced any further.

Construction activities associated with the build alternative would impact wetlands and surface

waters to varying degrees. Land clearing during construction would remove vegetative cover with the potential to increase surface runoff during storm events leading to erosion and increased sediment deposited in surface waters.

To aid in minimizing such impacts, placement and monitoring of erosion control measures for soil stabilization along with temporary and permanent vegetation measures at the start of, during, and after construction would be incorporated into project construction plans according to LADOTD's standard specifications.

Measures to minimize impacts to wetlands may

Table 14: Wetland Impact by Alternative

Alternative	Wetland Area (acres)
Alternative 1	
Option 1	0.4
Option 2	0.4
Option 3	0.4
Alternative 1R	
Option 1	0.8
Option 2	0.8
Option 3	0.8
No Build	0

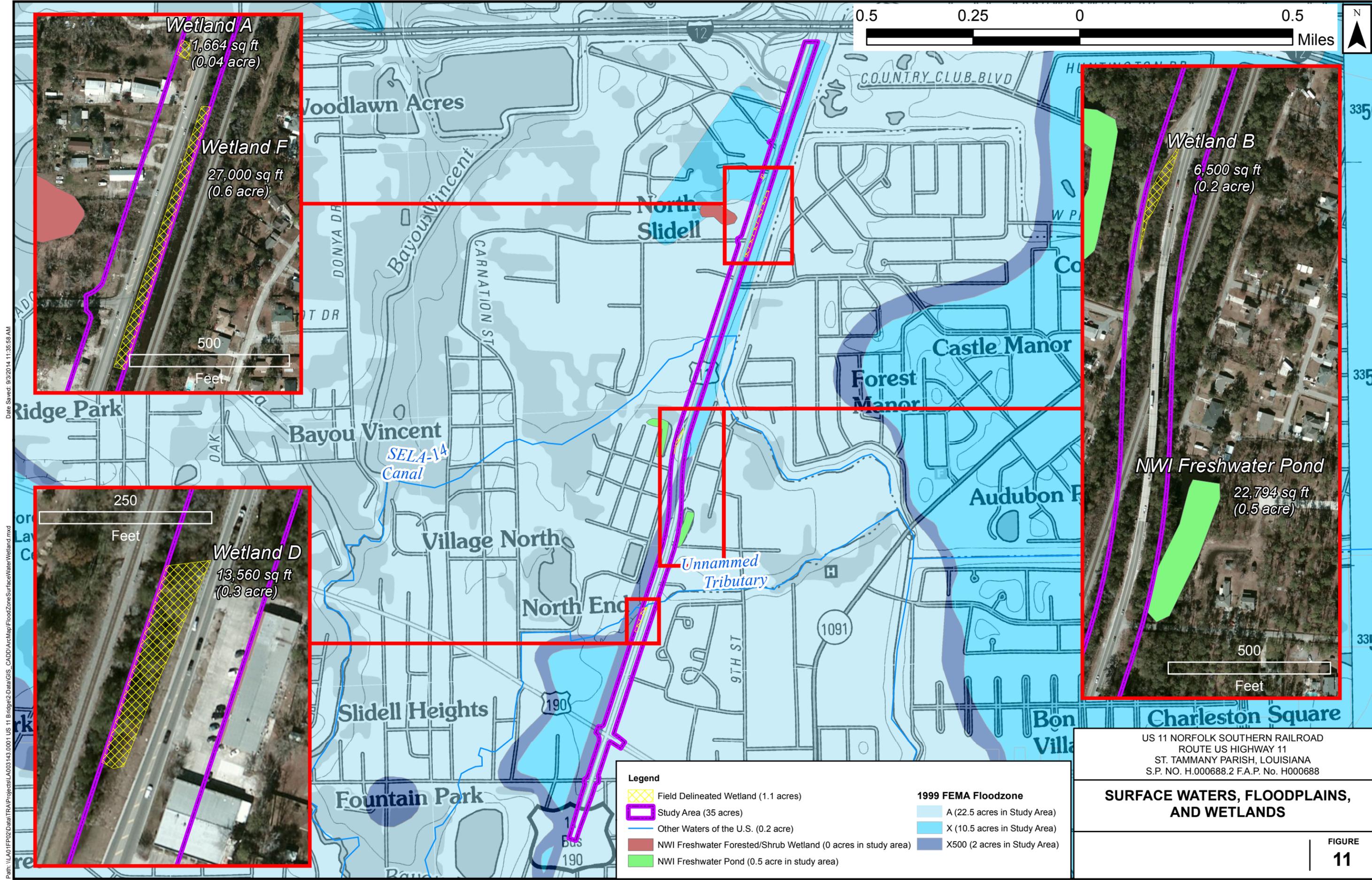
Sources: *Flood Insurance Rate Map, City of Slidell, Louisiana, St. Tammany Parish*. Community Panel Number 220204 0010 C, Revised April 21, 1999.

Flood Insurance Rate Map, St. Tammany Parish, Louisiana Community Panel Number 225205 0410D, Revised April 21, 1999.

include minimizing clearing of wetland vegetation to the limits of construction and minimizing use of wetland areas outside the construction limits for construction support activities (borrow sites, waste sites, storage, parking, access, etc.).

Final compensatory mitigation ratios and requirements for impacted areas classified as jurisdictional will be determined by the USACE New Orleans District through the Section 404 permit process.

It has been determined that there is no practicable alternative to the proposed



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Wetland A
1,664 sq ft
(0.04 acre)

Wetland F
27,000 sq ft
(0.6 acre)

500
Feet

Wetland B
6,500 sq ft
(0.2 acre)

NWI Freshwater Pond
22,794 sq ft
(0.5 acre)

500
Feet

250
Feet

Wetland D
13,560 sq ft
(0.3 acre)

Legend

- Field Delineated Wetland (1.1 acres)
- Study Area (35 acres)
- Other Waters of the U.S. (0.2 acre)
- NWI Freshwater Forested/Shrub Wetland (0 acres in study area)
- NWI Freshwater Pond (0.5 acre in study area)
- 1999 FEMA Floodzone A (22.5 acres in Study Area)
- X (10.5 acres in Study Area)
- X500 (2 acres in Study Area)

US 11 NORFOLK SOUTHERN RAILROAD
ROUTE US HIGHWAY 11
ST. TAMMANY PARISH, LOUISIANA
S.P. NO. H.000688.2 F.A.P. No. H000688

**SURFACE WATERS, FLOODPLAINS,
AND WETLANDS**

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construction involving impacts to wetlands and the proposed action includes all practicable measures to minimize harm to wetlands which may result from this project.

The No Build Alternative would not impact area wetlands and other waters of the U.S.

4.5.5 Coastal Zone

The LDNR Office of Coastal Management administers the Louisiana Coastal Resources Program (LCRP) in accordance with the federal Coastal Zone Management Act (CZMA). The proposed improvements are located within the coastal zone and are a use of state concern. A Coastal Use Permit (CUP) in compliance with the LCRP will be required for the project in conjunction with the USACE, New Orleans District.

4.5.6 Biological Resources



Wetland Area near Addis Boulevard and US 11.

Section 7 of the Endangered Species Act of 1973 (as amended) requires that federal agencies ensure any action authorized, funded, or carried out by that agency is not likely to adversely impact threatened or endangered species or result in destruction of critical habitat. Coordination with U.S. Fish & Wildlife Service (USFWS), Louisiana Ecological Services Office, and

the Louisiana Natural Heritage Program (LNHP) was made as part of the Solicitation of Views (SOV) process to determine if known rare, threatened, or endangered species exist within the Study Area.

In a response to a request for federal trust resources review for the proposed project (Appendix C), the USFWS responded to the proposed project stating it “is not likely to adversely affect those resources. This finding fulfills the requirements under Section 7(a)(2) of the Act.”

The LNHP maintains a database with known locations of federally listed threatened and endangered species as well as state species of special concern. The LNHP responded to the SOV stating that no impacts to rare, threatened, or endangered species or critical habitats are anticipated for the proposed project. The response also stated that no state or federal parks, wildlife refuges, scenic streams, or wildlife management areas are known to be at the project location (Appendix C).

The Study Area is located within an urban setting and proposed project improvements will primarily occur along the existing US 11 roadway facility which is bounded by railroad, commercial, and residential development. Additional ROW required for roadway widening will include previously developed or disturbed areas. Previous disturbance within the Study Area has resulted in habitat that is mainly early successional stands or young forests, weedy areas, and shrub thickets. Wetlands within the Study Area are a consequence of impoundment from construction of the railroad, US 11, the US 11 bridge embankments, and collector roads.

The Study Area does not likely contain habitat that is suitable to support rare, threatened, or endangered species.

The No Build Alternative would not impact threatened and endangered species or critical habitat.

4.6 Historic and Cultural Resources

Section 106 of the NHPA of 1966 (as amended) protects those properties that are listed in or eligible for listing in the NRHP. In accordance with the requirements of Section 106, an assessment was made of the cultural resources within the Study Area.

Methods used in this review and assessment were consistent with the applicable federal and Louisiana guidelines for conducting cultural and historic resource studies. Project-specific cultural resources data, as well as recorded archaeological sites and historic standing structures, were obtained from a review of archaeological site forms and reports on previous cultural resources surveys on file at the Division of Historic Preservation (DHP) Louisiana Department of Culture, Recreation & Tourism (LDCRT), and the SHPO.

A Phase 1 cultural resources survey of the proposed project area was conducted. The direct Area of Potential Effects (APE), including existing and required ROW for all alternatives, was examined (Figure 12). The direct APE comprised approximately 35 acres. The standing structure survey examined the indirect APE, which included the existing and required ROW including a 0.25-mile buffer to either side of the highway. This area is comprised of approximately 45 acres (Figure 12).

4.6.1 Archaeological Resources

Identification and assessment of potential cultural resources were conducted for the APE and included all areas that could include cultural resources and be directly or indirectly impacted by the proposed project. A geomorphological assessment of the APE was completed in order to determine the potential for the area to have fostered human development or to have been preserved. An overview of the region's prehistory is provided in the *Phase 1 Cultural Resources Survey for the Environmental Assessment of The US 11 Norfolk Southern Railroad, Route US 11, St. Tammany Parish, Louisiana* (2014) which has

been submitted to LDCRT as required under Section 106 of the NHPA.

A cultural resource investigation was completed in order to locate all archaeological remains within the APE and to assess their significance. A records search was conducted at the Division of Archaeology (DOA). The DOA maintains archaeological site information for the State of Louisiana including USGS 7.5-minute quadrangle maps depicting the locations of all recorded archaeological sites, site forms, and corresponding reports. Examination of these records indicates that three archaeological sites exist within the direct APE; however, none of the sites are listed on or are eligible for nomination to the NRHP. The field survey revealed no evidence for intact archaeological deposits based on shovel tests excavated within the APE.

The Build Alternative and No Build Alternative would not impact archaeological resources.

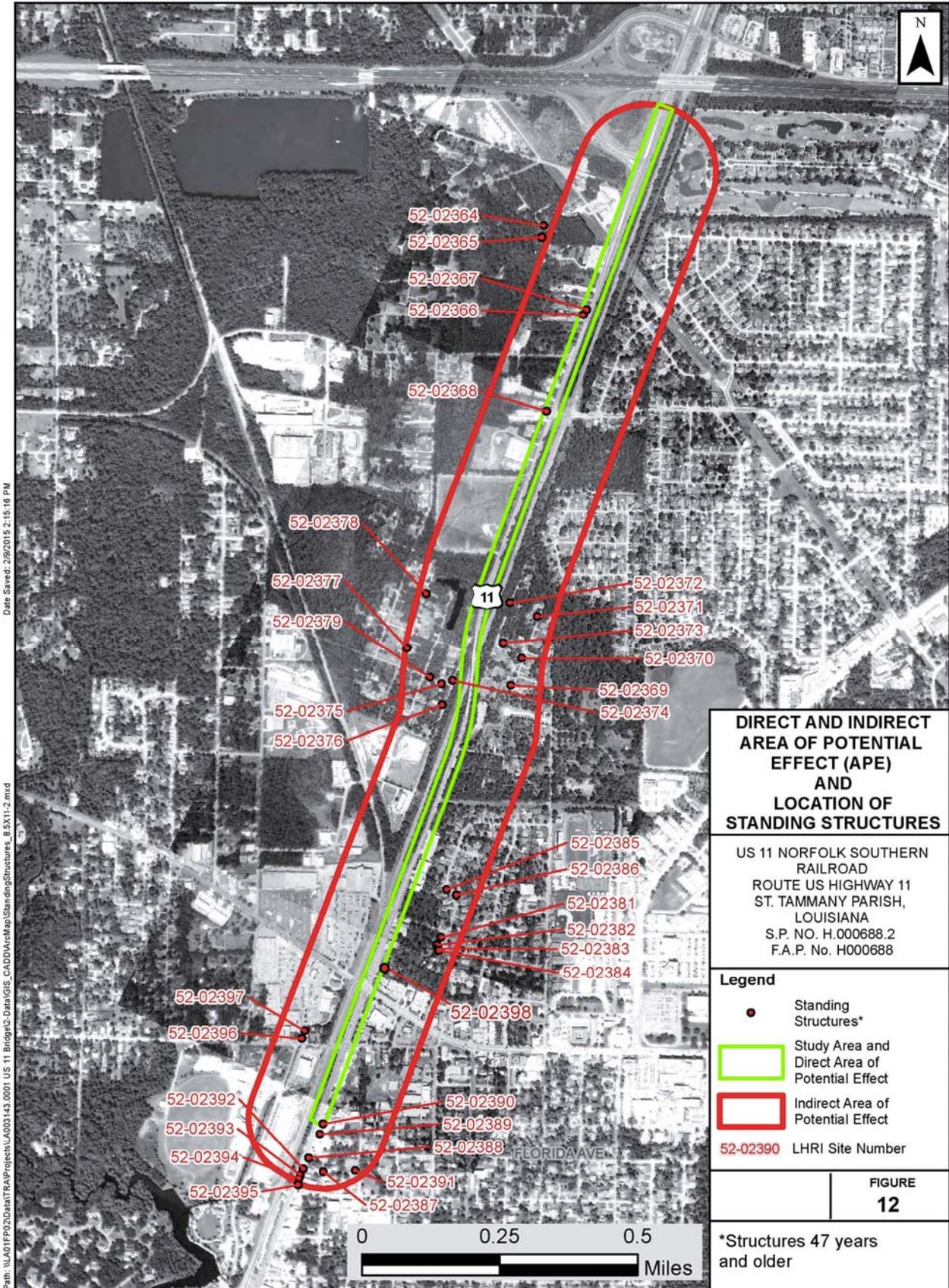
4.6.2 Historic Resources

Identification and assessment of potential historic resources was conducted for the direct and indirect APE (Figure 12) and included all areas that could include historic resources and be directly or indirectly impacted by the proposed project.

Prior to the field survey, ESI conducted a records search at the DHP. The DHP maintains Louisiana



1920s Residence located at 1184 Front Street (US 11) (LHRI 52-02398).



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Historic Resource Inventory (LHRI) and NRHP files for the State of Louisiana. Review of these files indicated that there are no NRHP properties within the indirect APE for the project. All structures that predate 1967 were recorded on LHRI forms and photo documented.

The field survey identified 35 buildings within the indirect APE that are at least 47 years of age or greater (Figure 12). Most of the structures, which are a mix of single-family and commercial buildings, are not individually eligible for nomination to the NRHP or as contributing elements to an NRHP district. Seven structures located within the APE have been recommended as eligible nominations to the NRHP. Five of the structures contribute to the Old Town Slidell historic district and were identified as The Rock Church (52-02392), the Furniture Shop (52-02395), Chateau Bleu (52-00078), the St. Tammany Chamber of Commerce (52-00079), and 208 West Hall Avenue (52-00080). Two structures, the Old Slidell Icehouse (52-02387) and a 1920s residence (52-02398), are located along US 11.

Proposed improvements for the Build Alternative avoid impact to the 1920s residence. An exterior modern handicap ramp attached to the Old Ice House may need to be relocated but should not impact the original building structure.

The No Build Alternative would not impact historic resources.

4.6.3 Aesthetic and Visual Resources

Louisiana's aesthetic and visual resources are an important component of the state's tourism industry and contribute significantly to the quality of life in Louisiana. These resources include a broad range of natural and developed areas from the coastal marshlands and swamps along the Gulf Coast to the rich cotton fields of North Louisiana, from its historic cities and towns to its forestlands and wildlife.

The visual experience and aesthetic quality of an area depend upon the pattern of land or topography, the pattern of water bodies, vegetation, and human development (FHWA 1990). More specifically, factors used to assess a person's visual experience and the aesthetic quality of an area may include:

- Uniqueness of the landscape in relation to the region as a whole;
- Whether the scenic area is a foreground, middle ground, or background view;
- Focus of the view;
- Scale of the elements in the scene;
- Number of potential viewers;
- Duration of the view; and
- Amount of disturbance to the landscape.

The Study Area includes part of a state highway system adjacent to an area that is suburban residential and commercial in character. Human development is visually prominent along US 11 at



Viewshed looking north along US 11 from US 190.

Source: Google Maps ©2014 Google

Gause Boulevard where the architecture is typical late 20th century shopping center and at the US 11 and I-12 interchange with residential development predominantly comprised of 1960s and older style homes.

The viewshed also includes the US 11 bridge over the NS railroad, billboard and business signs,

metal canopies, utility poles, concrete driveways, and parking lots. There would be no change to the nightscape, which is moderately accented with artificial light from shopping centers, fuel stations, street lights, and residential security lights.

The Preferred Alternative would not noticeably change the obscured view of the landscape from ground level.

Temporary construction impacts due to clearing will detract from the view at Addis Boulevard, Indiana Avenue, and Harvey Avenue. Tree growth would restore the current viewshed and partially obscure the build alternative within 15 years. The viewshed throughout the remainder of the Study Area will be minimally disturbed because the widening will be implemented along the existing US 11 alignment. The Preferred Alternative is anticipated to have minimal adverse impacts to the aesthetic and visual resources in the Study Area.

The No Build Alternative would not impact aesthetic and visual resources.

4.7 Section 4(f) and 6(f) Resources

Section 4(f) of the DOT Act of 1966 stipulates that FHWA cannot approve the use of land from publicly owned parks, recreational areas, wildlife and waterfowl refuges, or public and private historical sites, unless there is no feasible and prudent avoidance alternative following all possible planning to minimize harm to the property; or if the use of the land would have only a *de minimis* (very minor and acceptable) impact, or no adverse effect, to key features of the property.

The bridge over the NS railroad was identified as eligible for the NRHP under the Louisiana Historic Bridge Inventory (Mead & Hunt 2013). The bridge was identified as eligible under Criterion A for its association with important "events that have made a significant contribution to the broad patterns of our history". During the 1930s, increased attention was given to creating grade separations between railroads and roadways. Specific federal legislation was passed which

resulted in the U.S. Works Grade Crossing Program funding. The US 11 bridge over the NS railroad was constructed utilizing funds from this program. This information was submitted to the SHPO, who concurred that the existing US 11 Bridge over the Norfolk Southern railroad was eligible for the NRHP. This determination signified that the adverse effect from the potential demolition of the bridge would require compliance with the statewide *Programmatic Agreement Regarding Management of Historic Bridges in Louisiana* (Sept 21, 2015) to demonstrate that FHWA and LADOTD have complied with Section 106 of the NHPA.

Because the bridge cannot be rehabilitated to meet the purpose and need of the project without adversely affecting its historic integrity, it must be taken out of service by demolition and replaced. Requirements to assess whether all possible planning has been considered and whether there is a feasible and prudent avoidance alternative is specified in the *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges* (FHWA 1983). These requirements were applied to ensure that mitigation of adverse effects resulting from replacement of the bridge are considered. The proposed project meets all the applicability criteria specified in the *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges* and may be approved for transportation use based on the programmatic evaluation provided in Appendix A.

Section 6(f) of the Land and Water Conservation Act requires that unavoidable conversion of lands or facilities acquired or developed with Land and Water Conservation Act funds be replaced in kind or coordinated with the Department of Interior. No Section 6(f) lands would be impacted by the proposed project.

The No Build Alternative would not impact parks and public lands.

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4.8 Noise

Noise, by definition, is unwanted sound that interferes with normal activities and would not be considered a resource, but rather a condition that potentially affects both the human and natural environment. Noise is described in terms of loudness, frequency, and duration and is emitted from many sources, including airplanes, factories, railroads, power generating plants, and highway vehicles. Highway noise, or traffic noise, is usually a composite of noises from engine exhausts, drive trains, and tire-roadway interaction.

The magnitude of noise is usually described by its sound pressure. Because the range of sound pressure varies greatly, a logarithmic scale is used to relate sound pressures to some common reference level, particularly the decibel. Sound pressures described in decibels are called sound pressure levels and are often defined in terms of frequency-weighted scales (A, B, C, or D).

For a community noise impact assessment, the A-weighted scale is used almost exclusively in vehicle noise measurements because it places most emphasis on the frequency characteristics that correspond to a human's subjective response to noise (1,000 to 6,000 Hertz). Sound levels measured using A-weighting are often expressed as A-weighted decibels (dBA).

A noise monitoring program was conducted within the Study Area (Appendix CD-3) to establish existing sound levels in accordance with the LADOTD Highway Traffic Noise Policy (2011). Fourteen field-measured noise locations were identified for the collection of existing sound levels along roadways within the Study Area. Existing noise levels ranged from 45.1 to 66.1 dBA. Traffic noise Site ID E2 (collected during traffic peak) had the highest noise level at 66.1 dBA measured north of the intersection of US 11 with Lafayette Street. The lowest noise level measured was 45.1 dBA at traffic noise Site ID C3 (collected during traffic off peak) northwest of the intersection of Ben Thomas Road and Carnation Street.

The dominant noise source at each receiver site is existing traffic including automobiles, heavy trucks, and medium trucks and is usually a composite of noises from engine exhausts, drive trains, and tire/roadway interaction.

Future traffic noise level predictions were performed using the FHWA Traffic Noise Model 2.5 (TNM 2.5). The difference between the field-measured sound levels and TNM-calculated sound levels is within the acceptable range of ± 3 dBA (the amount of sound that is barely perceptible by the human ear) at all locations where existing measurements were taken.

Table 15: Traffic Noise Impact Summary by Alternative

Conditions		Total Number		Approaching or Exceeding LADOTD NAC		Impacted under Substantial Increase Criteria		Total Impacted	
		R	DU	R	DU	R	DU	R	DU
2013 Existing Conditions		199	285	4	4	N/A	N/A	4	4
2042 No Build Conditions		199	285	7	7	0	0	7	7
2042 Build Conditions	Alt 1, Option 1	193	272	10	10	0	0	10	10
	Alt 1, Option 2	193	272	10	10	0	0	10	10
	Alt 1, Option 3	193	272	10	10	0	0	10	10
	Alt 1R, Option 1	193	272	10	10	0	0	10	10
	Alt 1R, Option 2	193	272	10	10	0	0	10	10
	Alt 1R, Option 3	193	272	10	10	0	0	10	10

N/A Not applicable for the listed alternative.

Alt Alternative.

R Receiver.

DU Dwelling Unit.

NAC Noise Abatement Criteria.

Source: ARCADIS U.S., Inc., Noise Analysis Technical Report (July 2014) (Appendix CD-3).

A total of 199 noise receivers (representing a total of 285 dwelling units) were modeled within the Study Area.

As presented in Table 15 and shown on Figure 13, the 2013 existing conditions exterior sound levels at 4 receiver locations approach or exceed the Noise Abatement Criteria (NAC).

In the 2042 Alternative 1 build scenario, growth in traffic volumes and the proposed changes in horizontal and vertical alignment will cause exterior sound levels at 10 receiver locations to approach or exceed the NAC. None of these receiver locations will experience a substantial increase in noise level (Figure 14). The 2042 Alternative 1R build scenario results showed no noticeable differences when compared to Alternative 1 results (Figure 14).

In the 2042 No Build condition, growth in traffic volumes will cause exterior sound levels at 7 receiver locations to approach or exceed the NAC. None of these receiver locations will experience a substantial increase in noise level (Figure 13).

Traffic noise impacts occur when the predicted traffic sound levels equal or exceed the NAC, or when the predicted traffic sound levels exceed existing levels by 10 dBA.

Table 16 describes the LADOTD NAC threshold values that represent the noise level at which abatement measures, like noise walls, must be evaluated.

Noise abatement with vegetative or structural barriers is evaluated for both feasibility and reasonableness. For feasibility, a 5-dBA reduction in noise is considered to be a benefited receptor and at least one benefited receptor must receive an 8-dBA reduction in noise and the average cost per benefited receptor must not exceed \$35,000 to be considered reasonable.

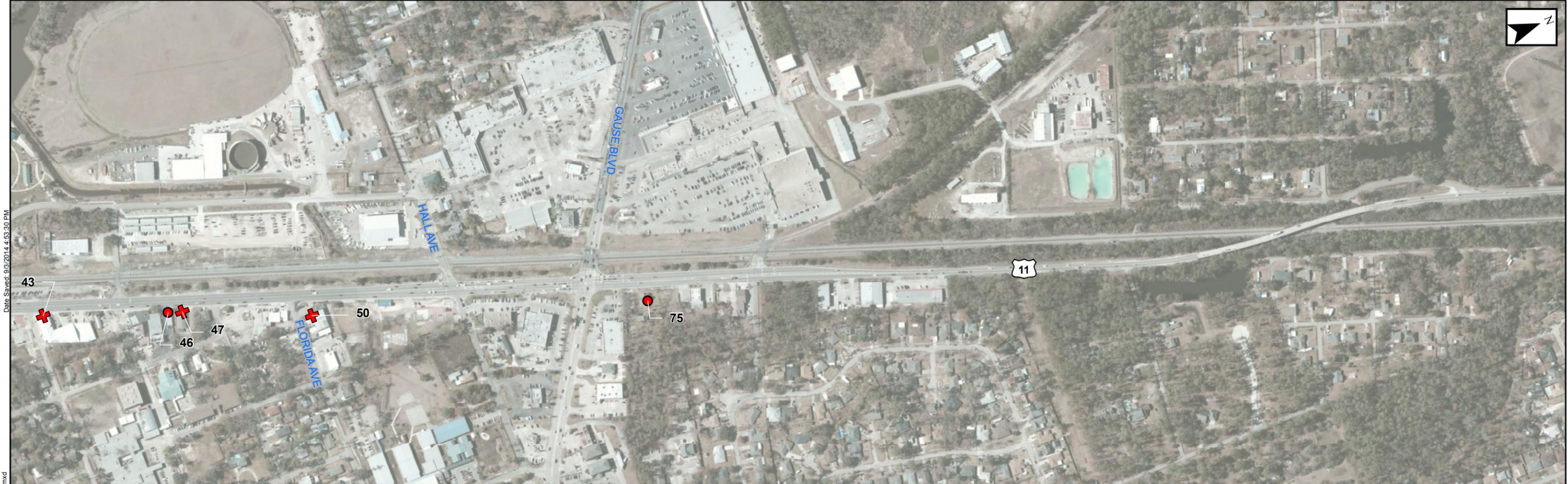
Various noise abatement measures were reviewed to mitigate noise impacts and protect public health in the vicinity of the proposed project. All impacted receivers were reviewed in detail for noise abatement. The types of abatement considered include: acquisition of right-of-way and land use designations, traffic management strategies, alignment alterations, and use of

Table 16: Noise Abatement Criteria

Activity Category	Hourly A-weighted Decibels ¹	Activity Category Description
A	56 (exterior)	Land on which serenity and quiet are of extraordinary significance and serve an important public need, and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	66 (exterior)	Residential.
C	66 (exterior)	Active sports areas, amphitheaters, auditoriums, campgrounds, cemeteries, daycare centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails, and trail crossings.
D	51 (interior)	Auditoriums, daycare centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios.
E	71 (exterior)	Hotels, motels, offices, restaurants/bars, and other developed land, properties, or activities not included in A through D or F.
F	-	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, shipyards, utilities (water resources, water treatment, electrical), and warehousing.
G	-	Undeveloped land that is not permitted.

¹Hourly A-weighted equivalent noise level in dBA - L_{eq} (hour).
Source: LADOTD Noise Policy.

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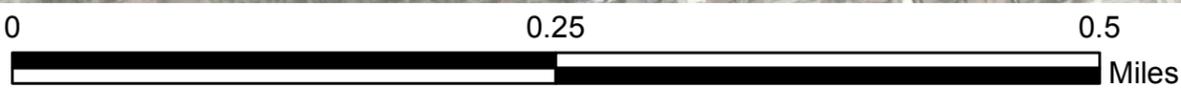


Legend

- 2042 Impact
- + 2013 Impact & 2042 Impact

**NOISE RECEIVER IMPACTS, 2013
EXISTING CONDITIONS AND
2042 NO BUILD
FIGURE 13**

US 11 NORFOLK SOUTHERN RAILROAD
ROUTE US HIGHWAY 11
ST. TAMMANY PARISH, LOUISIANA
S.P. NO. H.000688.2 F.A.P. No. H000688

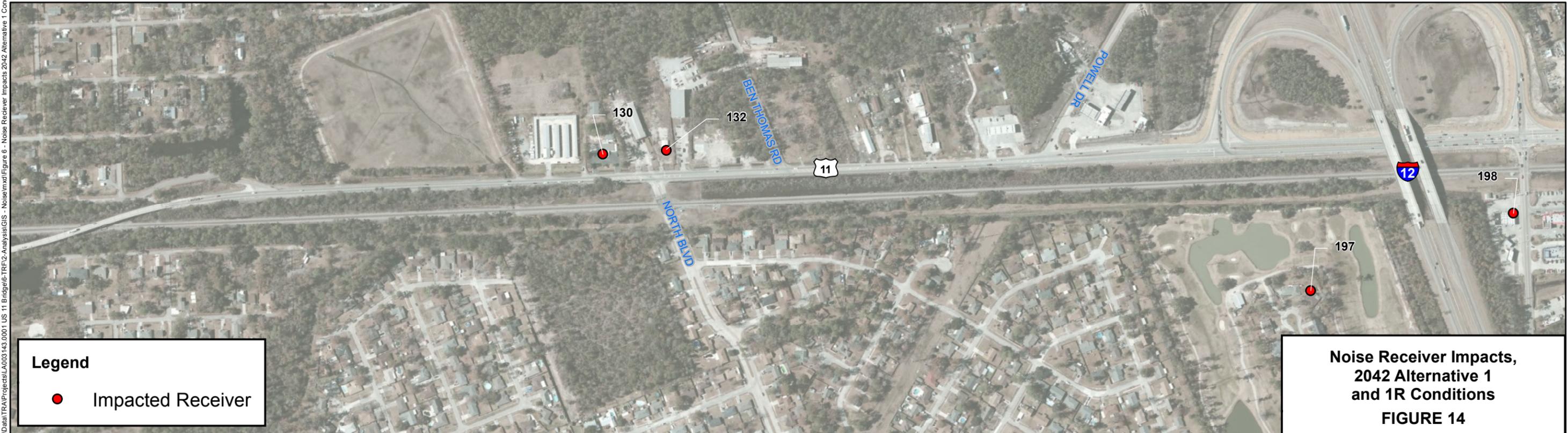
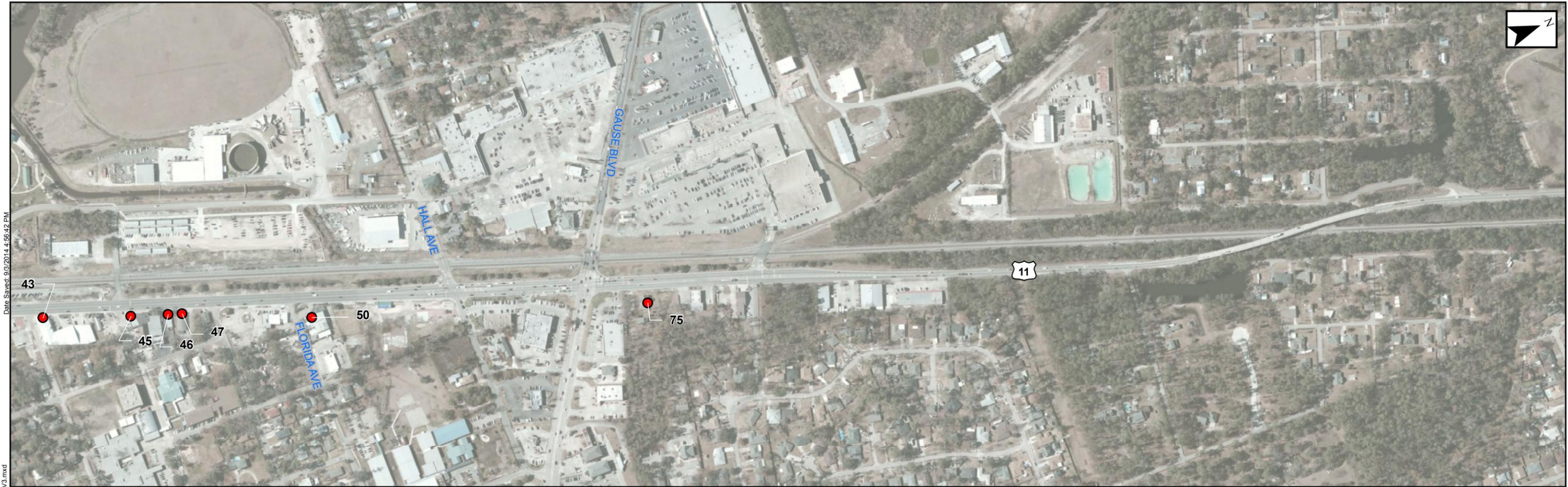


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Path: \\LA01FP0202\Data\TRA\Projects\LA003143\0001 US 11 Bridge\6-TRF\2-Analysis\GIS - Noise\mxd\Figure 4 - Noise Receiver Impacts 2013 and 2042 Conditions\3.mxd

Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, i-cubed, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

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- Impacted Receiver



**Noise Receiver Impacts,
2042 Alternative 1
and 1R Conditions**
FIGURE 14
US 11 NORFOLK SOUTHERN RAILROAD
ROUTE US HIGHWAY 11
ST. TAMMANY PARISH, LOUISIANA
S.P. NO. H.000688.2 F.A.P. No. H000688

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Path: \\LA01FP0202\Data\TRA\Projects\LA003143\0001 US 11 Bridge\6-TRF\2-Analysis\GIS - Noise\mxd\Figure 6 - Noise Receiver Impacts 2042 Alternative 1 Conditions\3.mxd

Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, F-cubed, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

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vegetative or structural barriers. None of the abatement measures considered were considered to be feasible. Land use in the area is already established. Traffic management cannot be enforced along this route due to its intended use as a principle arterial. Alignment alterations is not feasible due to the nature of this established roadway in the project area. Barriers were not feasible due to impacts to driveways along the corridor. Any barrier constructed providing a reasonable noise reduction would prevent access to the properties they shield. Noise barriers were also not considered reasonable due to the generalized cost of the structures. Results of the noise abatement analyses are included in Appendix CD-3.

4.9 Air Quality

The Clean Air Act Amendments of 1990 (CAAA) requires that a proposed project not cause any new violation of National Ambient Air Quality Standards (NAAQS), or increase the severity of existing violations, or delay attainment of NAAQS. National and state ambient air quality standards, developed for specific (criteria) pollutants to protect public health, safety, and welfare, are established in the CAAA.

The U.S. Environmental Protection Agency (USEPA) and Louisiana Department of Environmental Quality (LDEQ) are responsible for the protection of air quality within Louisiana. The USEPA established NAAQS for six air pollutants: carbon monoxide (CO), lead (Pb), nitrogen dioxide (NO₂), sulfur dioxide (SO₂), ozone (O₃), and particulate matter of 10 microns (PM-10) or less in size. NAAQS requires the transportation sector to meet specified standards for PM-10, CO, and ozone at ground level. Unlike PM-10 and CO, ozone is not directly emitted, but created by a chemical reaction between nitrogen oxides (NO_x) and volatile organic compounds (VOCs) in the presence of sunlight. Ground-level ozone is the primary component of smog.

Air quality is defined by primary standards which refer to air quality levels required to protect public health within an adequate margin of safety.

Secondary standards refer to air quality levels required to safeguard visibility, comfort, animals, and property from poor air quality. The CAAA requires that transportation plans, programs, and projects funded or approved by FHWA be in conformity with the State Implementation Plan (SIP), which represents the state's plan to either achieve or maintain the NAAQS for a particular pollutant.

Transportation conformity is a process required of MPOs, pursuant to the CAAA, to ensure that federal funding and approval are given to those transportation activities that are consistent with air quality goals. As the agency responsible for regional transportation planning, the RPC leads the analysis of the region's transportation sector impacts to air quality. Currently, the Greater New Orleans region is designated as an area in attainment. St. Tammany Parish, the City of Slidell, and the Study Area are in "attainment" for the criteria pollutants.

Due to the region's compliance with NAAQS, the RPC is not required to produce an air quality conformity analysis at this time. The region's last air quality conformity analysis was performed in 2004 in conjunction with the development of the 2027 MTP.

There are no air quality impacts for the Preferred Alternative or No Build Alternative.

4.10 Hazardous Materials Sites, Underground Storage Tanks, Pipelines, and Wells

A standard environmental records review and site reconnaissance was conducted to locate sites of potential concern for hazardous materials or previously identified recognized environmental conditions (RECs) on properties within the Study Area. This environmental site assessment was completed utilizing the standard practices outlined in *ASTM International E1527-05: Standard Practice for Environmental Site Assessments: Phase I Environmental Site Assessment Process* in conjunction with 40 CFR Part 312.

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Contamination of soils, groundwater, or surface waters can result from former use, storage, or disposal of hazardous materials on subject properties, or from migration of contaminants from adjacent properties. The purpose of conducting an environmental site assessment is to determine a property's potential for containing soil, groundwater, or surface water contamination with respect to the range of contaminants within the scope of the Comprehensive Environmental Response, Compensation and Liability Act (CERCLA) and petroleum products.

A REC is defined as the presence or likely presence of hazardous substances or petroleum products on a property under conditions that indicate an existing release, a past release, or an observable or obvious threat of a release of hazardous substances or petroleum products into structures on the property or into the ground, groundwater, or surface water of the property, excluding *de minimis* conditions that generally do not present a threat to human health or the environment and that generally would not be the subject of an enforcement action. A historical recognized environmental condition (HREC) is defined as an environmental condition that would have been considered a REC in the past, but may or may not be considered a REC currently.

A records search was conducted by Environmental Data Resources (EDR), Inc. (Appendix CD-4) for the Study Area and immediate surrounding area. Because EDR locates sites based on addresses, which are not always representative of the actual location of a site, the results of the EDR search were supplemented with a review of the LDEQ Electronic Document Management System (EDMS) records (Appendix CD-4). EDMS is LDEQ's electronic repository of official records that have been created or received by LDEQ.

Sites determined to be outside the Study Area were removed from consideration for further investigation because they would not present a material risk of harm to public health or the environment. Other sites listed in the EDR report are considered to represent *de minimis* conditions that generally do not present a material risk of

harm to public health or the environment. These sites were also removed from further investigation.

None of the unmapped sites identified in the EDR report are located within the Study Area and have been removed from further investigation.

In addition, historical aerial photographs, historical topographic maps, and Sanborn® Fire Insurance Maps of the Study Area and adjoining properties were reviewed for evidence of environmental concerns.

Database searches were followed by a field reconnaissance of the Study Area, which also identified sites not documented in the environmental databases. Figure 15 shows potential hazardous materials sites identified from the EDR report and EDMS review that are in proximity to all alternatives.

No HRECs were identified but several RECs were identified within the Study Area. Site No. 1, Jubilee Express Store #4815, and Site No. 9, Circle K #2896, are located within the Study Area. Site No. 8, Western International Gas & Cylinder, is located adjacent to the Study Area.



Circle K located at 1150 Front Street (US 11) north of US 190.

Source: ARCADIS



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The following observation was made regarding Site No. 7, the former McMurray filling station. Historic State Highway Commission plans for the Slidell Overpass indicate the presence of a McMurray filling station located on the east side of US 11 approximately 150 feet south of Indiana Avenue. The location of the area identified as the new entrance on the historic plans is visible and extends east from the current location of US 11. The remainder of the site area is overgrown with vegetation. No other information indicating closure of underground storage tanks (USTs) or other regulated activities was available for review through EDMS.

Required ROW for lane widening along, and intersection improvements associated with, the Preferred Alternative may impact Site Nos. 1, 7, and 9. It is anticipated that UST facilities at Site Nos. 1 and 9 will not be impacted by required ROW although appurtenant piping may be impacted. Due to limited historic information regarding Site No. 7, direct impacts are not known. Caution should be taken during construction-related activities.

The No Build Alternative would not impact sites identified to have known potential environmental conditions that may have the presence or likely presence of hazardous substances or petroleum products or that pose a material threat of release.

Oil and gas and water well information was obtained from the Louisiana Department of Natural Resources (LDNR) Strategic Online Natural Resource Information System (SONRIS) database and a response from the LDNR Office of Conservation (Appendix C). Information collected indicates five active and six plugged and abandoned water wells are located within the Study Area. No recorded oil and gas wells are located within the Study Area. Figure 15 identifies the water wells within the Study Area.

The Preferred Alternative and No Build Alternative would not impact water wells identified within the Study Area.

4.11 Traffic Pattern Changes

Due to the increased length of the proposed bridge crossing the NS railroad, Addis Boulevard and Indiana Avenue will no longer have direct



Figure 16. Addis Boulevard Access to Javery Avenue. Addis Boulevard south to Aviator Street to 4th Street north. Connection from 4th Street to Javery Avenue to Ben Thomas Road to US 11.

access to US 11. Options for access to the Addis Boulevard neighborhood include local roadway improvements to connect Addis Boulevard to 4th Street to Javery Road which intersects with Ben Thomas Road to the north leading to US 11 (Figure 16).

A second option includes local roadway improvements to connect Addis Boulevard to Strawberry Street and then to Carnation Street. Carnation Street offers connections to US 190 to the south or Ben Thomas Road to the north. (Figure 17).

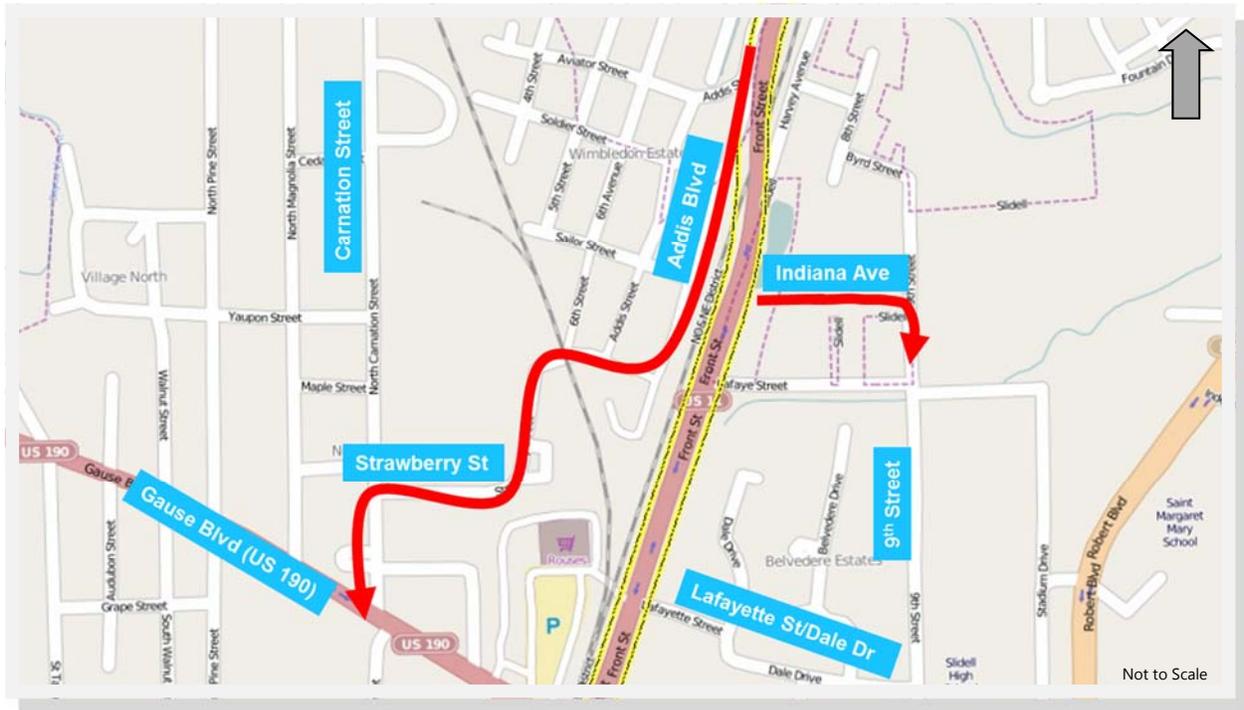


Figure 17. Addis Boulevard Access to Strawberry Street. Addis Boulevard south to Strawberry Street to Carnation Street to US 190. Indiana Avenue to 9th Street via existing roadways to US 11 or US 190.

Indiana Avenue currently exists as a through street from US 11 to 9th Street. The intersection of Indiana Avenue and US 11 is located immediately south of the existing bridge and is signed as a right in/right out intersection. Traffic will continue to utilize 9th Street to access US 11 via Dale Drive/Lafayette Street or US 190.

In November 2013, correspondence in the form of a signed petition was received from residents of the Addis Boulevard neighborhood (Soldier Town) and Javery Road regarding the Addis Boulevard access options. Residents were strongly opposed to the Javery Road connection citing increased traffic and safety concerns for families and children along Javery Road (Figure 16). The

petitioners were in favor of the Strawberry Street Connection (Figure 17).

4.12 Temporary Construction Impacts

Short-term impacts associated with construction of the Preferred Alternative are anticipated including erosion of areas cleared for construction, temporary increases in noise levels, and fugitive dust from use of heavy construction equipment. Temporary impacts to traffic flow and travel patterns are anticipated with construction of the Preferred Alternative. These impacts would occur along existing roads and at intersections during construction activities. The Preferred Alternative would impact traffic flow along US 11

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and US 190. Local and through traffic would be maintained during construction in accordance with LADOTD's *Standard Specifications for Roads and Bridges*.

Utilization of maintenance of traffic flow practices including construction sequencing, timing, and signing would be implemented.

Because much of the proposed work will take place adjacent to high-volume roadways, worker and motorist safety is of paramount concern. Traffic control standards will be used to establish and maintain a safe work zone. Workers are required to meet LADOTD standards for worker visibility and equipment driven on roadways must meet proper signage and licensing requirements. The contractor will take appropriate measures to prevent, minimize, and control the spill of hazardous materials in the construction area.

The use of construction equipment within sensitive areas should be minimized and all construction materials used for this project should be removed as soon as the work schedule permits. Any unanticipated hazardous materials and/or petroleum contamination encountered during construction should be handled according to applicable federal and state regulations for handling emergency discovery of hazardous materials.

By adopting the safety and coordination efforts described above, it is anticipated that the Preferred Alternative could be constructed with no adverse impacts to human health and safety or the environment.

4.13 Indirect and Cumulative Impacts

The Council on Environmental Quality regulations (40 CFR Parts 1500 through 1508) define three

types of impacts routinely assessed for proposed federal actions. Direct impacts, which are effects caused by the action and occur at the same time; indirect impacts which are caused by an action and are later in time or farther removed in distance, but reasonably foreseeable; and cumulative impacts. Cumulative impacts include the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions which may become substantial in the aggregate as time passes.

NEPA requires that the effects of the proposed project be considered in combination with effects from unrelated past, present, and reasonably foreseeable future actions as part of the decision-making process.

The Preferred Alternative would convert a small amount of previously disturbed undeveloped land into transportation use. This will improve accessibility and may induce further residential and commercial development within or near the Study Area. Future development could cause additional loss of natural resources from development, and it is reasonable to predict that land values adjacent to improvements may increase.

Additional planned developments are reasonably expected to occur near the Study Area and would be considered a foreseeable action. Future planned development is reasonably expected to occur under either the Preferred Alternative or No Build Alternative and will have corresponding development effects to the social, natural, and cultural environments within the Study Area.

Predominant cumulative effects from construction of the Preferred Alternative include change in land use and growth in traffic through the Study Area.





SECTION 5 Coordination & Public Involvement

In This Section..... Participation in the decision making process included community leaders, federal and state agencies, Native American Tribes, and the public. Outreach milestones include:

- ◆ Solicitation of Views
- ◆ Public Outreach
- ◆ Community Leaders
- ◆ Native American Tribal Outreach
- ◆ Agency Coordination

5.1 Introduction

Community leaders, federal and state agencies, Native American Tribes, and the public were invited to participate in the decision making process for this project. The outreach program is intended to initiate and continue discussion with stakeholders throughout the study duration. Outreach efforts including meeting dates, times, and locations and summaries of events are discussed below.

5.2 Solicitation of Views

The SOV process is designed to inform interested agencies and persons of the proposed project and request early comments regarding potential adverse economic, social, or environmental effects or other related concerns. Federal, state, and local agencies were invited to participate in the SOV process. LADOTD invited Federal tribes to participate in the SOV process. An SOV packet, including a project overview and figure of the Study Area boundaries, was mailed to various federal, state, and local agencies and Native American tribes requesting their views. In addition to identifying any concerns or issues as mentioned above, consultation to address cultural and historic resource issues pursuant to Section 106 of the NHPA (36 CFR Part 800) was

also requested. The SOV packet and distribution list are included in Appendix C.

5.3 Public Outreach

LADOTD invited Federal tribes to the public officials and key stakeholders meeting at the Slidell City Auditorium on August 22, 2013, from 3:00 p.m. to 4:00 p.m. In addition, letters of invitation reminded the addressees that the public meeting would be held at the same location on the same day from 5:00 p.m. to 7 p.m.

The public was invited to participate in a public meeting on August 22, 2013, held at the Slidell City Auditorium, Slidell, Louisiana, from 5:00 p.m. to 7:00 p.m. The purpose of the public meeting was to present an overview of the project, present the preliminary alternatives and intersection configurations, and give the public an opportunity to provide comments that will help the study team in the evaluation of alternatives. In addition, the meeting was an opportunity for any interested parties to request participation in Section 106 of the NHPA consultation to address cultural and historical resource issues related to the proposed project. The meeting handout included the alternatives and a comment form. The public meeting notice was published in The

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Times-Picayune on August 9, and August 16, 2013.

The public meeting was attended by 47 persons registering their attendance on the sign-in sheets. Of these persons, 13 were public officials, 19 were members of the public, 10 were representatives from LADOTD and FHWA, and 8 were members of the project consultant team. Four verbal comments were recorded by the transcriber at the public meeting, and five written comments were received through the close of the comment period on September 2, 2013. Due to the Labor Day Holiday, comments were received through September 3, 2013.

Commenters expressed support for the bridge replacement indicating that it is a needed improvement. Concern was expressed regarding alternate access to, safety of, increased traffic, and emergency response time to the Addis Boulevard neighborhood. Two options for access were presented. One commenter suggested providing both access options.

Additional concern that was expressed included closing Indiana Avenue and re-routing traffic traveling northbound on US 11 along 9th Street. Questions were asked regarding the location of median openings, traffic patterns, and impacts to business.

The construction schedule and duration of detours were issues of public concern, and two commenters asked if bike lanes would be included along the roadway.

A public meeting summary and transcript of verbal comments received was prepared for the August 22, 2013, public meeting. The summary includes a discussion of the public meeting events, attendance, comments, and outreach following the public meeting. A description of the

meeting format, copies of handouts, meeting sign-in sheets, and written comments received by the close of the comment period, September 2, 2013, are appended to the summary. The public meeting summary was distributed to federal and state agencies and local governments. The full record of this public meeting is available at LADOTD Headquarters in Baton Rouge.



Welcome Slide from the August 22, 2013 public meeting.
Source: ARCADIS

Following the August 22, 2013 Public Meeting, correspondence in the form of a signed petition was received from residents of the Addis Boulevard neighborhood (Soldier Town) and Javery Road regarding the Addis Boulevard access options. Residents were strongly opposed to the Javery Road connection citing increased traffic and safety concerns for families and children along Javery Road. The petitioners were in favor of the Strawberry Street Connection.



SECTION 6

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APPENDIX A

Programmatic Section 4(f) Evaluation and Approval for
FHWA Projects that Necessitate the Use of Historic Bridges

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**U.S. Department of Transportation
Federal Highway Administration**

**Programmatic Section 4(f) Evaluation and Approval for
FHWA Projects that Necessitate the Use of Historic
Bridges**

**US 11 Norfolk Southern Railroad
Route US Highway 11, St. Tammany Parish, Louisiana
State Project No. H.000688.2
Federal Aid Project No. H000688**

Introduction

This Section 4(f) programmatic evaluation has been completed for the US 11 Norfolk Southern Bridge, US 11, St. Tammany Parish, Louisiana, project in accordance with *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges* (Federal Highway Administration [FHWA] 1983). This approval is made pursuant to Section 4(f) of the Department of Transportation Act of 1966, 49 United States Code (USC) 303, and Section 18(a) of the Federal-Aid Highway Act of 1968, 23 USC 138.

This document has been prepared to demonstrate the following:

- There are no feasible and prudent alternatives to the use of the US 11 Bridge over the Norfolk Southern railroad.
- The project includes all possible planning to minimize harm resulting from the use of the US 11 Bridge over the Norfolk Southern railroad.
- The project meets the applicability criteria for the programmatic Section 4(f) evaluation for projects that necessitate the use of historic bridges, issued by FHWA.

Use

Even though the US 11 Bridge over the Norfolk Southern railroad is eligible for inclusion on the National Register of Historic Places (NHRP), it must perform as an integral part of the modern transportation system. When unable to maintain system continuity and integrity, the result is bridge rehabilitation or replacement. For this programmatic Section 4(f) evaluation, the proposed action will “use” a bridge that is eligible for inclusion on the NHRP and the historic integrity of the bridge will be impaired by demolition.

Applicability

This programmatic Section 4(f) evaluation may be applied by FHWA to the proposed project because it meets the following criteria:

1. The bridge is to be replaced or rehabilitated with federal funds.
2. The project will require the use of a historic bridge structure which is on or is eligible for listing on the NRHP.
3. The bridge is not a National Historic Landmark.
4. The FHWA Division Administrator has determined that the facts of the project match those set forth in this document.
5. Agreement among the FHWA, the State Historic Preservation Officer (SHPO), and the Advisory Council on Historic Preservation (ACHP) has been reached through procedures pursuant to Section 106 of the National Historic Preservation Act (NHPA; FHWA 1983).

Project Purpose and Need

The purpose of the project is to address safety features of the bridge crossing the NS railroad and system reliability of the roadway segment from US 190 north to Powell Drive. The project is needed in order to upgrade the bridge and roadway segment to current design standards for travel lane and shoulder widths, bridge width, stopping sight distances, and design speed, as well as system reliability. Roadway segment and bridge improvements will also provide better accommodations for bicycle users and will serve the surrounding community and the larger metropolitan area.

Description of Section 4(f) Property

The US 11 bridge that crosses the Norfolk Southern railroad was built in 1937 to bridge and roadway standards in place at that time. The bridge spans 828 feet and was constructed with a concrete deck and steel I-beams. The substructure is composed of concrete piers and bents, and the approaches are constructed on embankment. The existing bridge guardrails are concrete posts and rails. Additional guardrails were installed to the interior of the existing guardrail as part of a recent overlay project.

A March 2010, Louisiana Department of Transportation and Development (LADOTD) Bridge Inspection Report provides an evaluation of the superstructure and substructure according to National Bridge Inspection (NBI) condition ratings. The general condition ratings are an overall assessment of the physical condition of the deck, superstructure, substructure, and culverts and range from 0 (failed condition) to 9 (excellent condition). The superstructure includes the load-carrying members such as beams or girders that support the driving surface (deck). The substructure includes abutments, piers, and bents.

The bridge inspection shows that the bridge is structurally deficient. The inspection recorded an overall rating of 4, which indicates poor condition with advanced section loss, deterioration, or spalling. A rating of 4 was recorded for the superstructure and substructure, and a rating of 5 was recorded for the deck because it was recently resurfaced. A bridge is classified as "structurally deficient" with a general condition rating of 4 or less for the deck, superstructure, and substructure.

A further indication of the inadequacy of the US 11 bridge to remain in service is its bridge sufficiency rating. This rating takes many factors into account including structural adequacy and safety, serviceability, functional obsolescence, and essentiality for public use. A sufficiency rating of 80 or below qualifies the bridge for rehabilitation funding. A sufficiency rating of 50 or below qualifies it for replacement funding (FHWA 2006). The most recent bridge sufficiency rating for the US 11 bridge is 16.4, a clear signal that the bridge is ready for replacement.

The US 11 bridge was built to standards that no longer meet minimum American Association of State Highway and Transportation Officials (AASHTO) and LADOTD guidelines. In order to determine geometric adequacy, consideration must be given to the number of travel lanes, roadway width, approach roadway, vertical and horizontal clearances, sight distances, and functional classifications of the roadway. Because the bridge has two 12-foot travel lanes, has no shoulders, and is undivided, it is rated as functionally obsolete. The classification of functional obsolescence does not mean that the bridge is inherently unsafe. It is a term that identifies a bridge as one that does not perform adequately for its current use, which indicates that measures should be taken to improve functionality. Functional obsolescence is also a term that assigns priority status for federal funding for bridge replacement and rehabilitation.

Description of Proposed Action

The Preferred Alternative, as identified in the US 11 Norfolk Southern Railroad Environmental Assessment (EA), distributed in MONTH, 2015, is a new bridge designed to meet current standards located on the alignment of the existing bridge. The design is a fixed three-span structure that accommodates the extreme skew angle and crossing clearance requirements of the existing Norfolk Southern railroad including planned improvements. The minimum vertical clearance above the Norfolk Southern existing rails is 23 feet. The superstructure of the bridge will be supported by reinforced concrete abutments founded on pile supported foundations. The roadway segments north and south of the bridge follow the existing roadway alignment providing widening and improvements to accommodate a four lane divided highway including four 12-foot through-lanes, two 8-foot shoulders, and a 6-foot median.

Alternatives and Findings

In order to determine the applicability of this programmatic Section 4(f) evaluation to the proposed project, each of the following alternatives were considered.

1. Do nothing.
2. Build a new structure at a different location without affecting the historic integrity of the existing bridge, as determined by procedures implementing the NHPA.
3. Rehabilitate the historic bridge without affecting the historic integrity of the structure, as determined by procedures implementing the NHPA.

This list is all-inclusive. No reasonable and feasible alternative was found that would avoid use of the

US 11 Bridge over the Norfolk Southern railroad.

Either rehabilitation or replacement of the existing bridge is necessary to ensure public safety while maintaining system continuity and integrity. Because either rehabilitation or replacement of the bridge will impair the historic integrity of the bridge, it has been concluded that all reasonable and feasible alternatives considered would constitute a transportation use of the bridge.

1. Do Nothing

The Do Nothing alternative is identified in the EA as the No Build Alternative. The National Environmental Policy Act (NEPA) requires that doing nothing be considered during the environmental review process. This alternative was designated as the No Build Alternative, signifying that no new structures or major construction would take place.

In order to determine geometric adequacy, consideration must be given to the number of travel lanes, roadway width, approach roadway, vertical and horizontal clearances, sight distances, and functional classifications of the roadway. Because historic bridges are often geometrically inadequate, the functional classification of the existing bridge and average daily traffic (ADT) count are utilized to evaluate geometric adequacy. The existing bridge is undivided with two 12-foot travel lanes and no shoulders. The existing bridge guardrails are concrete posts and rails. Additional guardrails were installed to the interior of the existing guardrail as part of a recent overlay project.

Currently, the existing bridge is classified as urban arterial. The roadway classification also determines which standards will be applied to the new design. Current standards and guidelines for this functional classification require, at a minimum, 12-foot travel lanes and 8-foot shoulders, as well as adequate sight distances. The recommended design speed for the roadway classification is 45 miles per hour. The vertical clearance of the existing bridge cannot accommodate widening and meet the clearance requirements, both vertical and horizontal, for crossing the Norfolk Southern railroad. Under separate roadway projects, US 11 was widened to four lanes north of the US 11 Bridge from Interstate 12 south to Powell Drive and south of the US 11 Bridge from Lafayette Street to US 190 (Gause Boulevard).

The No Build Alternative does not address the stated deficiencies related to its age and design. For these reasons, it has been determined that doing nothing is not feasible and prudent.

2. Build on New Location without Using the Existing Bridge

Ways to build a new structure at a different location without affecting the historic integrity of the existing bridge were evaluated. Utilization of the existing structure as a parallel bridge adjacent to a new similar structure would not resolve the functional obsolescence and substandard roadway geometric features of the existing bridge. One bridge alternative was located along the existing bridge alignment. This bridge alignment crosses the railroad at a 74-degree angle requiring a 390-foot main span length with bridge approaches partially located on embankment. This new, parallel structure will have increased horizontal and vertical clearance extents due to the existing bridge crossing angle and Norfolk Southern's overhead grade separation design requirements.

If the existing bridge is widened, horizontal and vertical clearance requirements must be met to accommodate the existing Norfolk Southern rail line and planned future tracks. For the existing bridge, the piers could not remain in place and would have to be removed as they would be located within the location of Norfolk Southern's planned future tracks. A parallel bridge cannot be designed to allow the existing bridge to remain in place. In addition, conflicts transitioning from the parallel structure and the existing bridge to the roadway segments north and south of the bridge would result.

A second bridge alignment was developed that crosses the railroad at a 65-degree angle requiring a 250-foot main span length with bridge approaches located on embankment. This second bridge option will not allow for maintenance of traffic due to the location of the new bridge situated over a portion of the existing bridge. As a result, complete removal of the existing bridge would be required prior to construction of the new bridge, resulting in a prolonged detour. In addition, this alignment would require additional right-of-way and result in increased impacts.

Correction of structural and functional deficiencies sufficient enough to meet the purpose and need for the project cannot be met if rehabilitation of the existing US 11 bridge remains with widening. Rehabilitation while meeting current LADOTD roadway design criteria and Norfolk Southern railroad clearance requirements cannot be met.

For these reasons, it was determined that building a new bridge and keeping the existing bridge in place is not a prudent and feasible alternative.

3. Bridge Rehabilitation Preserving Historical Status

Bridge rehabilitation sufficient to allow the bridge to remain in service was considered as the only concept that would not replace the bridge. By keeping the bridge in service as a part of the US 11 transportation network, the bridge would not have to be moved or replaced. However, to remain in service, the bridge deficiencies related to its age and design must be addressed.

Two forms of rehabilitation were considered: rehabilitation to the original condition of the bridge and rehabilitation sufficient to meet the purpose and need for the project. Bridge rehabilitation can be considered feasible and prudent only if both of the following conditions can be met:

- The elements that make the bridge historically significant are preserved.
- Structural and functional deficiencies are addressed.

The US 11 bridge was determined significant for its association with important trends or events that have made significant contribution to the broad patterns of Louisiana history and for its association with federal New Deal-era funding through the U.S. Works Grade Crossing Program. During the eligibility determination process, no evidence was found to indicate the bridge is an important example of bridge design, engineering, or construction and, therefore, the bridge does not demonstrate significance under Criterion C (distinctive design or construction characteristics).

In consideration of the historic nature of the US 11 Bridge structural, functional and environmental issues for rehabilitation, while preserving the historical aspect of the bridge, were applied. These issues include, but are not limited to, span, deck and rail removal and replacement; approach roadwork; and structural repairs. In order to determine geometric adequacy, consideration must be given to the number of travel lanes, roadway width, approach roadway, vertical and horizontal clearances, sight distances, and functional classifications of the roadway. The existing bridge is classified as urban minor arterial. Current standards and guidelines for this functional classification require, at a minimum, 12-foot travel lanes and 8-foot shoulders, as well as adequate sight distances. None of these issues can be addressed through rehabilitation, and the bridge would still be deficient in horizontal and vertical clearance for the existing and planned Norfolk Southern railroad tracks. In addition, the existing piers could not remain in place and would have to be removed as they would be located within the location of Norfolk Southern's planned future tracks.

Rehabilitation to its original condition would maintain the original design of the bridge, but would not sufficiently address stated deficiencies related to its age and design including structural and functional deficiencies. Correction of structural and functional deficiencies would entail removal or replacement of the existing bridge components in order to meet current LADOTD roadway design criteria while meeting Norfolk Southern's clearance requirements.

The existing bridge structure cannot be rehabilitated to meet current design standards. Bridge rehabilitation efforts may improve the sufficiency rating and extend the service life of the bridge but the bridge will still have two 12-foot travel lanes with no shoulders, and will not meet capacity requirements of the current and projected ADT. Therefore, neither form of rehabilitation was determined to be a prudent and feasible alternative.

Measures to Minimize Harm

This programmatic Section 4(f) evaluation and approval may be used only for projects where the FHWA Division Administrator, in accordance with this evaluation, ensures that the proposed action includes all possible planning to minimize harm. The following criteria are applied to make this determination:

1. For bridges that are to be rehabilitated, the historic integrity of the bridge is preserved, to the greatest extent possible, consistent with unavoidable transportation needs, safety, and load requirements;
2. For bridges that are to be rehabilitated to the point that the historic integrity is affected or that are to be moved or demolished, FHWA ensures that, in accordance with Historic American Engineering Record (HAER) standards, or other suitable means developed through consultation, fully adequate records are made of the bridge;
3. For bridges that are to be replaced, the existing bridge is made available for an alternative use, provided a responsible party agrees to maintain and preserve the bridge; and
4. For bridges that are adversely affected, agreement among the SHPO, ACHP, and FHWA is reached through the Section 106 process of the NHPA on measures to minimize harm and those

measures are incorporated into the project. This programmatic Section 4(f) evaluation does not apply to projects where such an agreement cannot be reached.

For the US 11 Bridge over the Norfolk Southern railroad, criteria 2 and 4 are applicable. The existing US 11 Bridge over the Norfolk Southern railroad was recommended eligible for the NRHP under Criterion A as a result of the statewide historic bridge inventory completed in 2013. The SHPO concurred and a Memorandum of Agreement (MOA) between FHWA, LADOTD, and the SHPO has been executed to mitigate the adverse effect of demolishing the bridge. This MOA includes a commitment for recordation of the bridge utilizing HAER standards.

The MOA is the culmination of Section 106 consultation and is documented in the EA with a Finding of No Significant Impact.

Conclusions

As noted in the introduction, the objective of this Section 4(f) programmatic evaluation is to show that the proposed project complies with Section 4(f) of the Department of Transportation Act of 1966 by meeting the following conditions:

- There are no feasible and prudent alternatives to the use of the US 11 Bridge crossing over the Norfolk Southern railroad.
- The project includes all possible planning to minimize harm resulting from the use of the US 11 Bridge crossing over the Norfolk Southern railroad.
- The project meets the applicability criteria for the programmatic Section 4(f) evaluation for projects that necessitate the use of historic bridges, issued by FHWA.

Given the information presented in this Section 4(f) programmatic evaluation and approval, it is concluded that the proposed project meets the above-noted conditions and thereby complies with Section 4(f) of the Transportation Act of 1966.

FEDERAL HIGHWAY ADMINISTRATION

By _____
Charles Bollinger, Division Administrator

Date

References

AASHTO, Standing Committee on the Environment. 2007. Guidelines for Historic Bridge Rehabilitation and Replacement. March.

FHWA. 1983. Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges. Issued on: July 5. Published online at: <http://www.environment.fhwa.dot.gov/4f/4fbridge.asp>.

FHWA. 2006. The Impact of Load Rating Methods on Federal Bridge Program Funding. Attachment 2: Basic Method for Determining if a Structure is Deficient. Non-Regulatory Supplement for Subpart 650, Subpart D. Revised February. Published online at <http://fhwa.dot/bridge/bridgeloading02.cfm>.

DRAFT



APPENDIX B

Design Criteria, Typical Sections, and Plan & Profiles

B-1 Design Criteria

B-2 Typical Roadway and Bridge Sections

B-3 Plan & Profiles, Alternatives 1 and 1R

B-4 Intersection Build Layouts, Options 1, 2, and 3

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APPENDIX B-1

Design Criteria

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UA-2 Design Criteria
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany
State Project No. H.000688.2
Federal Aid Project No. H000688

Item No.	Item	Front Street (US 11) Gause Blvd W (US 190) to Lafayette Street	Front Street (US 11) Powell Drive to Florida Avenue	Front Street (US 11) Powell Drive to I-12
		Southern Terminus	EA Study Segment	Northern Terminus
		UA-2	UA-2	UA-2
1	Design Speed (mph)	45	45	45
2	Level of Service ¹	C	C	C
3	Number of Lanes (minimum)	4	2 (min) – 4 (typ)	4
4	Width of Travel Lanes (ft)	12	11 - 12	12
5	Width of Shoulders (ft)			
	(a) Inside	N/A	N/A	N/A
	(b) Outside	Not Used	8²	10
6	Shoulder Type	Not Used	Paved	Paved
7	Width of Median on Multi-lane Facilities (minimum) (ft)			
	(a) Depressed	N/A	N/A	N/A
	(b) Raised	Not Used	6² - 30	6 -18
	(c) Two-way Left Turn Lane	14	11 – 14 typ³	Not Used
8	Fore Slope (vertical-horizontal)	1:3 – 1:4	1:3 (min) – 1:4 (des)	1:3 – 1:4
9	Back slope (vertical-horizontal)	1:3	1:3	1:3
10	Pavement Cross Slope (%)	2.5	2.5	2.5
11	Minimum Stopping Sight Distance (ft)	360	360	360
12	Maximum Superelevation (%)	Not Used	4	Not Used
13	Minimum Radius (ft) ⁴			
	(a) With Normal Crown (-2.5% cross-slope)	Not Used	1,000⁶	Not Used
	(b) With 2.5% Superelevation	Not Used	750⁶	Not Used
	(c) With Full Superelevation	Not Used	700⁶	Not Used
14	Maximum Grade (%%)	0.5	6	0.5
15	Minimum Vertical Clearance (ft) ⁵	16	16	16
16	Minimum Clear Zone (ft)			
	(a) From edge of through travel lane ⁷	N/A	24	13
	(b) Outside from back of curb (when curb is used)	6	6 (min) – 22 (des)	Not Used
	(c) Median from back of curb ⁸	Not Used	4 (min) – 18 (des)	Not Used
	(d) (when curb is used)			
17	Bridge Design Live Load ⁹	AASHTO	AASHTO	AASHTO
18	Minimum Width of Bridges (face to face of bridge rail at gutter line)			
	(a) Curbed facilities (without sidewalks)	Traveled Way ¹⁰ Plus 8'	Traveled Way ¹⁰ Plus 8'	Not Used
	(b) Shoulder facilities	Not Used	Roadway Width	Roadway Width
19	Guardrail Required at Bridge Ends	Note ¹⁰	Note ¹⁰	Note ¹⁰

- 1 Level of Service D can be used in urban areas.
- 2 Curb may be used in place of shoulders on UA-2 facilities.
- 3 With Chief Engineer's approval, curb offsets may be eliminated and the minimum median width can be reduced to 4 feet. On principal arterials, particularly at intersections, the upper limit should be considered.
- 4 Cannot be used on multilane roadways (with four or more through lanes) without the Chief Engineer's approval.
- 5 It may be necessary to increase the radius of the curve and/or increase the shoulder width (maximum of 12 feet) to provided adequate stopping sight distance on structure.
- 6 At divisional islands a radius of 11,500' and Degree of Curve of 0°30' shall be used (specific to this design speed).
- 7 An additional 6 inches should be added for additional future surfacing.
- 8 Applies to facilities with shoulders. Refer to Roadside Design Guide when 1:3 fore slopes are used or for slopes flatter than 1:4.
- 9 Where left turn lanes are provided or where the median is less than 6 feet in width, the minimum clearance will be 1.5 feet from back of curb. For median slopes steeper than 1:6, a clear zone as outlined in the AASHTO Roadside Design Guide shall be provided.
- 10 LRFD for bridge design.
- 11 Refer to EDSM II.3.1.4 when sidewalks will be provided and for guardrail requirements.

Structural Design Criteria US 11 Norfolk Southern Railroad EA (H.000688)

Reference Publications	
State of Louisiana Department of Transportation and Development (LADOTD) Bridge Design Manual	4th Edition, 2005
State of Louisiana Department of Transportation and Development (LADOTD) LRFD Bridge Design Manual	1 st Edition, September 17, 2008
AASHTO Standard Specifications for Highway Bridges	17th Edition
AASHTO LRFD Bridge Design Specifications	6th Edition, 2012
American Railway Engineering and Maintenance-of-Way Association, Manual for Railway Engineering	2010
Norfolk Southern – Guidelines for Design and Construction of Privately Owned Industry Tracks	2011
American Society of Civil Engineers, Minimum Design Loads for Buildings and Other Structures, ASCE 7	2010
American Institute of Steel Construction (AISC), Manual of Steel Construction, Load and Resistance Factor Design (LRFD)	13 th Edition
American Concrete Institute (ACI), Building Code Requirements for Reinforced Concrete, ACI 318	2011
American Welding Society, Structural Welding Code, Steel, AWS D1.1/D1.1M	2010
Bridge Welding Code, AWS D1.5/D1.5M	2010
Bridge Type & Specifications	
Span Length	Roughly 250 feet. Length may change later.
Approach Spans Girder Types	AASHTO Types II, III, IV & Bulb-T based on the span lengths.
Main Span Girder Options	Steel plate girder, concrete spliced girders, or bulb-T girders (depends on right-of-way accessibility within the existing and proposed Norfolk Southern tracks).
Bridge Deck	Cast-in-situ concrete slab.
Barrier Type	Concrete F Shape PL-2.
Approach Span Substructure	Concrete bents founded on piles.
Main Span Substructure	Concrete piers with spread footing founded on pile clusters/shafts.
Bridge Design Loads	
Governing load based on Louisiana Design Vehicle Live Load 2011 (LADV-11), Lane Load	
Design vehicular speed – 45 mph (UA-2 functional classification)	

Structural Design Criteria US 11 Norfolk Southern Railroad EA (H.000688)

Reference Publications	
State of Louisiana Department of Transportation and Development (LADOTD) Bridge Design Manual	4th Edition, 2005
State of Louisiana Department of Transportation and Development (LADOTD) LRFD Bridge Design Manual	1 st Edition, September 17, 2008
AASHTO Standard Specifications for Highway Bridges	17th Edition
AASHTO LRFD Bridge Design Specifications	6th Edition, 2012
American Railway Engineering and Maintenance-of-Way Association, Manual for Railway Engineering	2010
Norfolk Southern – Guidelines for Design and Construction of Privately Owned Industry Tracks	2011
American Society of Civil Engineers, Minimum Design Loads for Buildings and Other Structures, ASCE 7	2010
American Institute of Steel Construction (AISC), Manual of Steel Construction, Load and Resistance Factor Design (LRFD)	13 th Edition
American Concrete Institute (ACI), Building Code Requirements for Reinforced Concrete, ACI 318	2011
American Welding Society, Structural Welding Code, Steel, AWS D1.1/D1.1M	2010
Bridge Welding Code, AWS D1.5/D1.5M	2010
Bridge Type & Specifications	
Span Length	Roughly 250 feet. Length may change later.
Approach Spans Girder Types	AASHTO Types II, III, IV & Bulb-T based on the span lengths.
Main Span Girder Options	Steel plate girder, concrete spliced girders, or bulb-T girders (depends on right-of-way accessibility within the existing and proposed Norfolk Southern tracks).
Bridge Deck	Cast-in-situ concrete slab.
Barrier Type	Concrete F Shape PL-2.
Approach Span Substructure	Concrete bents founded on piles.
Main Span Substructure	Concrete piers with spread footing founded on pile clusters/shafts.
Bridge Design Loads	
Governing load based on Louisiana Design Vehicle Live Load 2011 (LADV-11), Lane Load	
Design vehicular speed – 45 mph (UA-2 functional classification)	



APPENDIX B-2

Typical Roadway and Bridge Sections

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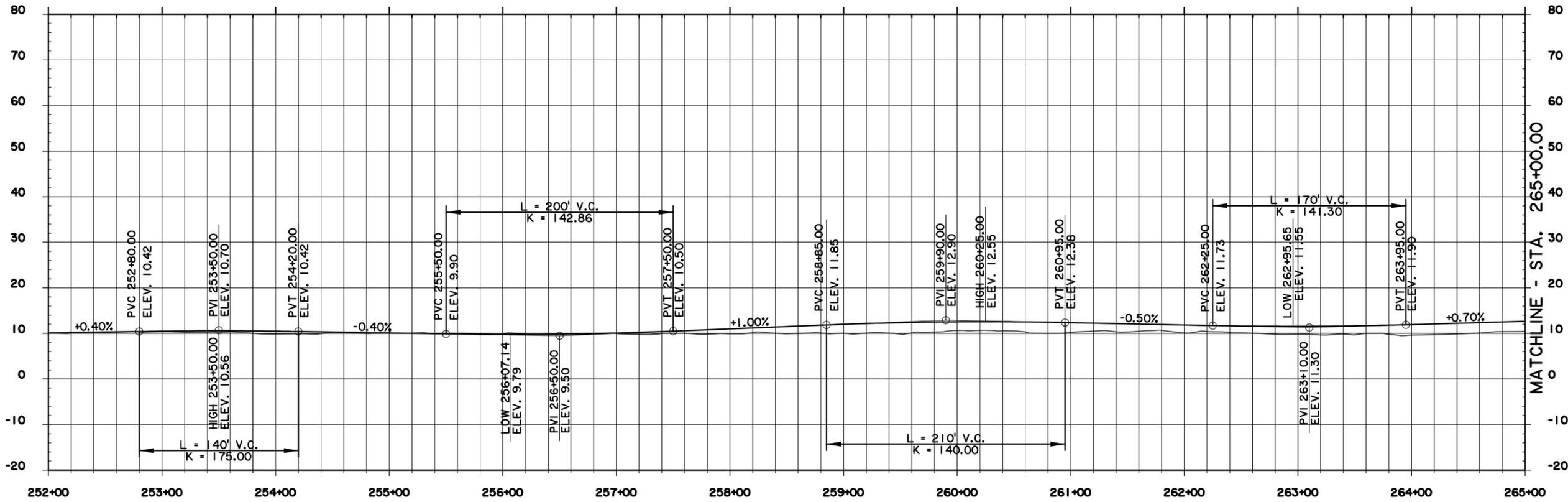
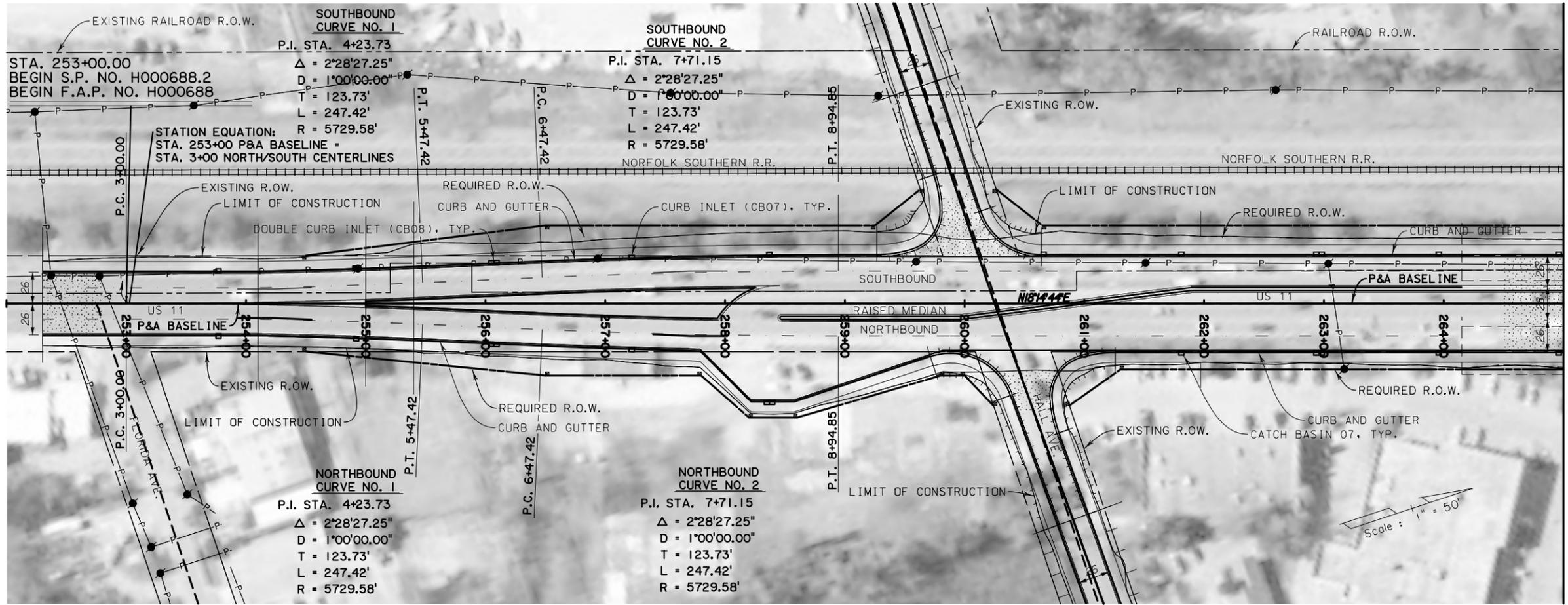


APPENDIX B-3

Plan & Profiles

Alternatives 1 and 1R

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MATCHLINE - STA. 265+00.00

DESIGNED	D. FULKS	DATE	05 FEB 2014
CHECKED	L. PORTA	BY	
REVISION	NO.	DATE	
NO.	4	DATE	

ST. TAMMANY
 FEDERAL PROJECT H.000688
 STATE PROJECT H.000688.2

PARISH PROJECT H.000688

DESIGNED D. FULKS
 CHECKED L. PORTA
 REVISION DESCRIPTION

DATE 05 FEB 2014
 SHEET 4

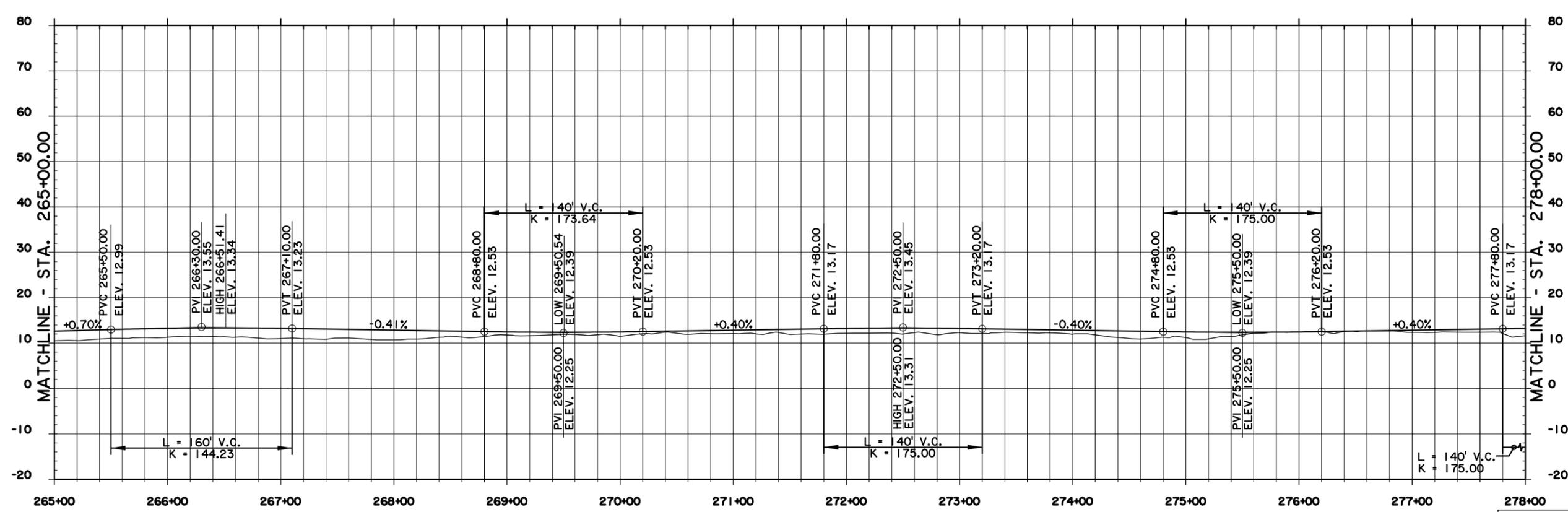
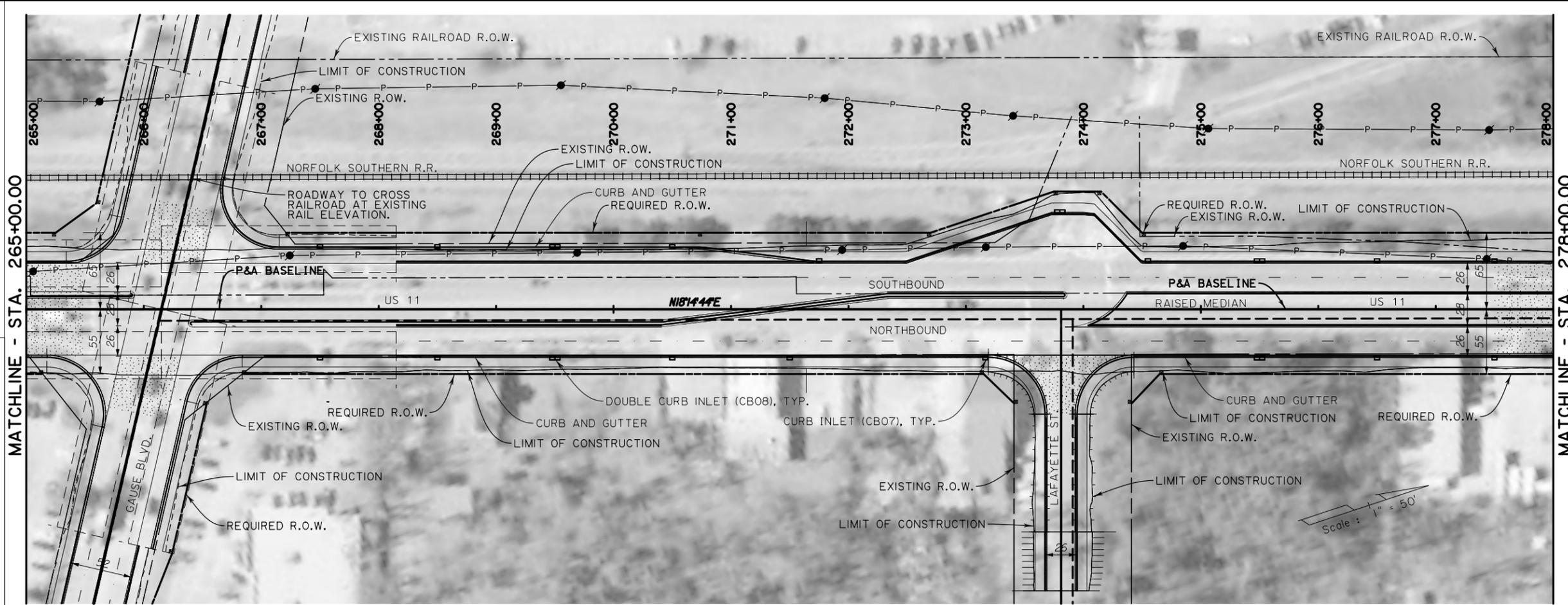
PLAN AND PROFILE
 ALTERNATIVE NO. 1
 STA. 252+00 TO STA. 265+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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LINE AND SYMBOL LEGEND

--- EXISTING HIGHWAY/STREET BASELINE	--- PROJECT & ADOPTED BASELINE
--- EXISTING HIGHWAY RIGHT-OF-WAY	--- REQUIRED CURB AND GUTTER
--- EXISTING RAILROAD TRACK	--- REQUIRED CLEAR ZONE LIMITS
--- EXISTING RAILROAD RIGHT-OF-WAY	--- REQUIRED CONSTRUCTION LIMITS
--- EXISTING OVERHEAD POWERLINE	--- REQUIRED HIGHWAY RIGHT-OF-WAY
--- EXISTING POWERPOLE	--- REQUIRED RIGHT-OF-WAY MARKER
	--- REQUIRED CURB INLET (CB-07)
	--- REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-5

DESIGNED	D. FULKS	PARISH	ST. TAMMANY
CHECKED	L. PORTA	FEDERAL PROJECT	H.000688
DATE	05 FEB 2014	STATE PROJECT	H.000688.2
SHEET	5	REVISION DESCRIPTION	

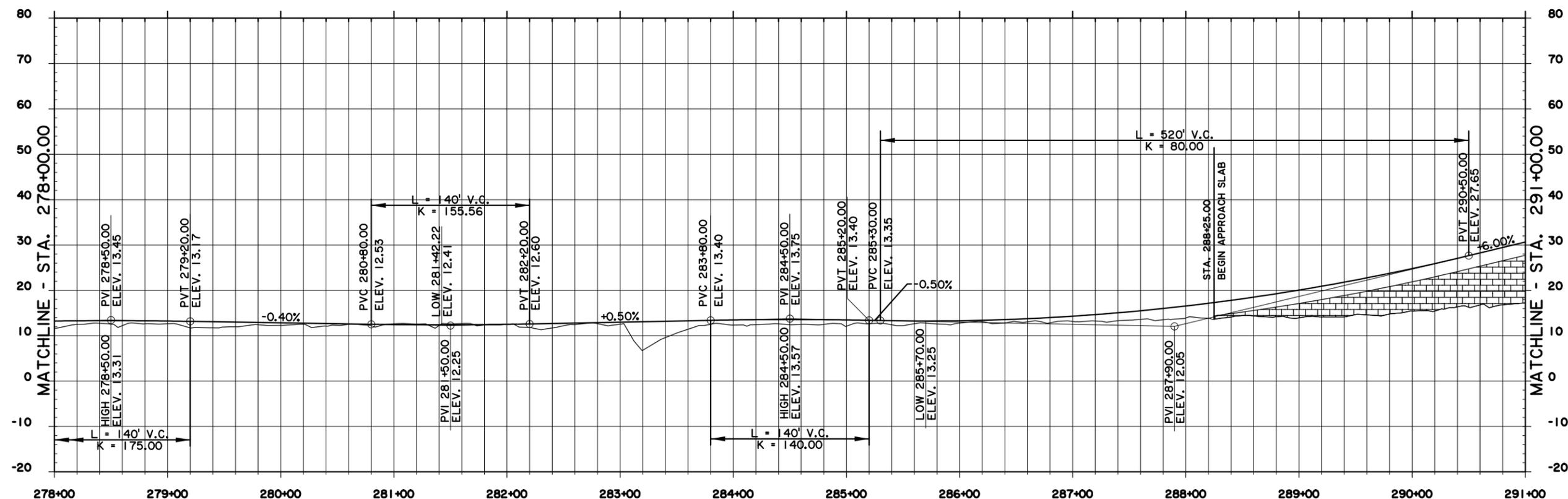
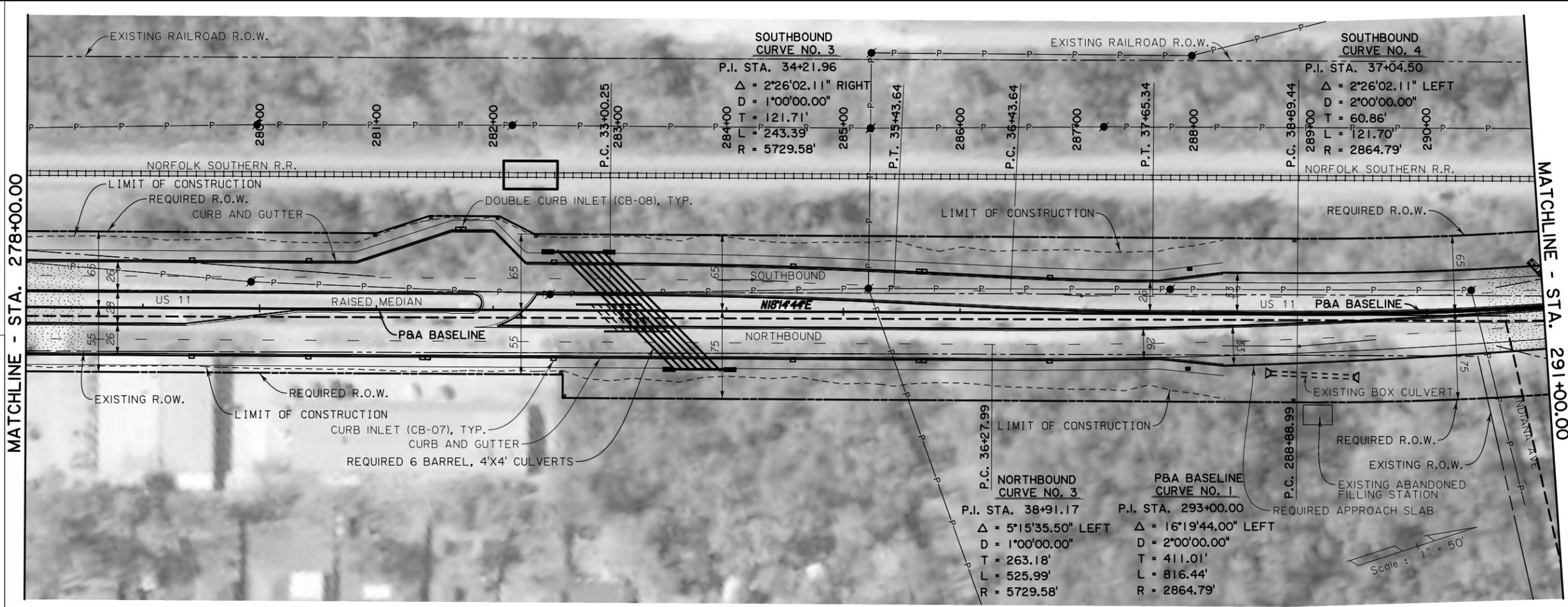
PLAN AND PROFILE ALTERNATIVE NO. 1
STA. 265+00 TO STA. 278+00
US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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LINE AND SYMBOL LEGEND

- EXISTING HIGHWAY/STREET BASELINE
- EXISTING HIGHWAY RIGHT-OF-WAY
- EXISTING RAILROAD TRACK
- EXISTING RAILROAD RIGHT-OF-WAY
- EXISTING OVERHEAD POWERLINE
- EXISTING POWERPOLE
- PROJECT & ADOPTED BASELINE
- REQUIRED CURB AND GUTTER
- REQUIRED CLEAR ZONE LIMITS
- REQUIRED CONSTRUCTION LIMITS
- REQUIRED HIGHWAY RIGHT-OF-WAY
- REQUIRED RIGHT-OF-WAY MARKER
- REQUIRED CURB INLET (CB-07)
- REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-5

PLAN AND PROFILE
 ALTERNATIVE NO. 1
 STA. 278+00 TO STA. 291+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

DESIGNED: D. FULKS
 CHECKED: L. PORTA
 RETAILED: J. HOWARD
 CHECKED: D. FULKS

DATE: 05 FEB 2014
 SHEET: 6

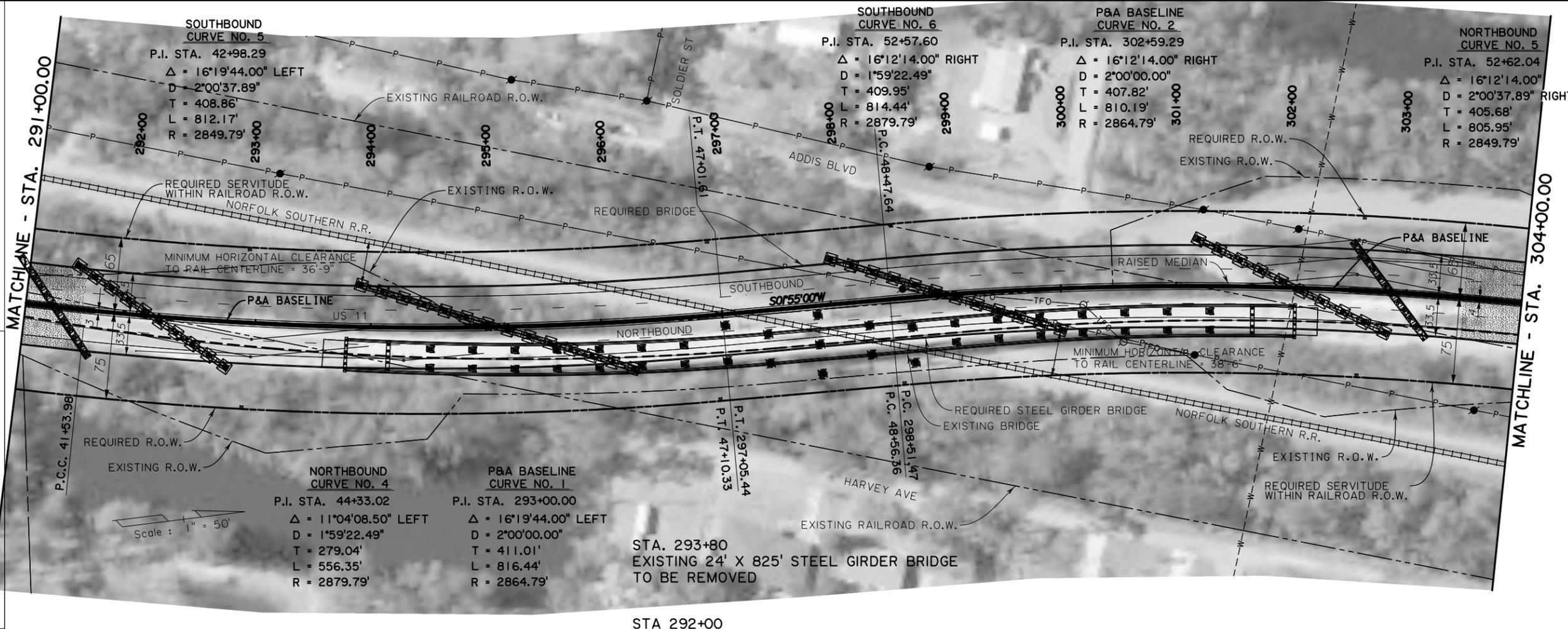
ST. TAMMANY
 FEDERAL PROJECT: H.000688
 STATE PROJECT: H.000688.2

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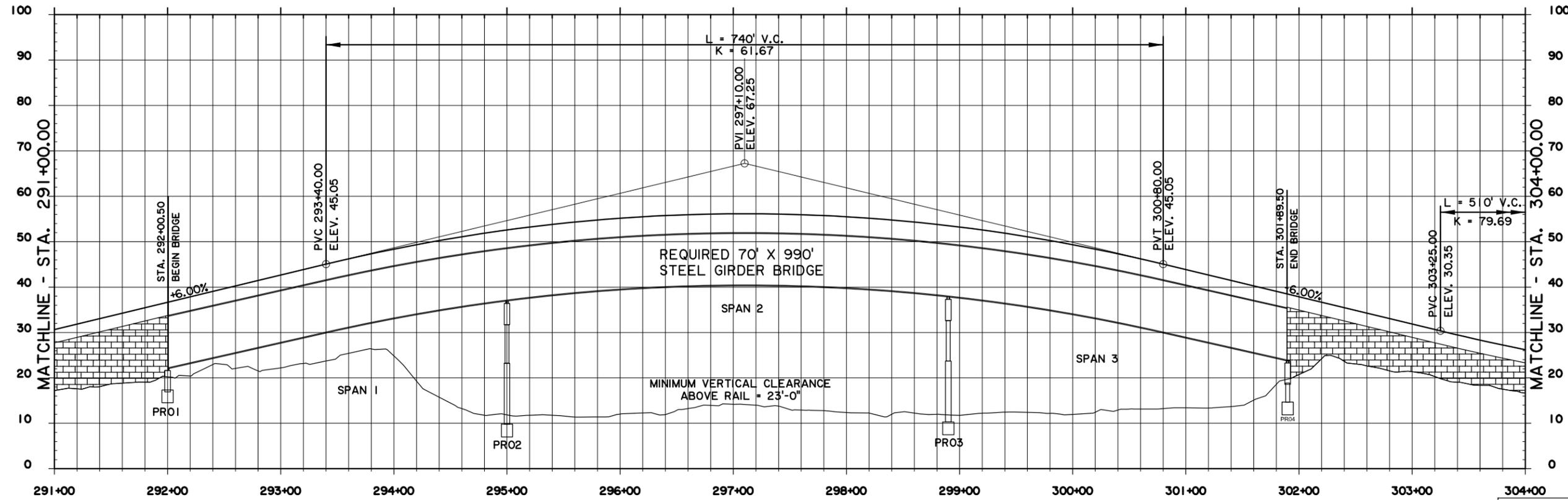
LINE AND SYMBOL LEGEND

- EXISTING HIGHWAY/STREET BASELINE
- EXISTING HIGHWAY RIGHT-OF-WAY
- EXISTING RAILROAD TRACK
- EXISTING RAILROAD RIGHT-OF-WAY
- EXISTING OVERHEAD POWERLINE
- EXISTING POWERPOLE
- PROJECT & ADOPTED BASELINE
- REQUIRED CURB AND GUTTER
- REQUIRED CLEAR ZONE LIMITS
- REQUIRED CONSTRUCTION LIMITS
- REQUIRED HIGHWAY RIGHT-OF-WAY
- REQUIRED RIGHT-OF-WAY MARKER
- REQUIRED CURB INLET (CB-07)
- REQUIRED CURB INLET (CB-08)



STA. 293+80
 EXISTING 24' X 825' STEEL GIRDER BRIDGE
 TO BE REMOVED

STA 292+00
 REQUIRED 70' X 990' STEEL GIRDER BRIDGE



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-5

SHEET NUMBER	7
DESIGNED	D. FULKS
CHECKED	L. PORTA
DATE	05 FEB 2014
BY	
REVISION DESCRIPTION	
NO.	
DATE	
PARISH	ST. TAMMANY
FEDERAL PROJECT	H.000688
STATE PROJECT	H.000688.2

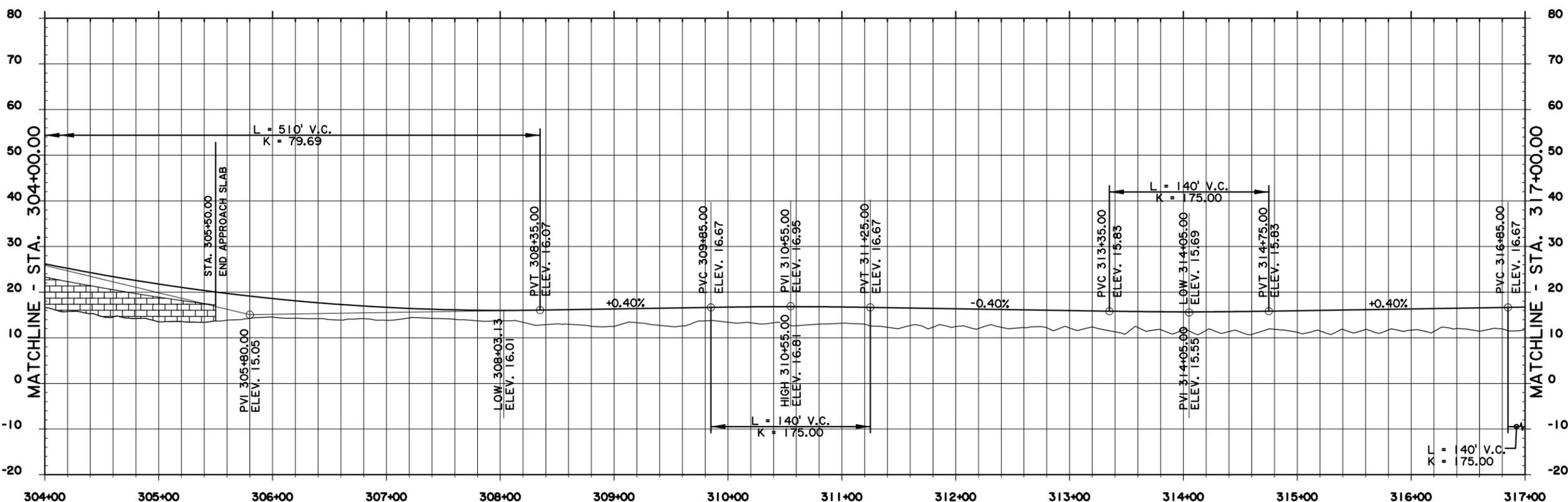
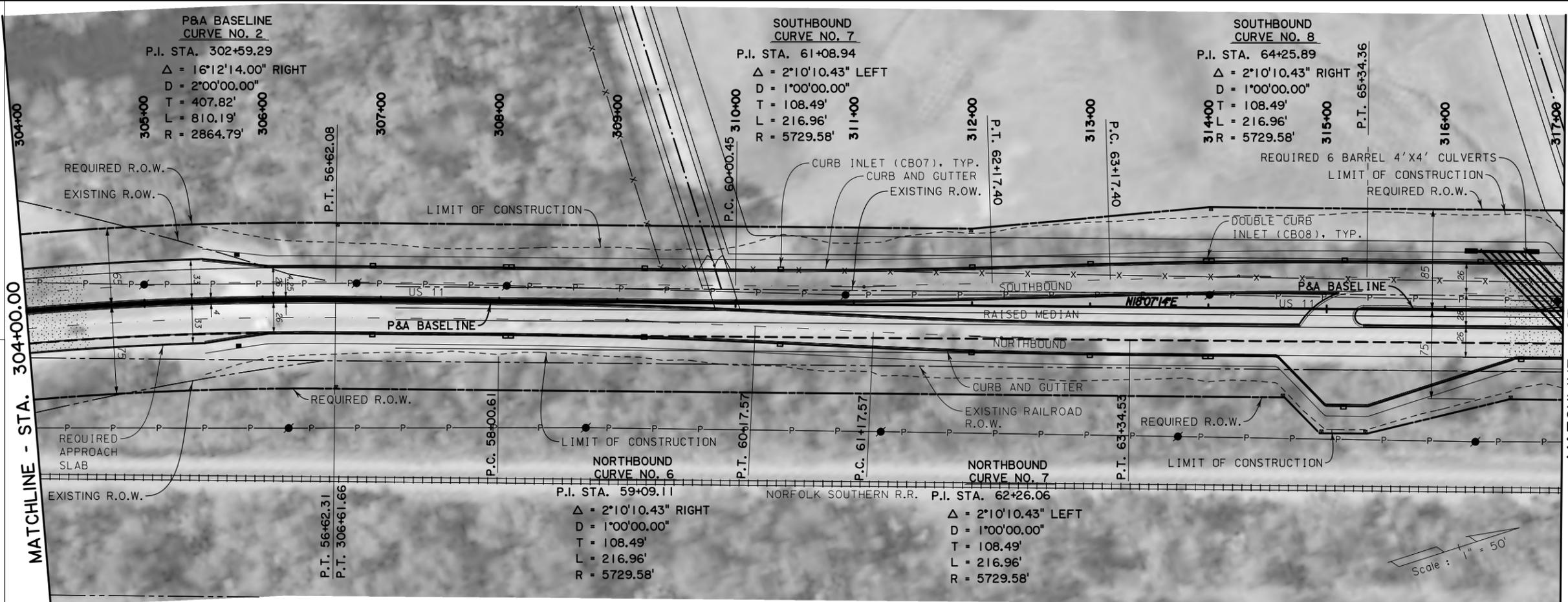
PLAN AND PROFILE
 ALTERNATIVE NO. 1
 STA. 291+00 TO STA. 304+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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LINE AND SYMBOL LEGEND

- EXISTING HIGHWAY/STREET BASELINE
- EXISTING HIGHWAY RIGHT-OF-WAY
- EXISTING RAILROAD TRACK
- EXISTING RAILROAD RIGHT-OF-WAY
- EXISTING OVERHEAD POWERLINE
- EXISTING POWERPOLE
- PROJECT & ADOPTED BASELINE
- REQUIRED CURB AND GUTTER
- REQUIRED CLEAR ZONE LIMITS
- REQUIRED CONSTRUCTION LIMITS
- REQUIRED HIGHWAY RIGHT-OF-WAY
- REQUIRED RIGHT-OF-WAY MARKER
- REQUIRED CURB INLET (CB-07)
- REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-5

DESIGNED: D. FULKS	PARISH: ST. TAMMANY	SHEET NUMBER: 0
CHECKED: L. PORTA	FEDERAL PROJECT: H.000688	DATE: 05 FEB 2014
DATE: 05 FEB 2014	STATE PROJECT: H.000688.2	BY: []
CHECKED: D. FULKS	REVISION DESCRIPTION: []	NO. DATE
DATE: 05 FEB 2014	REVISION DESCRIPTION: []	NO. DATE
CHECKED: D. FULKS	REVISION DESCRIPTION: []	NO. DATE
DATE: 05 FEB 2014	REVISION DESCRIPTION: []	NO. DATE
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DATE: 05 FEB 2014	REVISION DESCRIPTION: []	NO. DATE

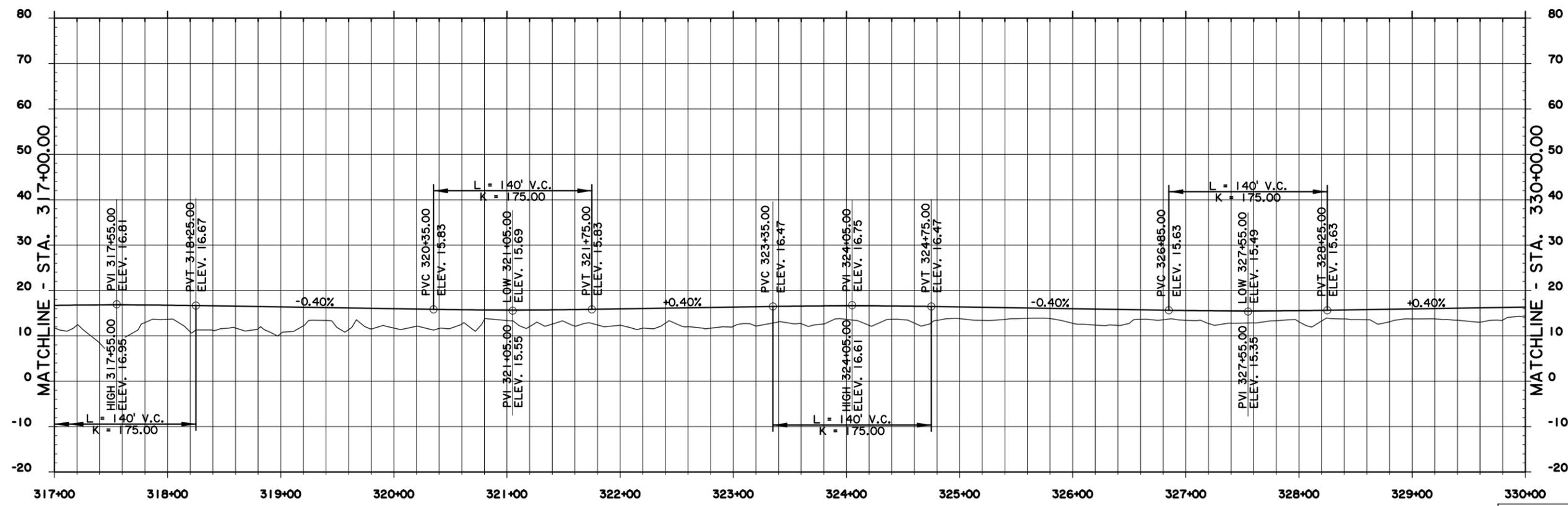
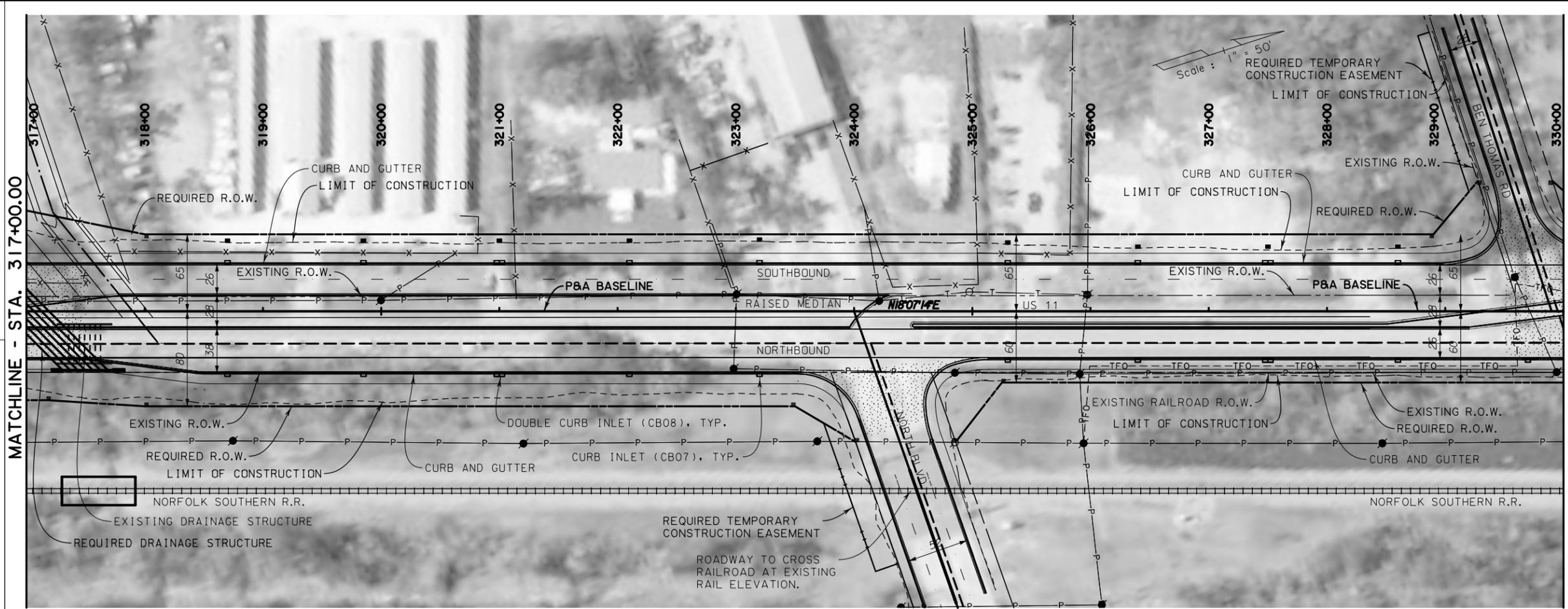
PLAN AND PROFILE
 ALTERNATIVE NO. 1
 STA. 304+00 TO STA. 317+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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LINE AND SYMBOL LEGEND

--- EXISTING HIGHWAY/STREET BASELINE	--- PROJECT & ADOPTED BASELINE
--- EXISTING HIGHWAY RIGHT-OF-WAY	--- REQUIRED CURB AND GUTTER
--- EXISTING RAILROAD TRACK	--- REQUIRED CLEAR ZONE LIMITS
--- EXISTING RAILROAD RIGHT-OF-WAY	--- REQUIRED CONSTRUCTION LIMITS
--- EXISTING OVERHEAD POWERLINE	--- REQUIRED HIGHWAY RIGHT-OF-WAY
● EXISTING POWERPOLE	■ REQUIRED RIGHT-OF-WAY MARKER
	■ REQUIRED CURB INLET (CB-07)
	■ REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-5

DESIGNED: D. FULKS	PARISH: ST. TAMMANY
CHECKED: L. PORTA	FEDERAL PROJECT: H.000688
DATE: 05 FEB 2014	STATE PROJECT: H.000688.2
CHECKED: D. FULKS	
DATE: 05 FEB 2014	
SHEET: 9	

PLAN AND PROFILE
ALTERNATIVE NO. 1
STA. 317+00 TO STA. 330+00
US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

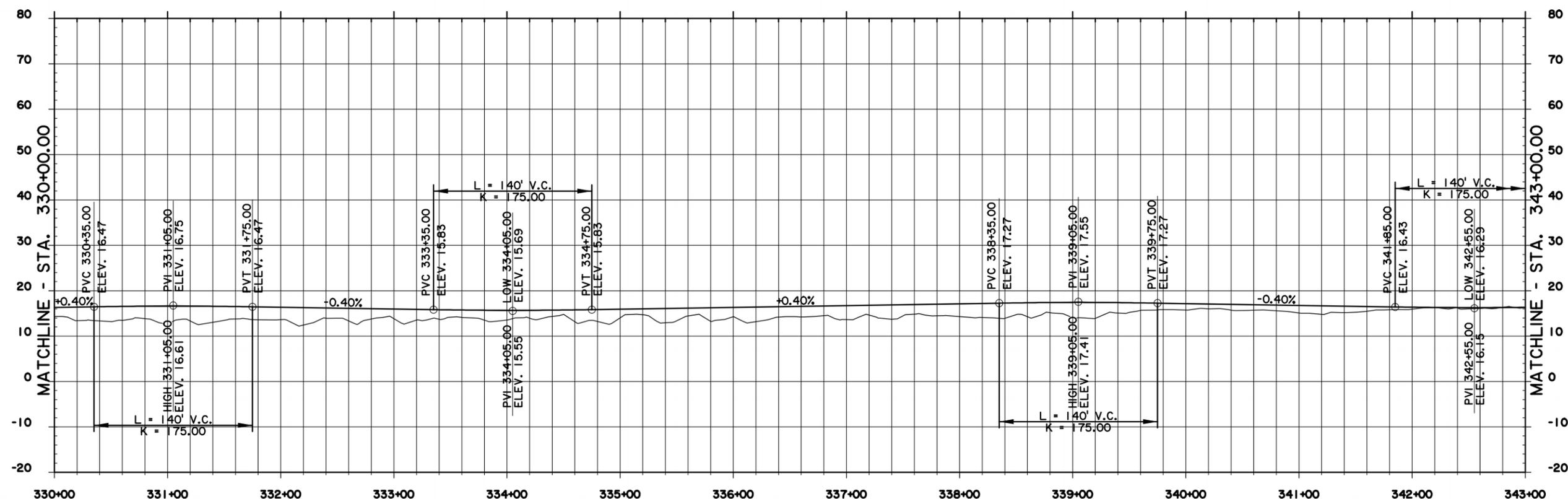
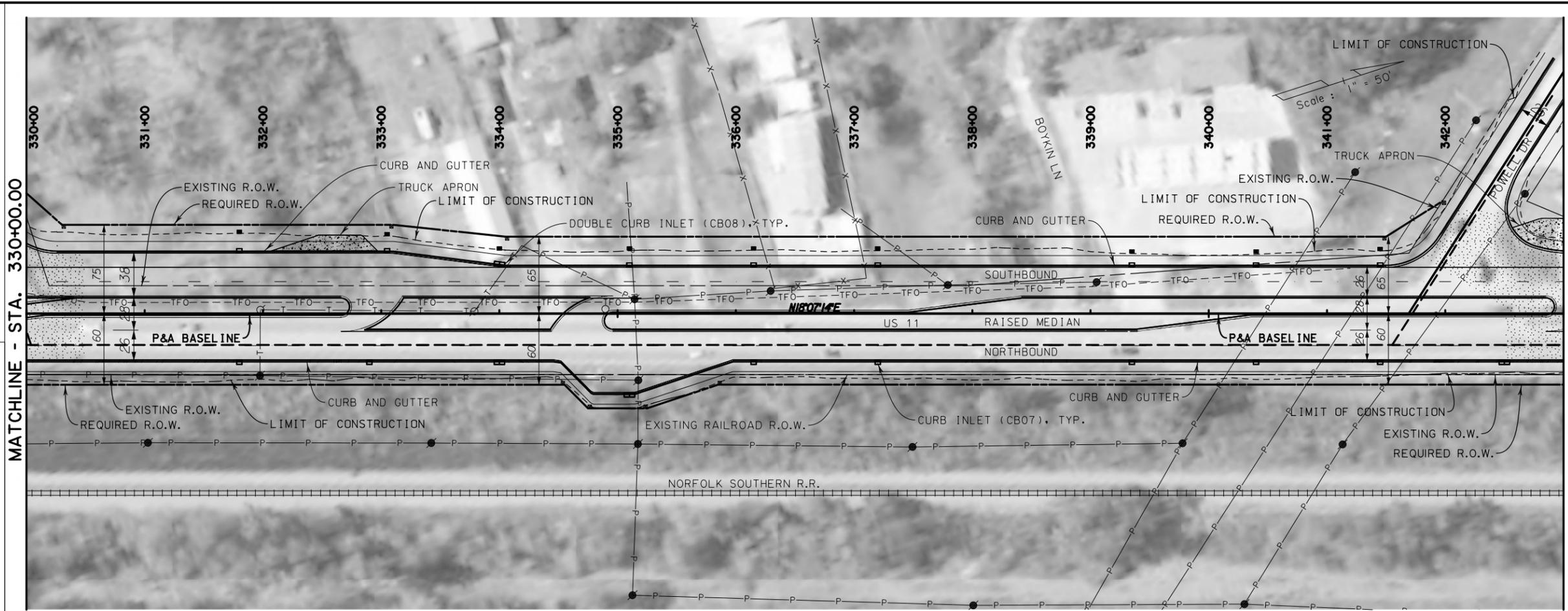
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1800 N. CALLEWAY BLVD., SUITE 900
MCKEITHEN, LOUISIANA 70002
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PROJECT & ADOPTED BASELINE
 REQUIRED CURB AND GUTTER
 REQUIRED CLEAR ZONE LIMITS
 REQUIRED CONSTRUCTION LIMITS
 REQUIRED HIGHWAY RIGHT-OF-WAY
 REQUIRED RIGHT-OF-WAY MARKER
 REQUIRED CURB INLET (CB-07)
 REQUIRED CURB INLET (CB-08)

EXISTING HIGHWAY/STREET BASELINE
 EXISTING HIGHWAY RIGHT-OF-WAY
 EXISTING RAILROAD TRACK
 EXISTING RAILROAD RIGHT-OF-WAY
 EXISTING OVERHEAD POWERLINE
 EXISTING POWERPOLE

LINE AND SYMBOL LEGEND



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-5

PLAN AND PROFILE
 ALTERNATIVE NO. 1
 STA. 330+00 TO STA. 343+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

DESIGNED: D. FULKS
 CHECKED: L. PORTA
 DETAILED: J. HOWARD
 CHECKED: D. FULKS

ST. TAMMANY
 FEDERAL PROJECT: H.000688
 STATE PROJECT: H.000688.2

DATE: 05 FEB 2014
 SHEET: 10

NO. DATE BY REVISION DESCRIPTION

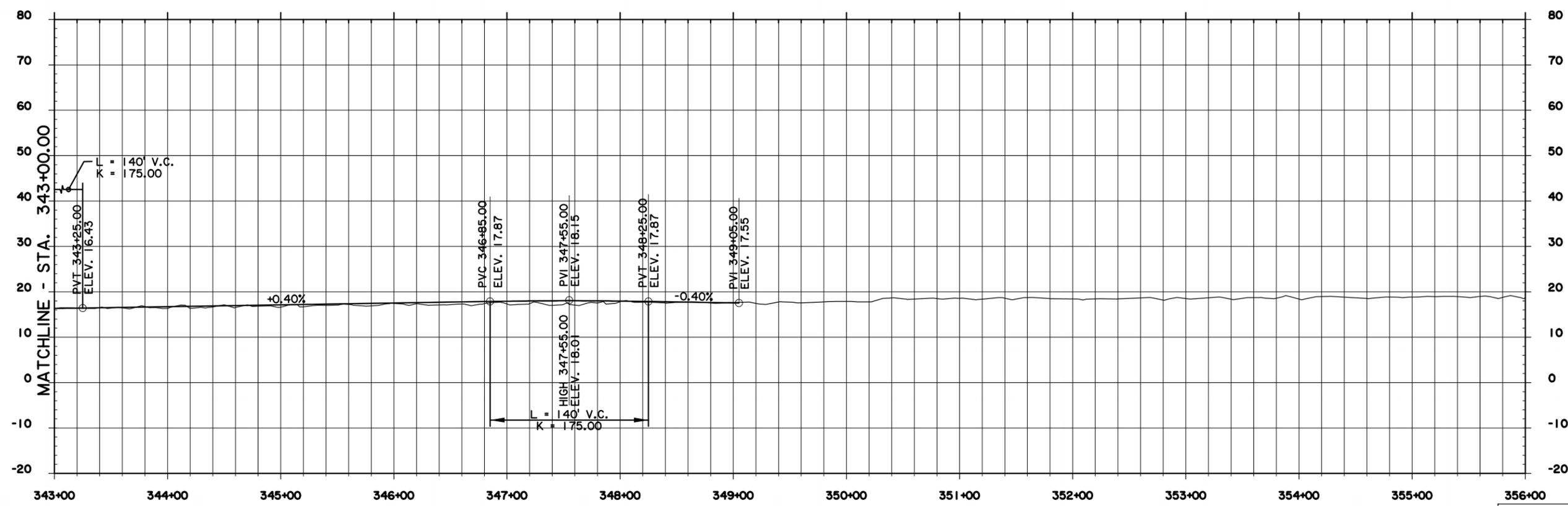
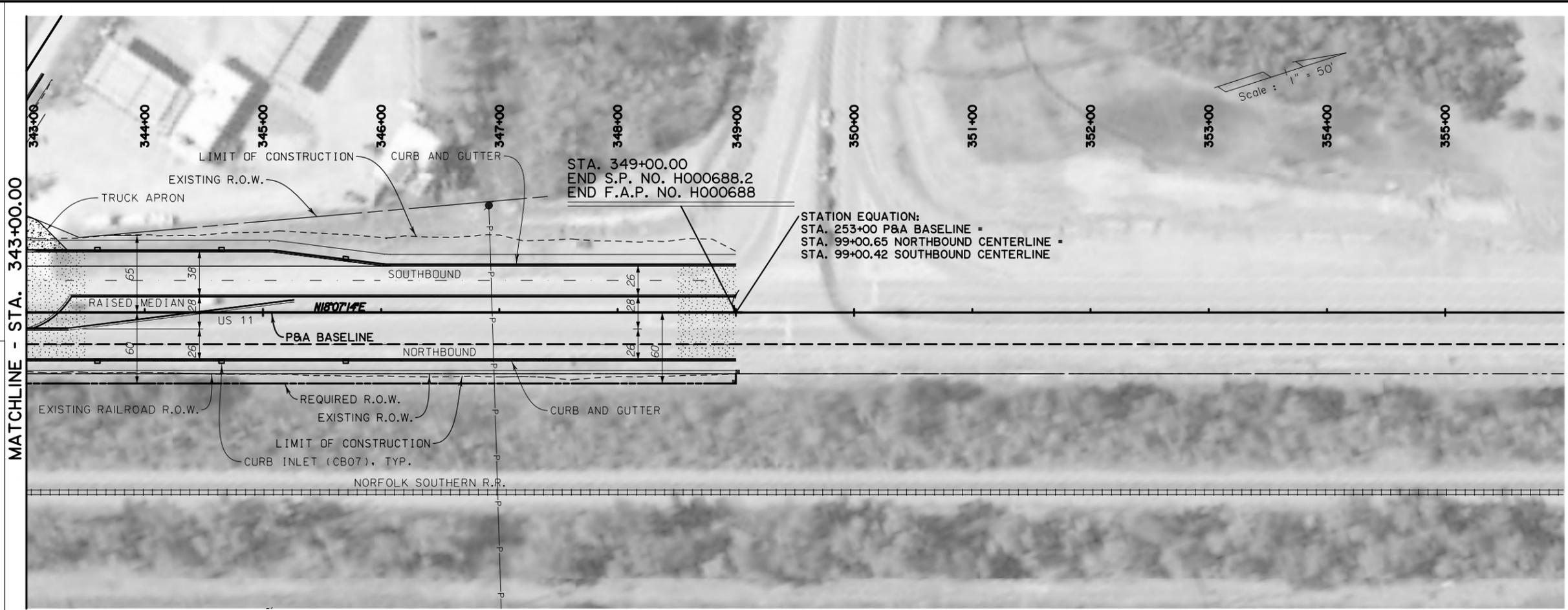
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PROJECT & ADOPTED BASELINE
 PROJECTED CURB AND GUTTER
 REQUIRED CLEAR ZONE LIMITS
 REQUIRED CONSTRUCTION LIMITS
 REQUIRED HIGHWAY RIGHT-OF-WAY
 REQUIRED RIGHT-OF-WAY MARKER
 REQUIRED CURB INLET (CB-07)
 REQUIRED CURB INLET (CB-08)

EXISTING HIGHWAY/STREET BASELINE
 EXISTING HIGHWAY RIGHT-OF-WAY
 EXISTING RAILROAD TRACK
 EXISTING RAILROAD RIGHT-OF-WAY
 EXISTING OVERHEAD POWERLINE
 EXISTING POWERPOLE

LINE AND SYMBOL LEGEND



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-5

DESIGNED D. FULKS
 CHECKED L. PORTA
 DETAILED J. HOWARD
 CHECKED D. FULKS
 DATE 05 FEB 2014
 SHEET 11

ST. TAMMANY
 FEDERAL PROJECT H.000688
 STATE PROJECT H.000688.2

NO. DATE BY

REVISION DESCRIPTION

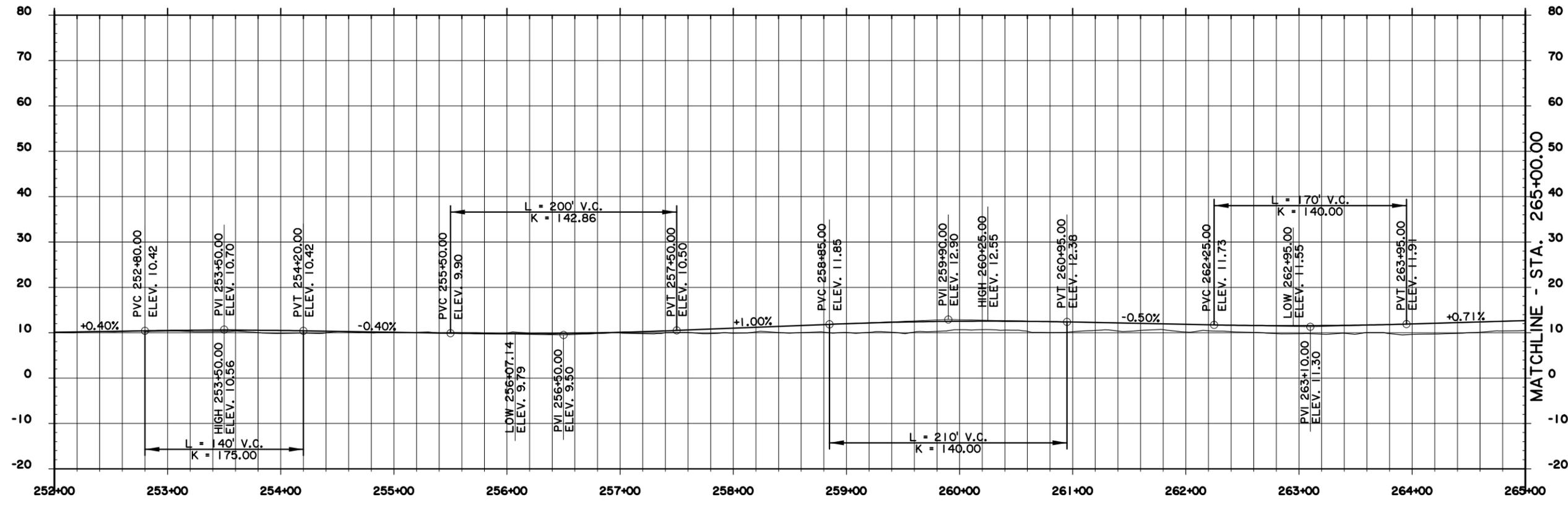
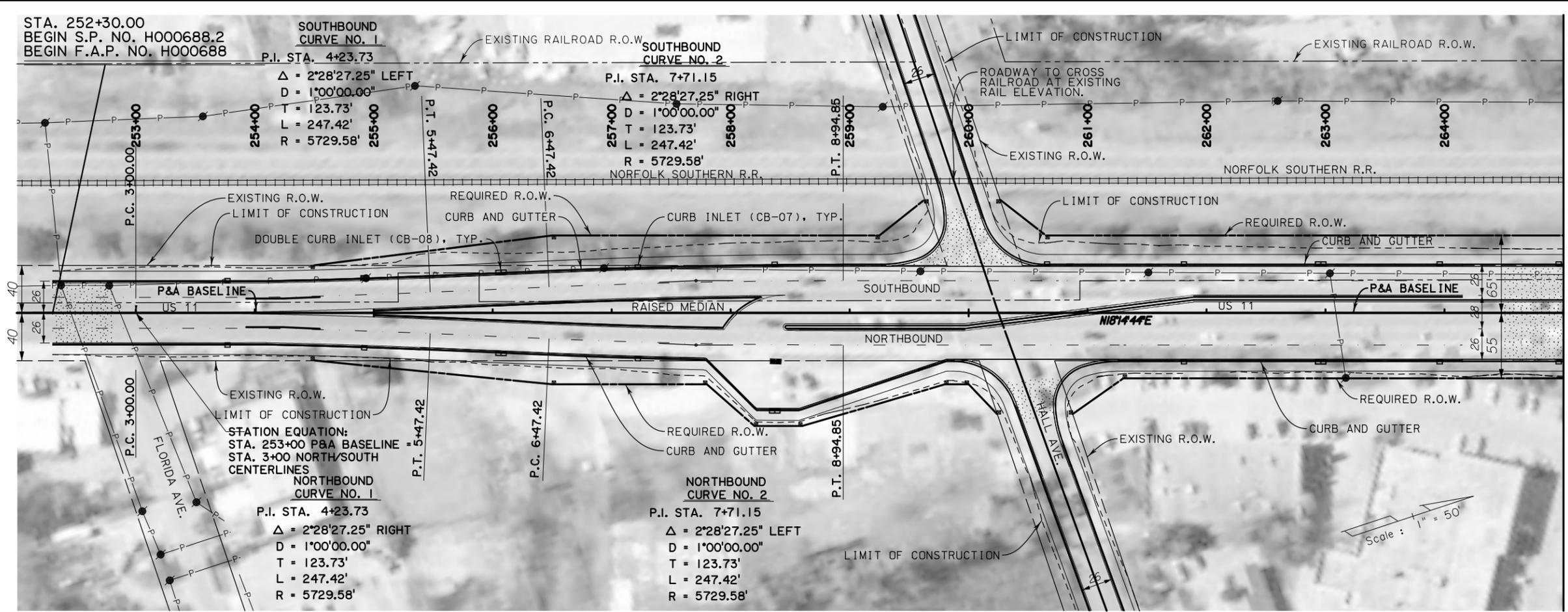
PLAN AND PROFILE
 ALTERNATIVE NO. 1
 STA. 343+00 TO STA. 349+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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LINE AND SYMBOL LEGEND

- EXISTING HIGHWAY/STREET BASELINE
- EXISTING HIGHWAY RIGHT-OF-WAY
- EXISTING RAILROAD TRACK
- EXISTING RAILROAD RIGHT-OF-WAY
- EXISTING OVERHEAD POWERLINE
- EXISTING POWERPOLE
- PROJECT & ADOPTED BASELINE
- REQUIRED CURB AND GUTTER
- REQUIRED CLEAR ZONE LIMITS
- REQUIRED CONSTRUCTION LIMITS
- REQUIRED HIGHWAY RIGHT-OF-WAY
- REQUIRED RIGHT-OF-WAY MARKER
- REQUIRED CURB INLET (CB-07)
- REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

PLAN AND PROFILE
 ALTERNATIVE NO. 1R
 STA. 252+00 TO STA. 265+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

DESIGNED: D. FULKS
 CHECKED: L. PORTA
 DETAILED: J. HOWARD
 CHECKED: D. FULKS

DATE: 05 FEB 2014
 SHEET: 4

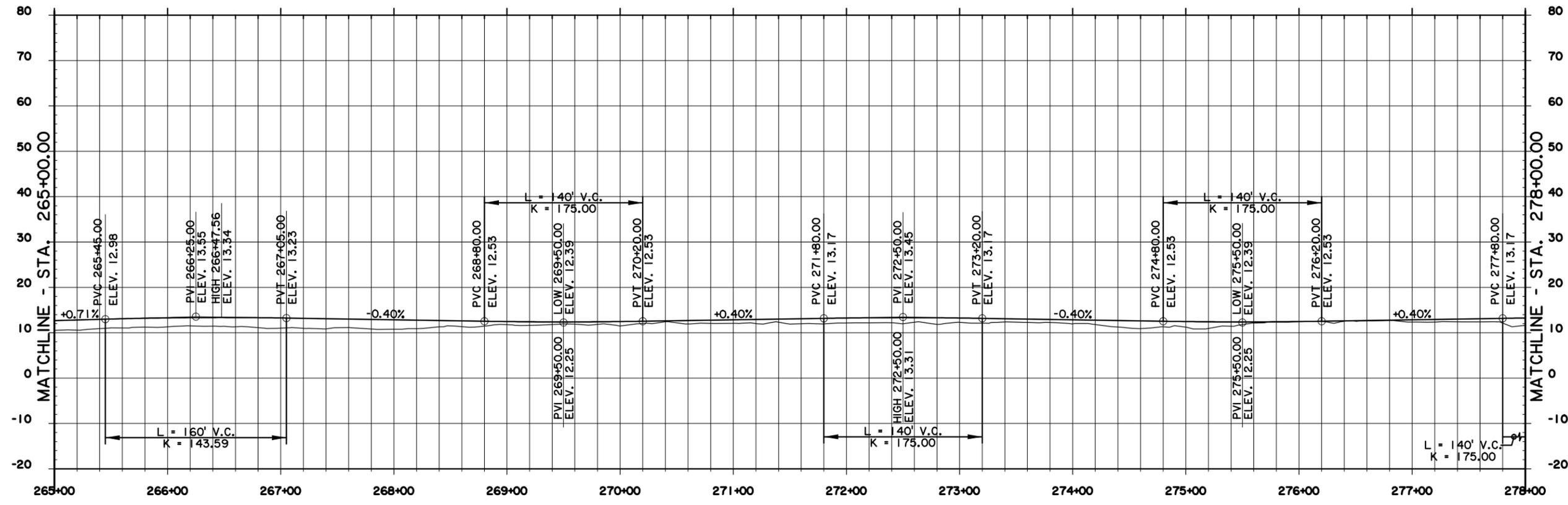
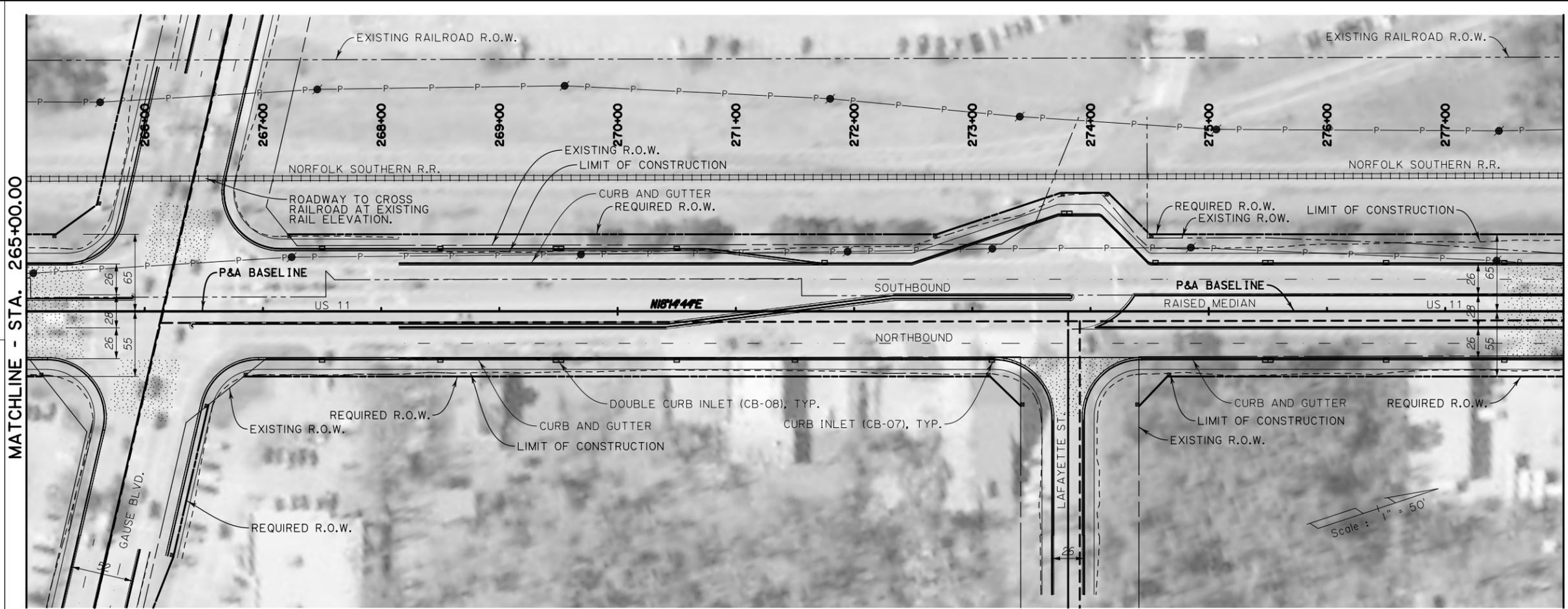
ST. TAMMANY
 FEDERAL PROJECT: H.000688
 STATE PROJECT: H.000688.2

ARCADIS
 5800 N. CALIFORNIA BLVD., SUITE 900
 METRO CENTER, DORSETT, MISSISSIPPI 39063
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LINE AND SYMBOL LEGEND

- EXISTING HIGHWAY/STREET BASELINE
- EXISTING HIGHWAY RIGHT-OF-WAY
- EXISTING RAILROAD TRACK
- EXISTING RAILROAD RIGHT-OF-WAY
- EXISTING OVERHEAD POWERLINE
- EXISTING POWERPOLE
- PROJECT & ADOPTED BASELINE
- REQUIRED CURB AND GUTTER
- REQUIRED CLEAR ZONE LIMITS
- REQUIRED CONSTRUCTION LIMITS
- REQUIRED HIGHWAY RIGHT-OF-WAY
- REQUIRED RIGHT-OF-WAY MARKER
- REQUIRED CURB INLET (CB-07)
- REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

DESIGNED	D. FULKS	PARISH	ST. TAMMANY
CHECKED	L. PORTA	FEDERAL PROJECT	H.000688
DATE	05 FEB 2014	STATE PROJECT	H.000688.2
SHEET	5	REVISION DESCRIPTION	

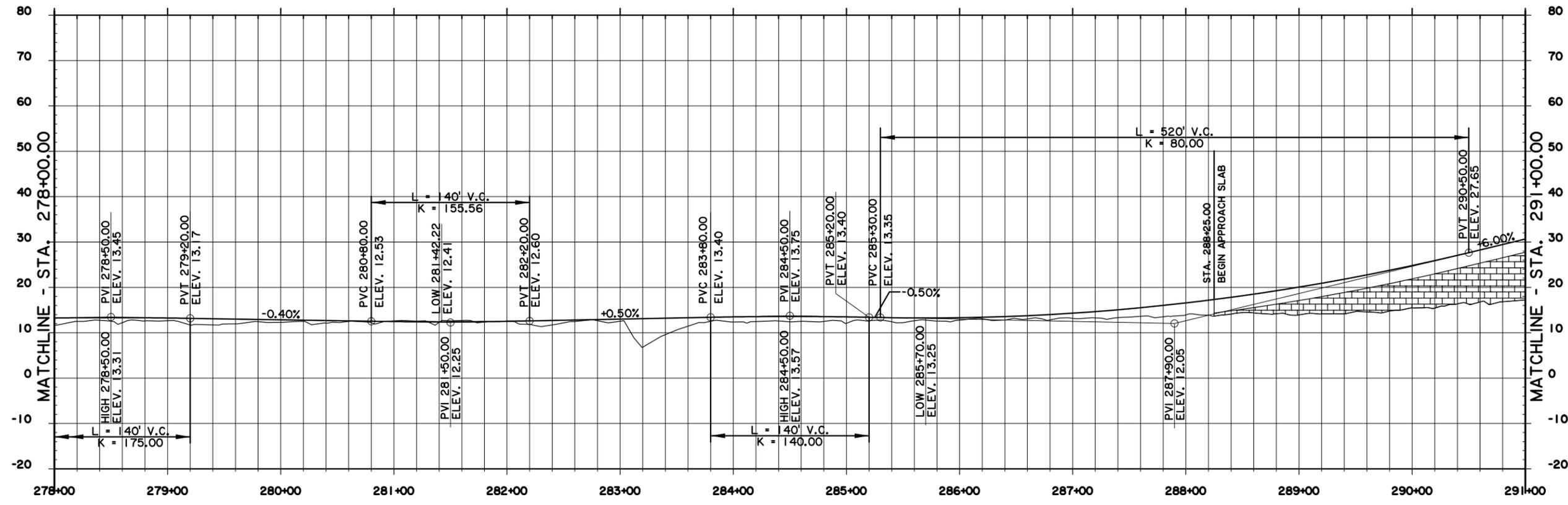
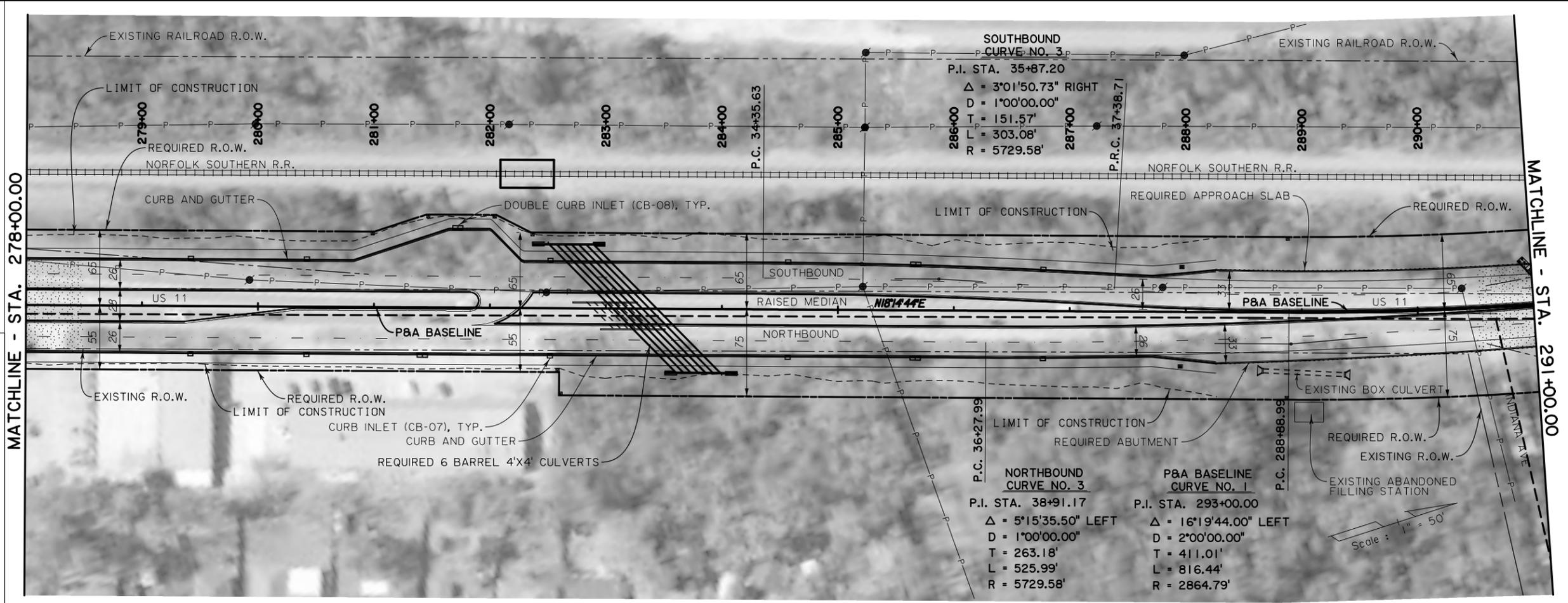
PLAN AND PROFILE
ALTERNATIVE NO. 1R
STA. 265+00 TO STA. 278+00
US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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LINE AND SYMBOL LEGEND

--- EXISTING HIGHWAY/STREET BASELINE	--- PROJECT & ADOPTED BASELINE
--- EXISTING HIGHWAY RIGHT-OF-WAY	--- REQUIRED CURB AND GUTTER
--- EXISTING RAILROAD TRACK	--- REQUIRED CLEAR ZONE LIMITS
--- EXISTING RAILROAD RIGHT-OF-WAY	--- REQUIRED CONSTRUCTION LIMITS
--- EXISTING OVERHEAD POWERLINE	--- REQUIRED HIGHWAY RIGHT-OF-WAY
--- EXISTING POWERPOLE	--- REQUIRED RIGHT-OF-WAY MARKER
	--- REQUIRED CURB INLET (CB-07)
	--- REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

DESIGNED: D. FULKS	FARISH: ST. TAMMANY	SHEET NUMBER: 6
CHECKED: L. PORTA	FEDERAL PROJECT: H.000688	STATE PROJECT: H.000688.2
DATE: 05 FEB 2014	DATE: 05 FEB 2014	DATE: 05 FEB 2014
SHEET: 6	SHEET: 6	SHEET: 6

PLAN AND PROFILE
 ALTERNATIVE NO. 1R
 STA. 278+00 TO STA. 291+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

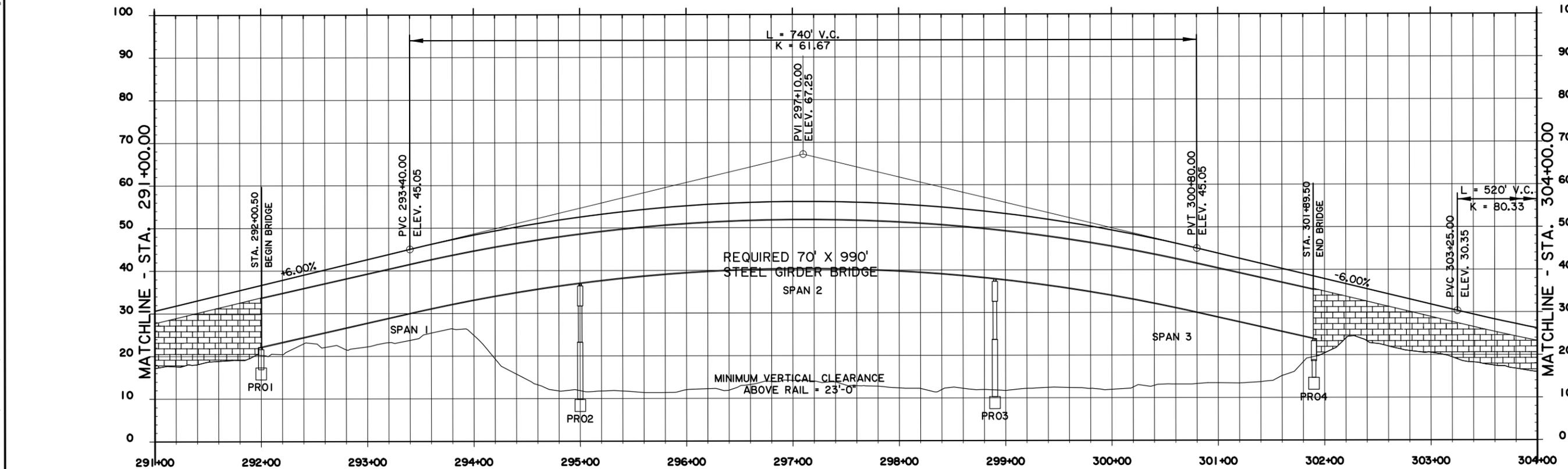
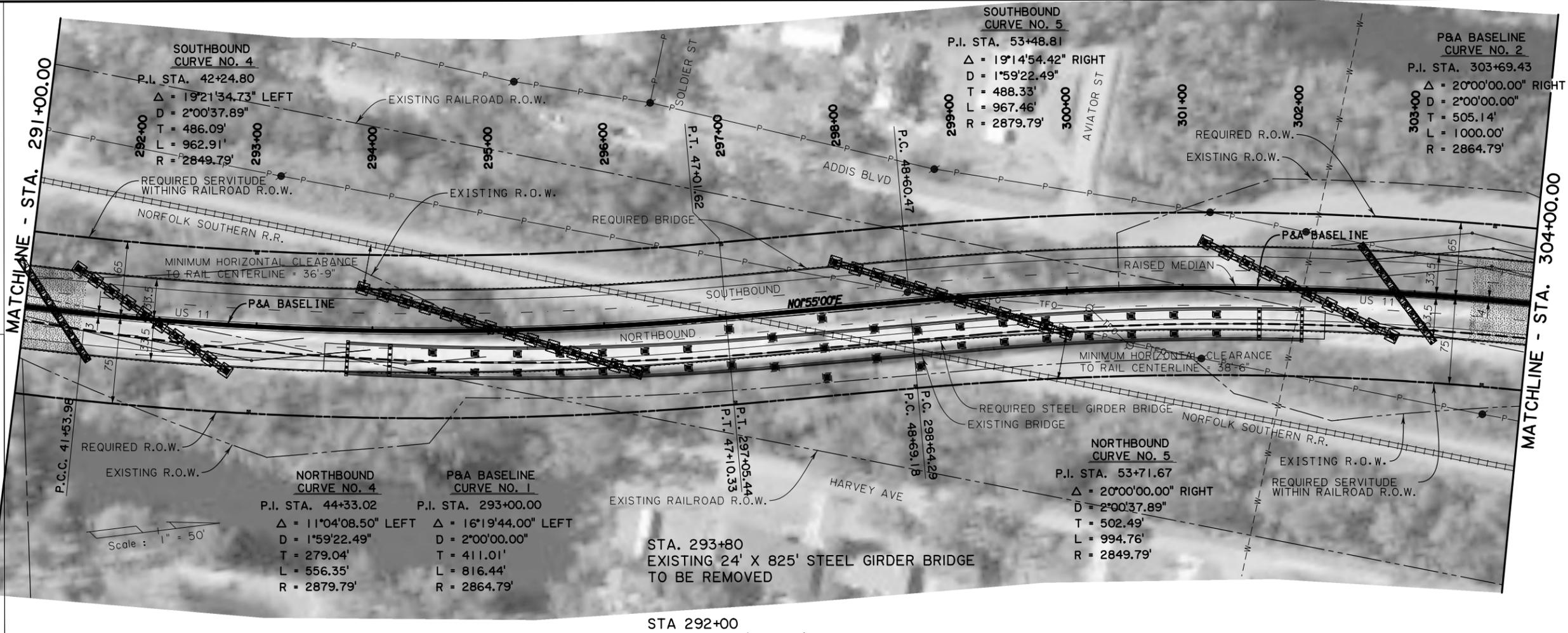
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PROJECT & ADOPTED BASELINE
 REQUIRED CURB AND GUTTER
 REQUIRED CLEAR ZONE LIMITS
 REQUIRED CONSTRUCTION LIMITS
 REQUIRED HIGHWAY RIGHT-OF-WAY MARKER
 REQUIRED CURB INLET (CB-07)
 REQUIRED CURB INLET (CB-08)

EXISTING HIGHWAY/STREET BASELINE
 EXISTING HIGHWAY RIGHT-OF-WAY
 EXISTING RAILROAD TRACK
 EXISTING RAILROAD RIGHT-OF-WAY
 EXISTING OVERHEAD POWERLINE
 EXISTING POWERPOLE

LINE AND SYMBOL LEGEND



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

DESIGNED	D. FULKS	PARISH	ST. TAMMANY	SHEET NUMBER	7
CHECKED	L. PORTA	FEDERAL PROJECT	H.000688		
DRAWN	D. FULKS	STATE PROJECT	H.000688.2		
DATE	05 FEB 2014	DATE			
NO.	7	REVISION DESCRIPTION			

PLAN AND PROFILE
 ALTERNATIVE NO. 1R
 STA. 291+00 TO STA. 304+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

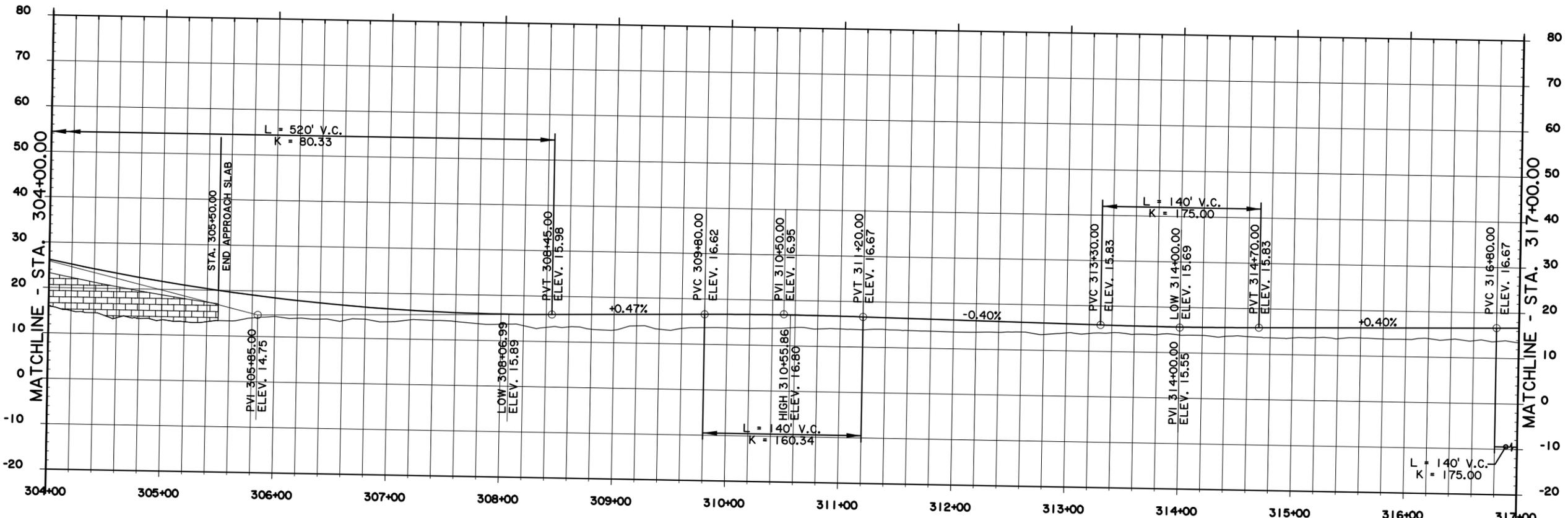
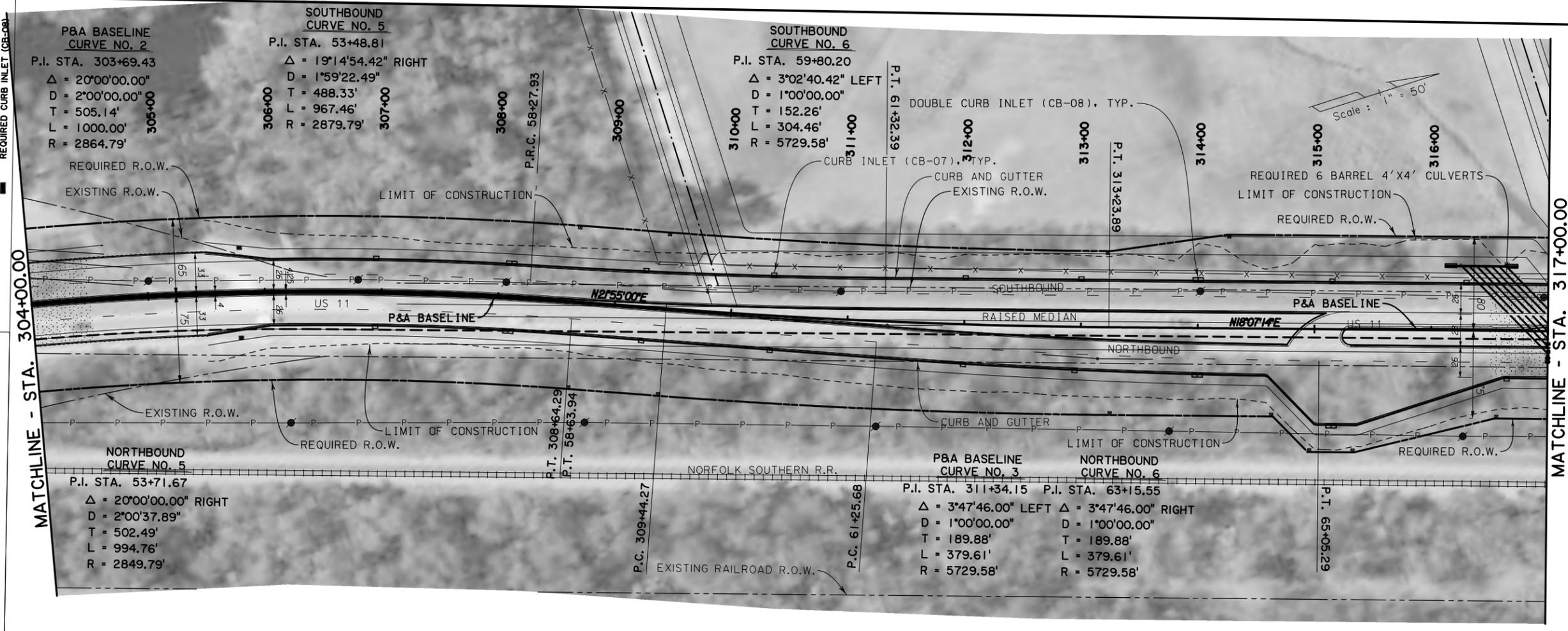
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PROJECT & ADOPTED BASELINE
 REQUIRED CURB AND GUTTER
 REQUIRED CLEAR ZONE LIMITS
 REQUIRED CONSTRUCTION LIMITS
 REQUIRED HIGHWAY RIGHT-OF-WAY
 REQUIRED RIGHT-OF-WAY MARKER
 REQUIRED CURB INLET (CB-07)
 REQUIRED CURB INLET (CB-08)

EXISTING HIGHWAY/STREET BASELINE
 EXISTING HIGHWAY RIGHT-OF-WAY
 EXISTING RAILROAD TRACK
 EXISTING RAILROAD RIGHT-OF-WAY
 EXISTING OVERHEAD POWERLINE
 EXISTING POWERPOLE

LINE AND SYMBOL LEGEND



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

DESIGNED	D. FULKS	PARISH	ST. TAMMANY
CHECKED	L. PORTA	FEDERAL PROJECT	H.000688
DATE	05 FEB 2014	STATE PROJECT	H.000688.2
SHEET	B		

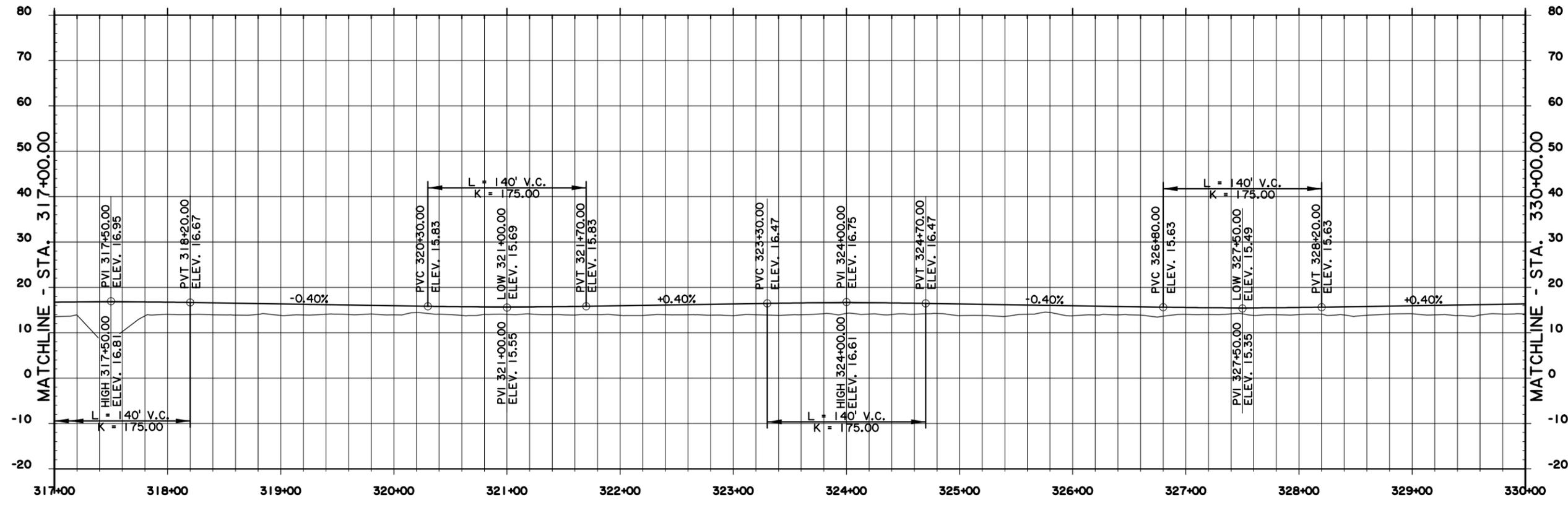
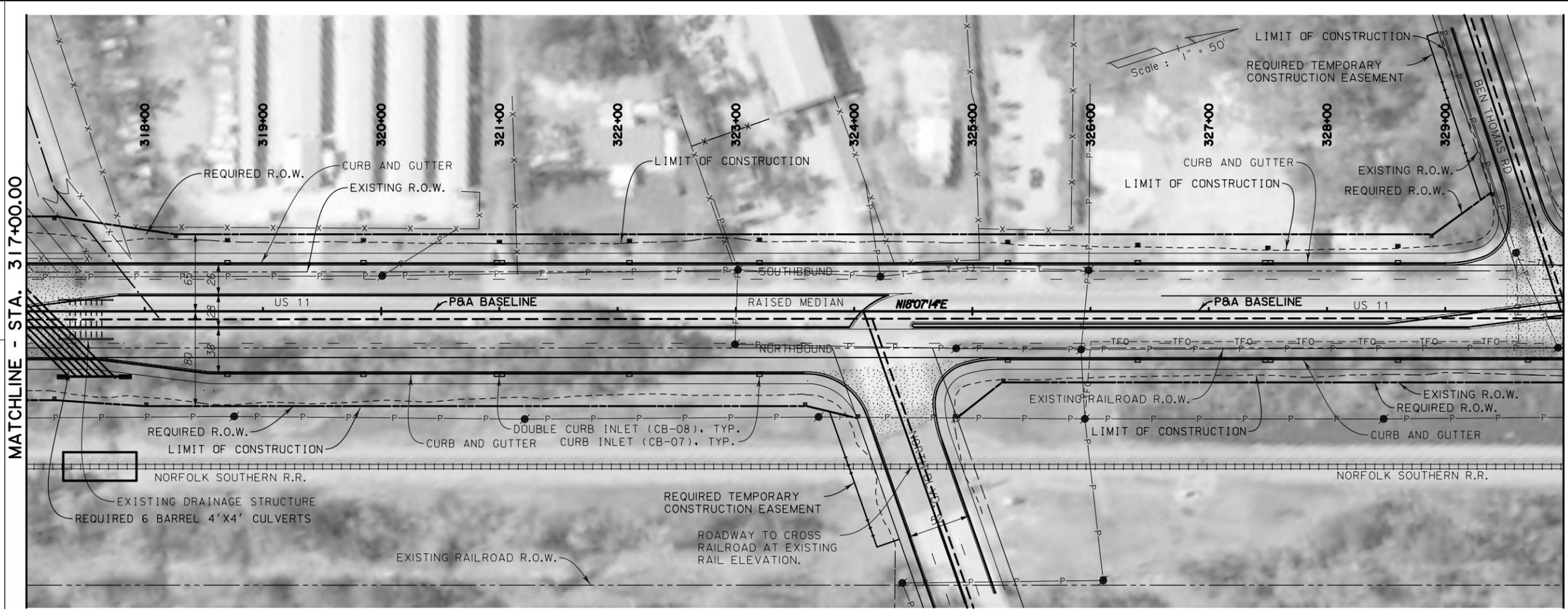
PLAN AND PROFILE
 ALTERNATIVE NO. 1R
 STA. 304+00 TO STA. 317+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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 1001 PINEBLISS BLVD., SUITE 500
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LINE AND SYMBOL LEGEND

--- EXISTING HIGHWAY/STREET BASELINE	--- PROJECT & ADOPTED BASELINE
--- EXISTING HIGHWAY RIGHT-OF-WAY	--- REQUIRED CURB AND GUTTER
--- EXISTING RAILROAD TRACK	--- REQUIRED CLEAR ZONE LIMITS
--- EXISTING RAILROAD RIGHT-OF-WAY	--- REQUIRED CONSTRUCTION LIMITS
--- EXISTING OVERHEAD POWERLINE	--- REQUIRED HIGHWAY RIGHT-OF-WAY
● EXISTING POWERPOLE	■ REQUIRED RIGHT-OF-WAY MARKER
	■ REQUIRED CURB INLET (CB-07)
	■ REQUIRED CURB INLET (CB-08)



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

DESIGNED	D. FULKS	FARISH	ST. TAMMANY	SHEET NUMBER	9
CHECKED	L. PORTA	FEDERAL PROJECT	H.000688	DATE	05 FEB 2014
REVISION DESCRIPTION		STATE PROJECT	H.000688.2		

BY: _____ NO. _____ DATE _____

PLAN AND PROFILE
ALTERNATIVE NO. 1R
STA. 317+00 TO STA. 330+00
US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

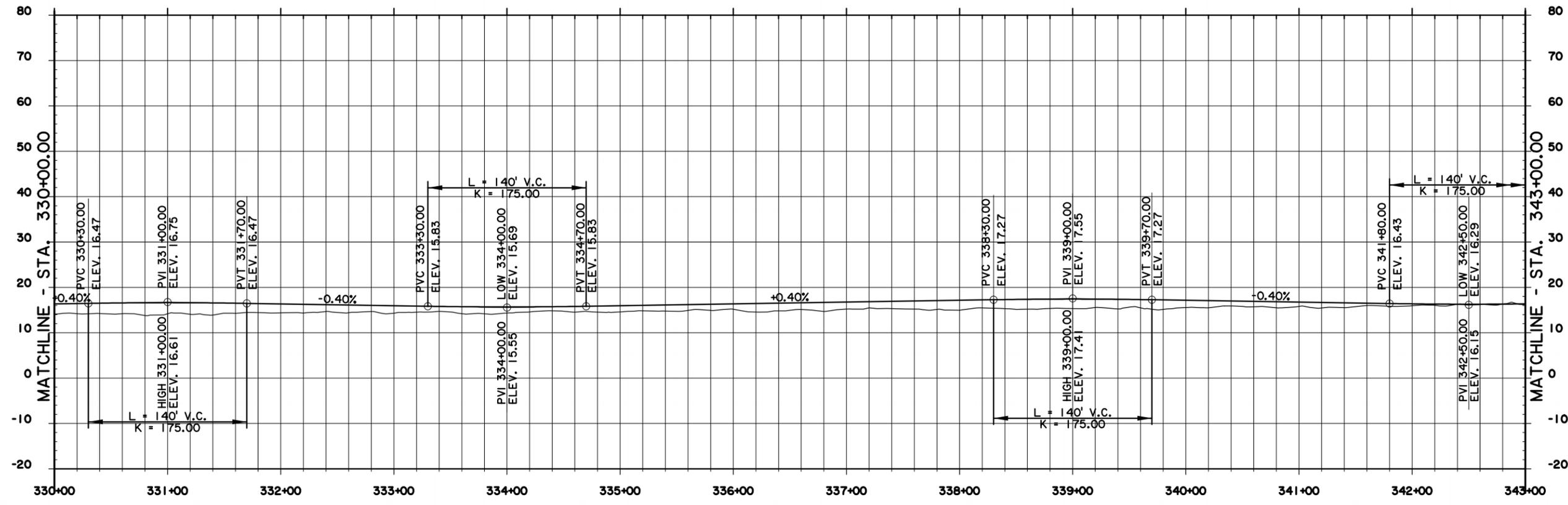
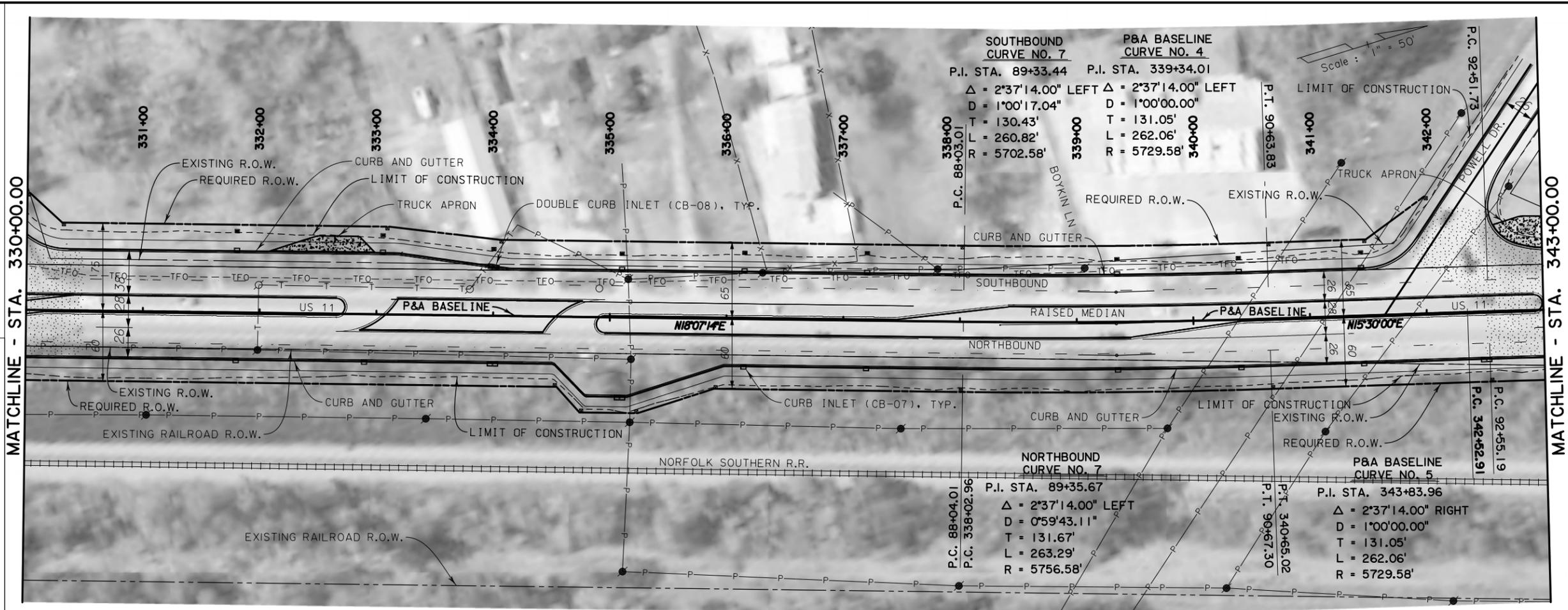
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PROJECT & ADOPTED BASELINE
 REQUIRED CURB AND GUTTER
 REQUIRED CLEAR ZONE LIMITS
 REQUIRED CONSTRUCTION LIMITS
 REQUIRED HIGHWAY RIGHT-OF-WAY
 REQUIRED RIGHT-OF-WAY MARKER
 REQUIRED CURB INLET (CB-07)
 REQUIRED CURB INLET (CB-08)

EXISTING HIGHWAY/STREET BASELINE
 EXISTING HIGHWAY RIGHT-OF-WAY
 EXISTING RAILROAD TRACK
 EXISTING RAILROAD RIGHT-OF-WAY
 EXISTING OVERHEAD POWERLINE
 EXISTING POWERPOLE

LINE AND SYMBOL LEGEND



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

DESIGNED	D. FULKS	PARISH	ST. TAMMANY
CHECKED	L. PORTA	FEDERAL PROJECT	H.000688
CHECKED	J. HOWARD	STATE PROJECT	H.000688.2
CHECKED	D. FULKS	DATE	05 FEB 2014
NO.	10	SHEET	10
REVISION DESCRIPTION	BY	DATE	

PLAN AND PROFILE
 ALTERNATIVE NO. 1R
 STA. 330+00 TO STA. 343+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

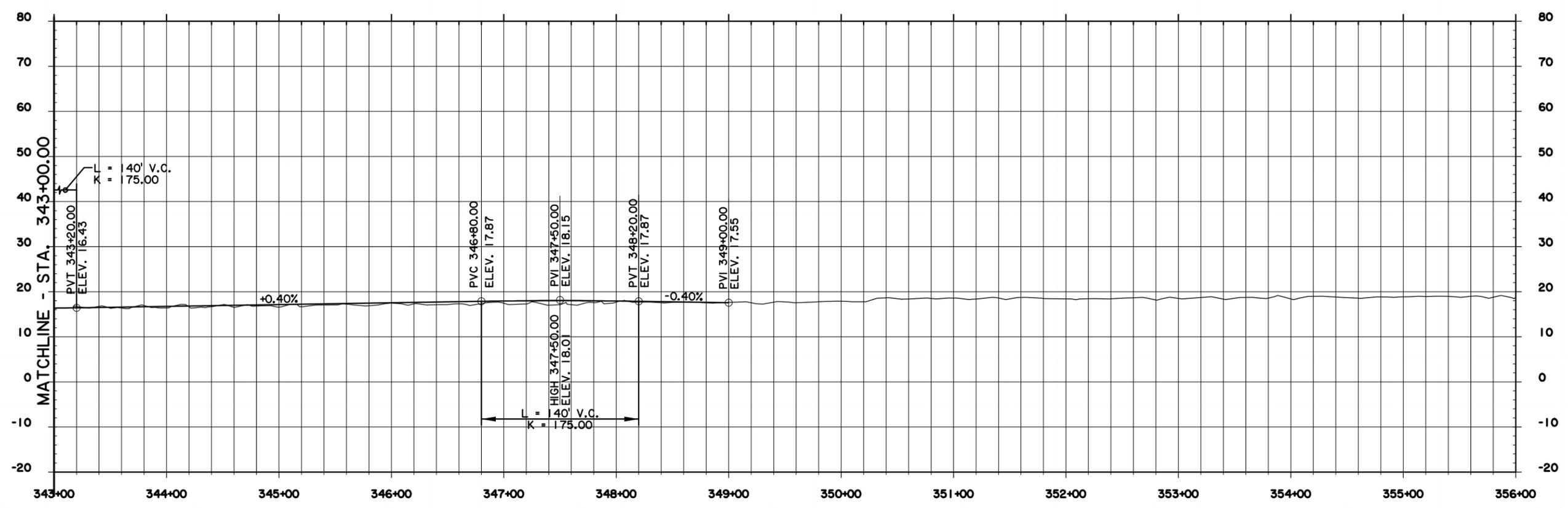
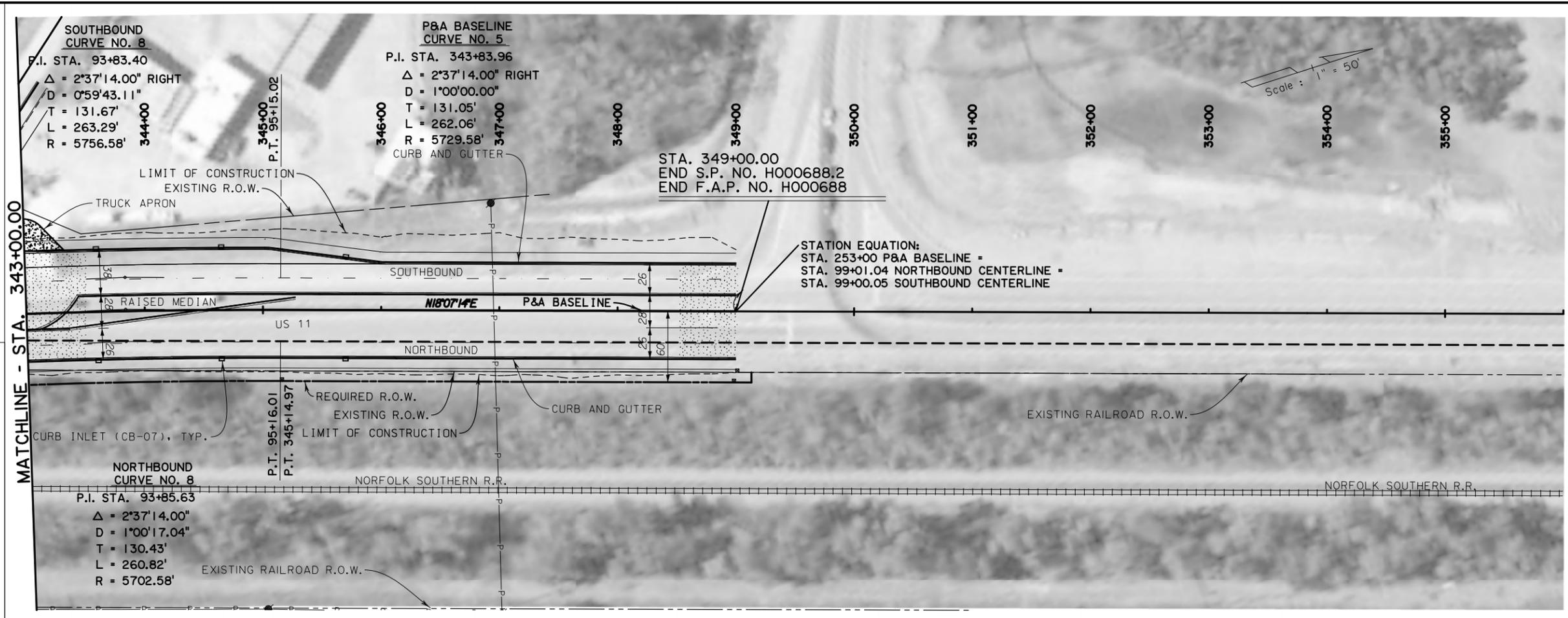
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 FT. WORTH, TEXAS 76102

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PROJECT & ADOPTED BASELINE
 PROJECTED CURB AND GUTTER
 REQUIRED CLEAR ZONE LIMITS
 REQUIRED CONSTRUCTION LIMITS
 REQUIRED HIGHWAY RIGHT-OF-WAY
 REQUIRED RIGHT-OF-WAY MARKER
 REQUIRED CURB INLET (CB-07)
 REQUIRED CURB INLET (CB-08)

EXISTING HIGHWAY/STREET BASELINE
 EXISTING HIGHWAY RIGHT-OF-WAY
 EXISTING RAILROAD TRACK
 EXISTING RAILROAD RIGHT-OF-WAY
 EXISTING OVERHEAD POWERLINE
 EXISTING POWERPOLE

LINE AND SYMBOL LEGEND



PRELIMINARY AND SUBJECT TO CHANGE

FIGURE 3-6

DESIGNED: D. FULKS
 CHECKED: L. PORTA
 DETAILED: J. HOWARD
 CHECKED: D. FULKS

DATE: 05 FEB 2014
 SHEET: 11

PARISH: ST. TAMMANY
 FEDERAL PROJECT: H.000688
 STATE PROJECT: H.000688.2

PLAN AND PROFILE
 ALTERNATIVE NO. 1R
 STA. 343+00 TO STA. 356+00
 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE

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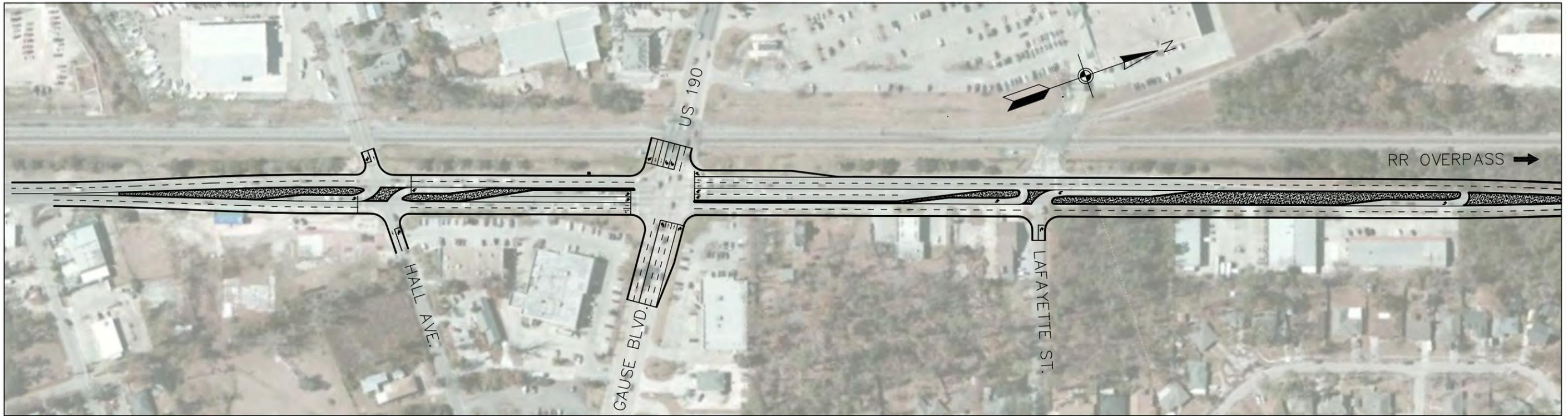
APPENDIX B-4

Intersection Build Layouts Options 1, 2, and 3

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Southern Section



Northern Section



BUILD OPTION 2
Conceptual Layouts
Subject to Change

SHEET NUMBER 2

PARISH ST. TAMMANY
 FEDERAL PROJECT H.000688
 STATE PROJECT H.000688.2

DESIGNED CHECKED
 DETAILED CHECKED
 DATE DEC. 2013
 SHEET

REVISION DESCRIPTION
 BY
 DATE



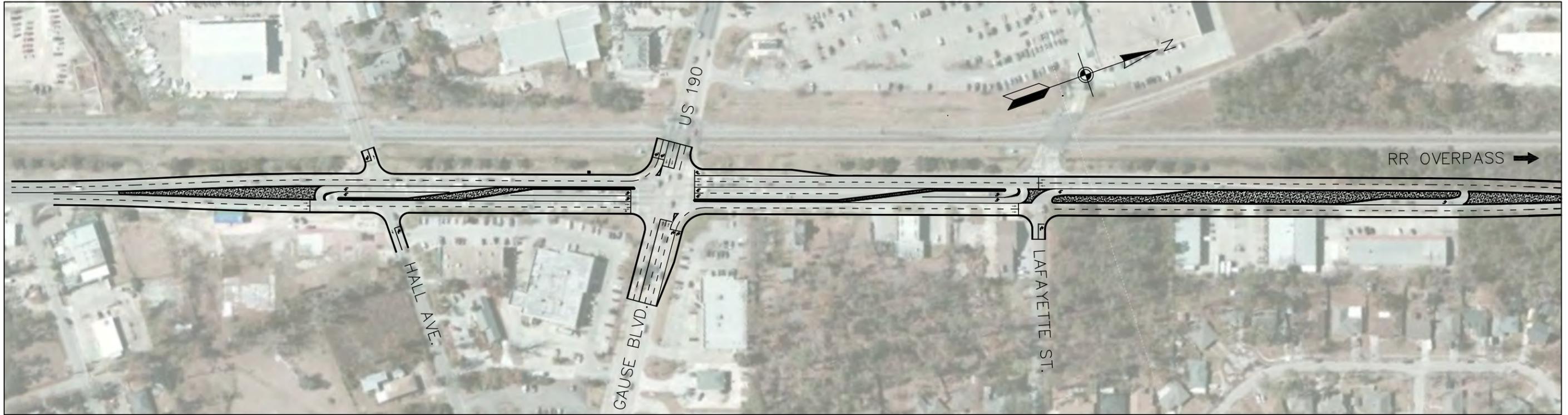
US 11 NORFOLK SOUTHERN RAILROAD
 ENVIRONMENTAL ASSESSMENT
 CONCEPTUAL LAYOUTS



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 Baton Rouge, LA 70816

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Southern Section



Northern Section



BUILD OPTION 3
Conceptual Layouts
Subject to Change

SHEET NUMBER 3

PARISH ST. TAMMANY
 FEDERAL PROJECT H.000688
 STATE PROJECT H.000688.2

DESIGNED CHECKED
 DETAILED CHECKED
 DATE DEC. 2013
 SHEET

REVISION DESCRIPTION
 BY
 DATE



US 11 NORFOLK SOUTHERN RAILROAD
 ENVIRONMENTAL ASSESSMENT
 CONCEPTUAL LAYOUTS



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 Baton Rouge, LA 70816

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APPENDIX C

Agency Coordination

C-1 Solicitation of Views and Responses

C-2 Solicitation of Views - Tribes

C-3 Section 106

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APPENDIX C-1

Solicitation of Views and Responses

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«Courtesy» «First» «M»«Last_Name»
«Title»
«Org_1»
«Org_2»
«Address»
«City», «State» «Zip»

Subject:
Solicitation of Views and
Initiation of Section 106 Consultation
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana
State Project No. H.000688.2
F.A.P No. H000688

Dear «Salutation»:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns and reach agreeable decisions while taking into account the interests of all parties.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A Stage 0 Feasibility Study for this project was completed in 2008. A project summary and figures are attached for your review. If you would like to review the Stage 0 Study in its entirety, please request a copy from Beth Beam by e-mail at elizabeth.beam@arcadis-us.com or by U.S. mail to Ms. Beth Beam, ARCADIS, 10352 Plaza Americana Drive, Baton Rouge, Louisiana 70816. A project overview, project study area map, and figures are attached for your review.

We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

Imagine the result

ARCADIS U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge
Louisiana 70816
Tel 225 292 1004
Fax 225 218 9677
www.arcadis-us.com

INFRASTRUCTURE

Date:
3 May 2013

Contact:
Beth Beam

Extension:
215

Email:
elizabeth.beam@arcadis-us.com

Our ref:
LA003143.0000
LDOTD/3143/C/1/kp



«Courtesy» «First»
«Last_Name»
3 May 2013

Closing

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by **Monday, June 3, 2013**. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688.2 in your reply.

Sincerely,

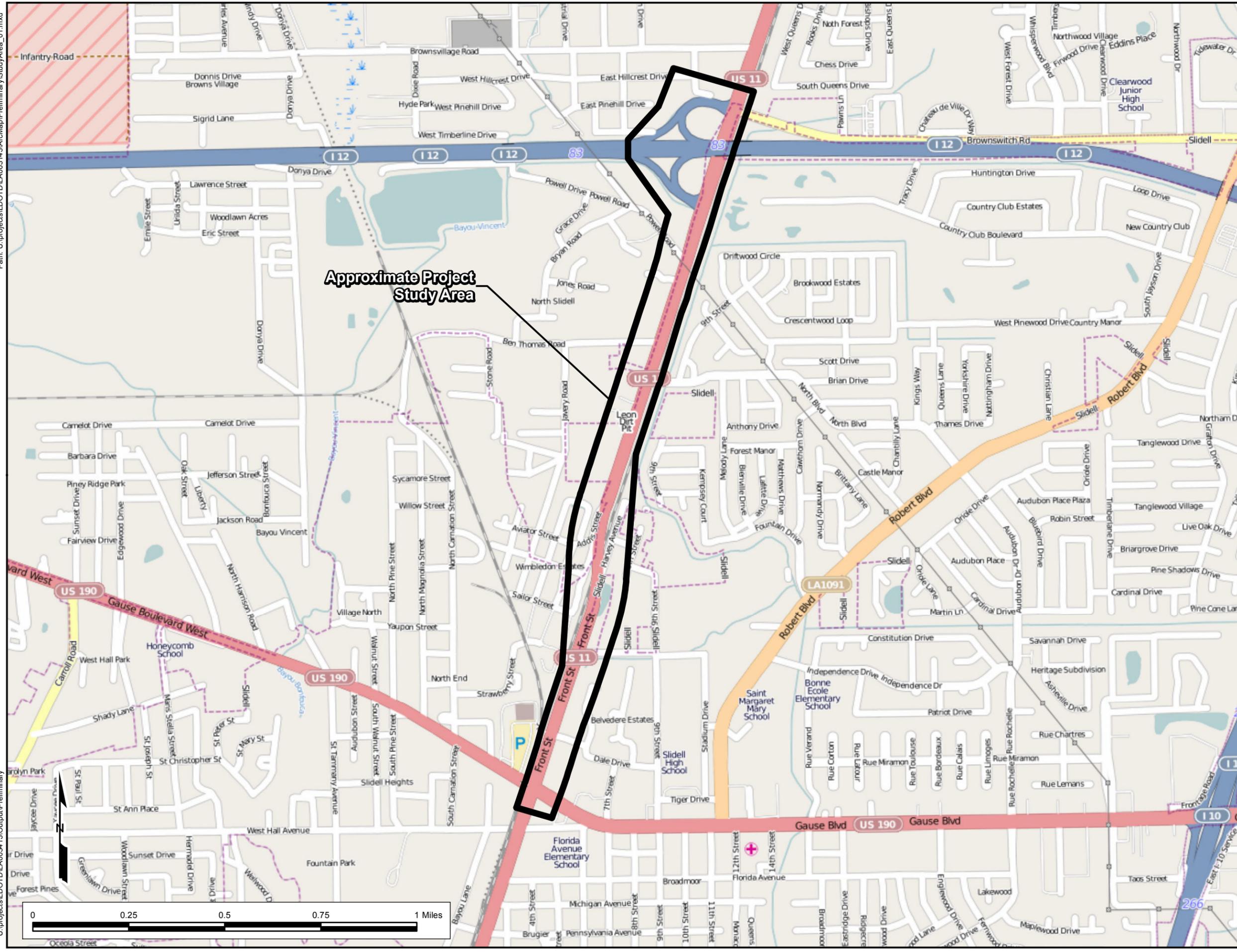
ARCADIS U.S., Inc.

A handwritten signature in black ink that reads "Elizabeth Beam".

Elizabeth Beam, AICP
Senior Planner/Scientist

Attachments

U:\projects\DOTD\LA003143\Output\Preliminary\U:\projects\DOTD\LA003143\ArcMap\PreliminaryStudyArea_01.mxd



PROJECT LOCATION AND STUDY AREA MAP

US 11 Norfolk Southern Railroad Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana
State Project No. H.000688.2
F.A.P No. H000688

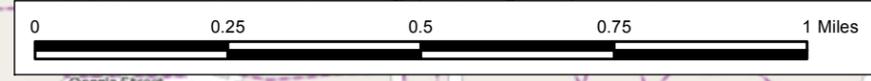
Legend

Preliminary Study Area

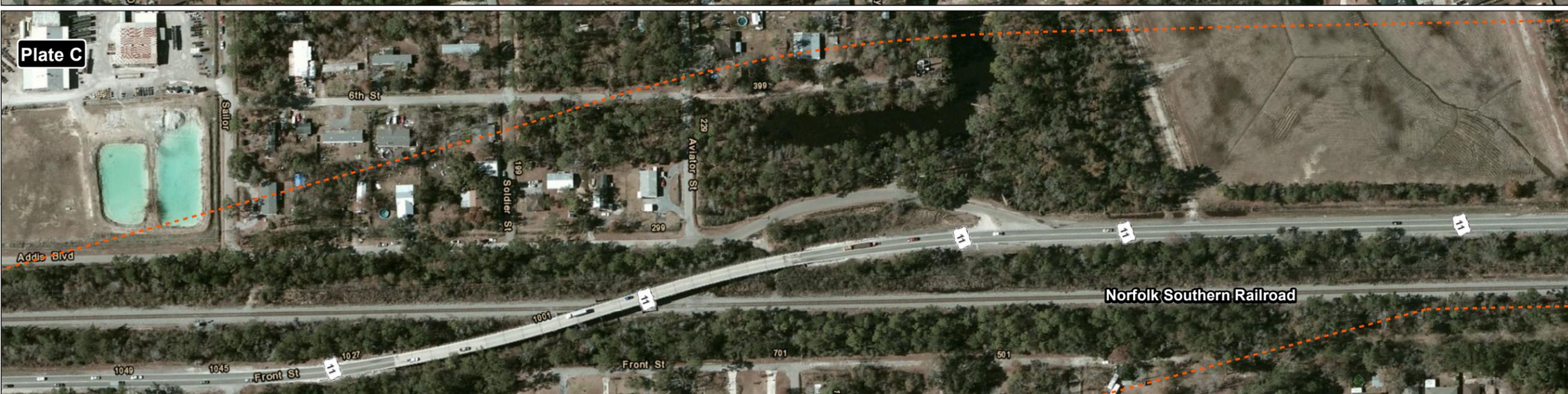
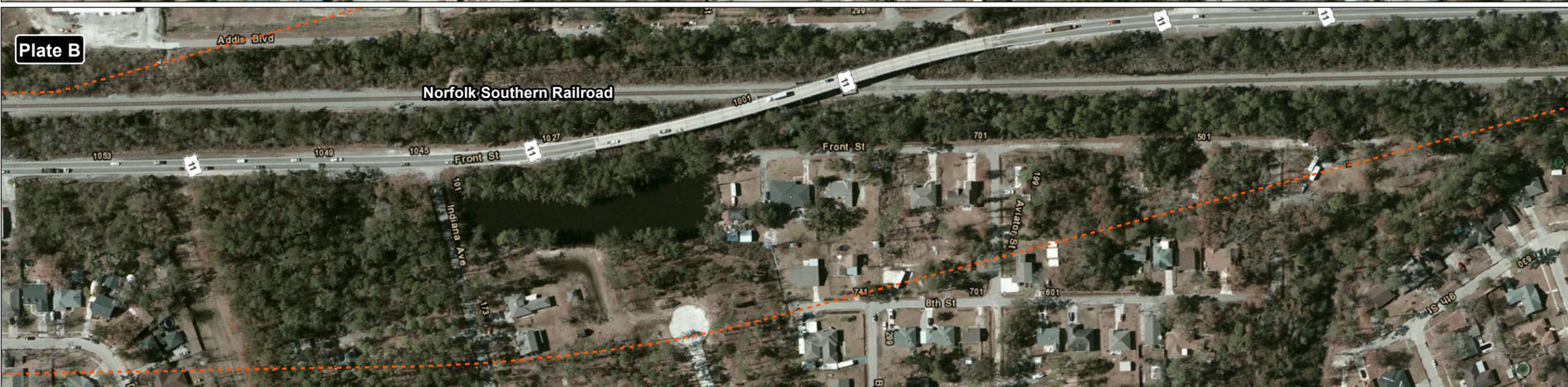
Reference: ESRI Base Map Data, Open Street Map



DRAWING BY: JWC	DATE: 05.01.2013
PROJECT NUMBER: LA003143.0000	FIGURE NUMBER: 1



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US 11 Norfolk Southern Railroad
 Environmental Assessment
 State Project No. H.000688.2
 E.A.P No. H000688
 Route US 11
 St. Tammany Parish, Louisiana

CORRIDOR PLATES

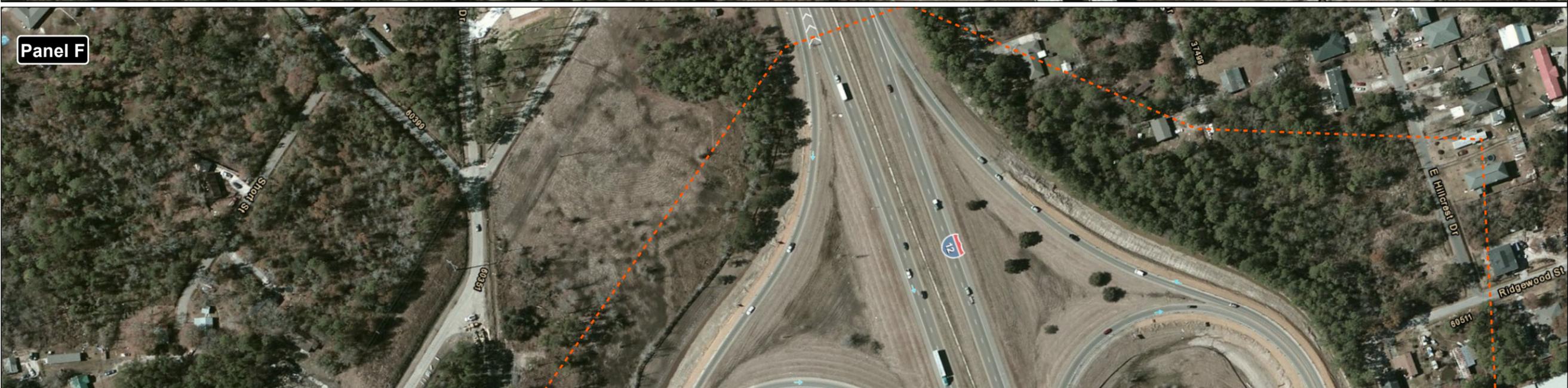
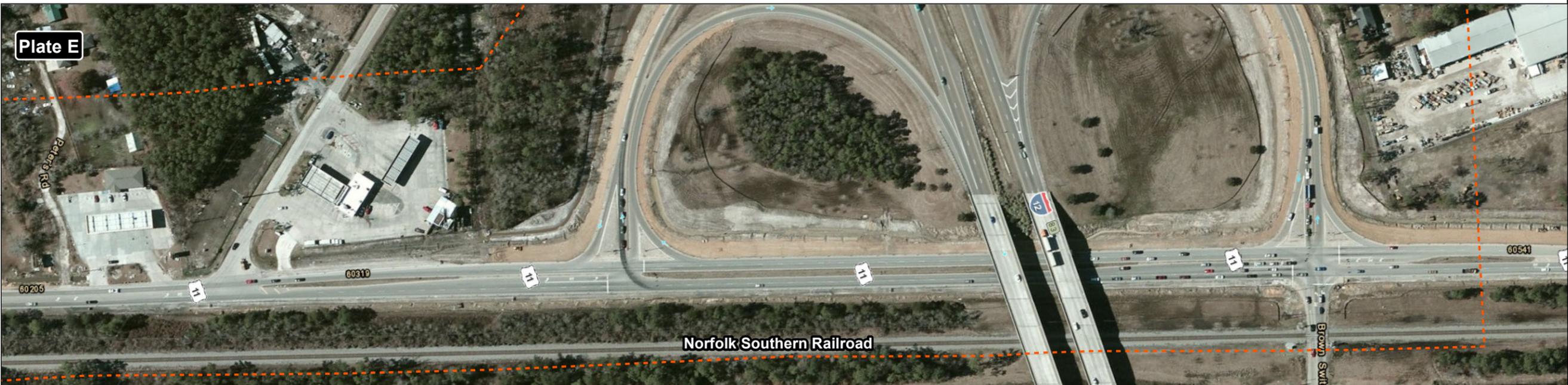
Legend

--- Preliminary Study Area

Reference: ESRI Base Maps & Imagery

Date: 04/23/2013
 Project Number: LA003413.0000
 Figure No.: **2 - a**

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US 11 Norfolk Southern Railroad
 Environmental Assessment
 State Project No. H.000688.2
 F.A.P No. H000688
 Route US 11
 St. Tammany Parish, Louisiana

CORRIDOR PLATES

Legend
 - - - Preliminary Study Area



Reference: ESRI Base Maps & Imagery



Date: 04/23/2013
 Project Number: LA003413.0000

Figure No.: **2 - b**

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US 11 Norfolk Southern Railroad
SOV Mailing List
Environmental Assessment
H.000688.2

Cat 1	Cat 2	Salutation	Courtesy	First	M	Last Name	Title	Org 1	Org 2	Address	City	State	Zip
State	A	Mr. Adams	Mr.	Elliott		Adams	Director of Business Development	Louisiana Department of Economic Development	Office of Business Development	P.O. Box 94185	Baton Rouge	LA	70804-9185
Federal	E	Representative Alexander	The Honorable	Rodney		Alexander	U.S. Congressman	U.S. House of Representatives (District 5)		1900 Stubbs Avenue, Suite B	Monroe	LA	71201
Federal	A	Mr. Allred	Mr.	Charles	R.	Allred	Supervisory Env Specialist, Section Chief	U.S. Army Corps of Engineers	Vicksburg District	4155 Clay Street	Vicksburg	MS	39183-3435
Parish	E	Mr. Artigue	Mr.	Richard		Artigue		St. Tammany Parish Council, District 13		53057 Hwy 433	Slidell	LA	70461
Federal	A	Mr. Bechdol	Mr.	Michael		Bechdol		U.S. Environmental Protection Agency	Source Water Protection (6WQ-S)	1445 Ross Ave, Suite 1200	Dallas	TX	75202-2733
Parish	E	Mr. Bellisario	Mr.	E.L.		Bellisario		St. Tammany Parish Council, District 9		3090 Gause Blvd, #535	Slidell	LA	70461
Other	T	Mr. Bennett	Mr.	Gilmer		Bennett		Apalachee Talimali Band of Louisiana		P.O. Box 84	Libuse	LA	71348
Parish	E	Mr. Binder	Mr.	Jerry		Binder	Chariman	St. Tammany Parish Council, District 12		470 Hickory Drive	Slidell	LA	70458
Federal	E	Representative Boustany	The Honorable	Charles		Boustany Jr., MD	U.S. Congressman	U.S. House of Representatives (District 3)		800 Lafayette Street, Suite 1400	Lafayette	LA	70501
State	A	Ms. Breaux	Ms.	Pam		Breaux	Attn: Mike Varnado, Division of Historic Preservation	Louisiana Department of Culture, Recreation & Tourism		P.O. Box 44247	Baton Rouge	LA	70804-4247
Parish	E	Ms. Brister	Ms.	Pat		Brister	President	St. Tammany Parish		P.O. Box 628	Covington	LA	70434
State	E	Representative Burns	The Honorable	Timothy G.	"Tim"	Burns		Louisiana House of Representatives (District 89)		1 Sanctuary Blvd., Suite 306	Mandeville	LA	70471
Parish	E	Mr. Canulette	Mr.	Chris		Canulette		St. Tammany Parish Council, District 8		109 Stratford Drive	Slidell	LA	70458
Federal	E	Representative Cassidy	The Honorable	Bill		Cassidy, MD	U.S. Congressman	U.S. House of Representatives (District 4)		5555 Hilton Avenue, Suite 100	Baton Rouge	LA	70808
State	A	Mr. Causey	Mr.	Jake		Causey	Chief Engineering	Louisiana Department of Health & Hospitals	Center for Environmental Health	P.O. Box 4489	Baton Rouge	LA	70821
Federal	E	Representative Cedric	The Honorable	Richmond		Cedric	U.S. Congressman	U.S. House of Representatives (District 2)		2021 Lakeshore Drive, Suite 309	New Orleans	LA	70122
Other	T	Chief Chretien, Jr.	Chief	Edward		Chretien, Jr.		Atakapa-Ishak Nation		P.O. Box 1532	Lake Charles	LA	70602
State	E	Representative Cromer	The Honorable	Gregory		Cromer		Louisiana House of Representatives (District 90)		P.O. Box 2088	Slidell	LA	70459
State	T	Principal Chief Dardar, Jr.	Principal Chief	Thomas		Dardar, Jr.		United Houma Nation		20986 Highway 1	Golden Meadow	LA	70357
Local		Ms. Davis	Ms.	Shannon		Davis	Director	St. Tammany Parish	Public Works	620 N. Tyler Street	Covington	LA	70433
State	T	Chairman Davis	Chairman	Rufus		Davis, Jr.		Adai Caddo Indians of Louisiana		Route 2, Box 246	Robeline	LA	71469
State	E	Senator Donahue	The Honorable	Jack		Donahue		The State Senate (District 11)		P.O. Box 896	Mandeville	LA	70470
City	E	Mayor Drennan	Mayor	Freddy		Drennan		Mayor, City of Slidell		2045 Second Street	Slidell	LA	70458
State	A	Mr. Dubea	Mr.	Wade		Dubea	Assistant Commissioner	Louisiana Department of Agriculture & Forestry	Office of Forestry	P.O. Box 1628	Baton Rouge	LA	70821
Local		Ms. Edwards	Ms.	Amy		Edwards	Chairman of the Board	St. Tammany West Chamber of Commerce		610 Hollycrest Blvd	Covington	LA	70433
Federal	E	Representative Fleming	The Honorable	John		Fleming, MD	U.S. Congressman	U.S. House of Representatives (District 4)		6425 Youree Drive, Suite 350	Shreveport	LA	71105
Local		Ms. Fontenot	Ms.	Sidney		Fontenot	Director	St. Tammany Parish	Dept of Development/Planning	21454 Koop Drive	Mandeville	LA	70471
State	A	Mr. Ford	Mr.	Mark		Ford	Director	Office of Indian Affairs		P.O. Box 94004	Baton Rouge	LA	70804-9004
State	T	Chairwoman Foret	Chairwoman	Marlene		Foret		Grand Cailou/Dulac Band of the Biloxi Chitimacha		114 Retreat Drive	Bourg	LA	70343
Other	T	Chief Gil	Chief	James	Greywol	Gil		Louisiana Choctaw Turtle Tribe		379 Sharon Lane	Lake Charles	LA	70611
Other	T	Chief Gillum	Chief	Elwin	Warhors	Gillum		Chakta Tribe		61357 Dixie Ranch Road	Slidell	LA	70460
Federal	A	Ms. Gilmore	Ms.	Cathy		Gilmore		U.S. Environmental Protection Agency	Federal Activities Branch (6E-F)	1445 Ross Ave, Suite 1200	Dallas	TX	75202-2733
State	A	Mr. Hardeman	Mr.	Cleve		Hardeman	Director of Outdoor Recreation	Louisiana Department of Culture, Recreation & Tourism	Office of State Parks	P.O. Box 44426	Baton Rouge	LA	70804
Parish	E	Ms. Heintz	Ms.	Elizabeth	B.	Heintz	President	St. Tammany Parish Public Schools		321 N. Theard Street	Covington	LA	70433-2835
Local		Ms. Herring	Ms.	Ashlee		Herring	Deputy Planning Section Chief	City of New Orleans	Office of Homeland Security & Emergency Preparedness	1300 Perdido Street	New Orleans	LA	70112
Local		Ms. Ingram-Hunter	Ms.	Tara		Ingram-Hunter	Director	City of Slidell Planning Department		250 Bouscaren Street, Suite 203	Slidell	LA	70458
State	G	Ms. Jones	Ms.	Billie	M.	Jones	Project Developer	Office of Cultural Development	Department of Culture, Recreation, and Tourism	P.O. Box 44247	Baton Rouge	LA	70802
Federal	A	Mr. Kamien	Mr.	Douglas	J.	Kamien, PE	Deputy for Programs and Project Management	U.S. Army Corps of Engineers	Vicksburg District	4155 Clay Street	Vicksburg	MS	39183-3435
Federal	E	Senator Landrieu	Senator	Mary		Landrieu		United States Senate	Hale Boggs Federal Building	500 Poydras Street, Rm 1005	New Orleans	LA	70130
State	A	Mr. Leblanc	Mr.	John		Leblanc		Louisiana Department of Public Safety and Corrections	Highway Safety Commission	P.O. Box 66336	Baton Rouge	LA	70896
Federal	A	Mr. Mayer	Mr.	Martin		Mayer	Chief, Regulatory Branch	U.S. Army Corps of Engineers	New Orleans District	P.O. Box 60267	New Orleans	LA	70160-0267
State	G	Ms. Michon	Ms.	Carolyn		Michon	Manager	Louisiana Department of Wildlife and Fisheries	Louisiana Natural Heritage Program	2000 Quail Drive, Room 432	Baton Rouge	LA	70808-9038
State	T	Chief Naquin	Chief	Albert		Naquin		Isle de Jean Charles Band		100 Dennis Street	Montegut	LA	70377
State	T	Chairman Neal	Chairman	Tom		Neal		Clifton Choctaw Tribe of Louisiana		1312 Clifton Road	Clifton	LA	71447
State	E	Senator Nevers	The Honorable	Ben		Nevers		The State Senate (District 12)		724 Avenue F	Bogalusa	LA	70427
Federal	A	Mr. Norton	Mr.	Kevin		Norton	State Conservationist	U.S. Department of Agriculture	Natural Resources Conservation Service	3737 Government Street	Alexandria	LA	71302
Federal	A	Ms. Oberlies	Ms.	Karen		Oberlies	Technical Support	U.S. Department of the Army		P.O. Box 60267	New Orleans	LA	70538
State	E	Senator Pearson	The Honorable	J. Kevin		Pearson		Louisiana House of Representatives (District 76)		620 Oak Harbor Blvd., Suite 203	Slidell	LA	70458
Local		Ms. Pelegrin	Mr.	Alan		Pelegrin	Flood Plain Administrator	St. Tammany Parish		21454 Koop Drive	Mandeville	LA	70471
Other	S	Mr. Perret	Mr.	Ken		Perret	President	Louisiana Good Roads & Transportation Association		P.O. Box 3713	Baton Rouge	LA	70821
State	A	Mr. Phillippe	Mr.	Jamie		Phillippe		Louisiana Department of Environmental Quality	Office of Environmental Services	P.O. Box 4313	Baton Rouge	LA	70821-4313
State	T	Chairman Procell	Chairman	John		Procell		Choctaw-Apache Tribe of Ebarb		P.O. Box 1428	Zwolle	LA	71486
Federal	G	Mr. Raborn	Mr.	Larry	E.	Raborn	Operations Project Manager	U.S. Army Corps of Engineers	Project Resources Management Branch, Flood Control	4155 Clay Street	Vicksburg	MS	39183-3435
State	T	Mr. Rainwater	Mr.	James		Rainwater		The Louisiana Choctaw Tribe		15212 Hubb Road	Pride	LA	70770
City	E	Mr. Reeves	Mr.	Thomas	P.	Reeves	Council Administrator	City of Slidell		2045 Second Street	Slidell	LA	70458
State	A	Mr. Rieck	Mr.	Brad		Rieck	Deputy Supervisor	U.S. Fish & Wildlife Service	Lafayette Field Office	646 Cajundome Boulevard, Suite 400	Lafayette	LA	70506
State	E	Representative Ritchie	The Honorable	Harold	L.	Ritchie		Louisiana House of Representatives (District 75)		302 Louisiana Avenue	Bogalusa	LA	70427
Federal	E	Representative Scalise	The Honorable	Steve		Scalise	U.S. Congressman	U.S. House of Representatives (District 1)		110 Veterans Blvd., Suite 500	Metairie	LA	70005
State	E	Representative Schroder, Sr	The Honorable	John	M.	Schroder, Sr.		Louisiana House of Representatives (District 77)		222 N. Vermont Street, Ste K&M	Covington	LA	70433
Local		Ms. Brackett	Ms.	Sawn		Brackett	CEO	East St. Tammany Chamber of Commerce		118 West Hall Avenue	Slidell	LA	70460
State	A	Mr. Sibley	Mr.	Tenney		Sibley	Chief Sanitarian	Louisiana Department of Health & Hospitals	Sanitarian Services	P.O. Box 4489	Baton Rouge	LA	70821
State	E	Representative Simon	The Honorable	Scott	M.	Simon		Louisiana House of Representatives (District 74)		P.O. Box 2088	Slidell	LA	70459
Parish	E	Mr. Smith	Mr.	Thomas	J.	Smith		St. Tammany Parish Council, District 14		62390 John Smith Road	Pearl River	LA	70452
State	A	Mr. Solvey	Mr.	Greg		Solvey	Attn: Myra G. Diaz, Natural Hazards Program Specialist	FEMA Region VI		800 North Loop 288	Denton	TX	76201
State	A	Mr. Spicer	Mr.	Brad		Spicer	Assistant Commissioner	Louisiana Department of Agriculture & Forestry	Office of Soil/Water Conservation	P.O. Box 3554	Baton Rouge	LA	70821-3554
State	A	Mr. St. Romain	Mr.	Charles		St. Romain		Division of Administration	Louisiana State Land Office	P.O. Box 44124	Baton Rouge	LA	70804
Parish	E	Mr. Stefancik	Mr.	Steve		Stefancik		St. Tammany Parish Council, District 11		107 Royal Drive	Slidell	LA	70460
Other	S	Mr. Vandersteen	Mr.	Buck		Vandersteen		Executive Director	Louisiana Forestry Association	P.O. Drawer 5067	Alexandria	LA	71301

US 11 Norfolk Southern Railroad
SOV Mailing List
Environmental Assessment
H.000688.2

State	A	Mr. Adams	Mr.	Elliott		Adams	Director of Business Development	Louisiana Department of Economic Development	Office of Business Development	P.O. Box 94185	Baton Rouge	LA	70804-9185
State	A	Ms. Veillon	Ms.	Susan		Veillon, CFM	Floodplain Management Program Coordinator	Louisiana Department of Transportation and Development		1201 Capitol Access Road, 5th Floor	Baton Rouge	LA	70802
State	T	Chairman Verdun	Chairman	Charles		Verdin		Point au Chien Tribe		793 Aragon Road	Montegut	LA	70377
State	T	Chairman Verdun	Chairman	Randy		Verdun		Bayou Lafourche Band of the Biloxi Chitimacha		122 Oakwood Lane	Denham Springs	LA	70726
Federal	E	Senator Vitter	Senator	David		Vitter		United States Senate		2800 Veterans Memorial Blvd., Suite 2	Metairie	LA	70002
Federal	A	Mr. Washburn	Mr.	Eric		Washburn	CG Bridge Administrator, Western Rivers	Commander, 8th USCG District	Bridge Branch	1222 Spruce Street, Room 2.102d	St. Louis	MO	63103
State	A	Mr. Welsh	Mr.	James	H.	Welsh	Commissioner	Louisiana Department of Natural Resources	Office of Conservation	P.O. Box 94275	Baton Rouge	LA	70804-9275
State	S	Mr. Wilkins	Mr.	James	G.	Wilkins		Louisiana State University	Sea Grant Legal Advisory Service	227B Sea Grant Building	Baton Rouge	LA	70803
State	T	Chief Womack	Chief	Jackie		Womack		Four-Winds Cherokee Tribe		P.O. Box 118	Merryville	LA	70653
State	A	Captain Clark	Captain	Paul		Clark	Commander	Louisiana State Police, Troop L		2600 North Causeway	Mandeville	LA	70471
State								Bogue Chitto Pearl River Soil & Water Conservation District of Louisiana		1111 Washington Street	Franklinton	LA	70438
Federal								National Marine Fisheries Service	Habitat Conservation Division	LSU Center for Wetland RES	Baton Rouge	LA	70803-7535
State	A							Division of Administration	State Planning Office	PO Box 94095	Baton Rouge	LA	70804
Federal	A	Geological Survey						U.S. Department of Interior	U.S. Geological Survey	3535 South Sherwood Forest, Suite 120	Baton Rouge	LA	70816
State	A	Department of Interior						U.S. Department of Interior	National Park Service, NPS/Atlanta Federal Center	100 Alabama Street, SW	Atlanta	GA	30303
State	A							Louisiana Department of Natural Resources	Office of Mineral Resources	P.O. Box 2827	Baton Rouge	LA	70821
Local		Mr. McManus	Mr.			McManus				2045 Second Street, Suite 320	Slidell	LA	70458
Local								St. Tammany Historical Society Inc.		P.O. Box 1001	Mandeville	LA	70470-1001
Local								St. Tammany Parish Sheriff		P.O. Box 1120	Covington	LA	70433
State	E	Senator Crowe	The Honorable	A.G.		Crowe		The State Senate (District 1)		646 Carnation Street	Slidell	LA	70460
State	E	Ms. Ganucheau	Ms.	Jeanne-Marie		Ganucheau			Hale Boggs Federal Building	500 Poydras Street, Rm 1005	New Orleans	LA	70130
Local	A	Mr. Roesel	Mr.	Jeff		Roesel	Deputy Director	Regional Planning Commission	Regional Transportation Management Center	10 Veterans Memorial Blvd	New Orleans	LA	70124-1162
Local	A	Ms. Lala	Ms.	Rebecca		Lala	Traffic Engineer	St. Tammany Parish		21410 Koop Drive	Mandeville	LA	70471
State	A	Ms Altazan-Dixon	Ms.	Beth		Altazan-Dixon	EPS III, Performance Mangement	LDEQ/Office of the Secretary	Business and Community Outreach and Incentives Division	P.O. Box 4301	Baton Rouge	LA	70821-4301
State	G	Ms. Charrier	Ms.	Christine		Charrier	Program Manager	LA Department of Natural Resources	Office of Coastal Management, Permits Section	617 North Third Street, Suite 1078	Baton Rouge	LA	70804

**Louisiana Ecological Services Office****ESA Technical Assistance Form**General Information**Name:** Louisiana Department of Transportation and Development**Point of Contact:** ARCADIS-US (Scott Hoffeld)**Address:** 10352 Plaza Americana Dr**City:** Baton Rouge**State:** Louisiana**Zip Code:** 70816**Phone Number 1:** 2252921004 **Phone Number 2:** 2252921004**Email Address:** greg.badon@arcadis-us.comProposed Project Information**Project Reference ID:** 1024**Project Latitude:** 30.299803 **Project Longitude:** -89.774668**Project Parish(es):** Saint Tammany

Project Description: The US 11 Norfolk Southern Railroad project includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes for the segment of US Highway 11 (US 11) from Interstate 12 (I-12) south to US Highway 190 (US 190; Gause Boulevard) in Slidell, St. Tammany Parish, Louisiana. The logical termini for the proposed project are US 11 at I-12 and US 11 at US 190. The project consists of providing all necessary services required to prepare an Environmental Assessment (EA) in accordance with the National Environmental Policy Act, as amended, and the Federal Highway Administration's regulations and guidelines.

The purpose of the proposed project is to replace a structurally deficient bridge, while increasing capacity and alleviating congestion.

The project area is located to the east of Airport Road and west of Robert Boulevard (LA 1091) in Slidell, Louisiana. The project study area extends south along US 11 from I-12 to the intersection of US 11 at US 190. The EA will investigate the potential for effects to cultural resources, threatened and endangered species, natural resources, and the



Louisiana Ecological Services Office

ESA Technical Assistance Form

human environment within the study area. The proposed project would improve existing roadway infrastructure and will require additional right-of-way.

Based on the information provided, the proposed project is not an activity that would affect a federally listed threatened or endangered species; nor is there proposed or designated critical habitat present within this Parish.

Therefore, a "no effect" conclusion is appropriate. No further ESA coordination with the Service is necessary for the proposed action, unless there are changes in the scope or location of the proposed project or the project has not been initiated one year from the date of this letter.

If the proposed project has not been initiated within one year, follow-up coordination should be accomplished with the Service prior to making expenditures because our threatened and endangered species information is updated annually. If the scope or location of the proposed project is changed, coordination should occur as soon as such changes are made.

This finding completes project review by the Service for effects to Federal trust resources under our jurisdiction and currently protected by the ESA.

Please keep a copy of this pre-development coordination for your records. Additionally, if you would like a copy of this activity kept on file by the Service, please submit a copy to the Louisiana Ecological Services office.

Mailing Address: 646 Cajundome Blvd., Suite 400, Lafayette, LA 70506 Attn: Biological Science Technician
Email: Lafayette@fws.gov
Fax: 337/291-3139

If you have additional questions, please contact Louisiana ES Office Biological Science Technician at 337/291-3100 for further assistance.



Louisiana Ecological Services Office

ESA Technical Assistance Form

Project Type: Other

Does the project propose to obtain, remodel, refurbish, or rehabilitate existing structures in such a way that does not significantly alter the present capacity or use, and does not alter surrounding land areas that were previously undisturbed? **No**

Does the project propose to reconstruct, resurface, or enhance infrastructure and/or cityscape (e.g. streets, sewers, sidewalks, etc.) within the current footprint of the infrastructure and in a manner that does not disturb previously undisturbed ground? **No**

Is the construction project located entirely within the footprint of an established urban/suburban area (incorporated villages, towns, or cities)? **Yes**

Beam, Elizabeth

From: Trahan, Amy <amy_trahan@fws.gov>
Sent: Monday, May 13, 2013 8:56 AM
To: Beam, Elizabeth
Subject: Threatened and Endangered Species Reviews

Elizabeth,

I am writing to let you know about our self-screening website (www.fws.gov/lafayette) for threatened and endangered species reviews. The website is easy to use and it is a faster turnaround on response to projects. If you receive a "no effect" determination from this website, then you do not have to send that project to us at all; just print out the page and keep it for your records. If you get a page that tells you your project may have an issue and that you need to send it in for further review, just fill out the page with your project's information, print that out and send it to us with any other information that would help us review the project (i.e. maps). Our office would like everyone to use this website, due to staff shortages and, as I said earlier, a faster turnaround rate for consultants. If you could let your colleagues know about this websites as well, it would be appreciated. If you have any questions or problems with the website, please do not hesitate to contact me at the below phone number or email address.

Thank you for your time.

--

Amy Trahan
U.S. Fish and Wildlife Service
646 Cajundome Blvd., Suite 400
Lafayette, LA 70506
(337) 291-3126
amy_trahan@fws.gov



Office of the Secretary
PO Box 94245 | Baton Rouge, LA 70804-9245
ph: 225-379-3005 | fx: 225-379-3002

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

June 4, 2013

STATE PROJECT NO.:H.000688.2
F.A.P. NO.: H000688
ROUTE: LA 11
NAME: US 11 NORFOLK SOUTHERN RAILROAD
PARISH: ST. TAMMANY

Elizabeth Beam, AICP
Senior Planner/ Scientist
Arcadis
10352 Plaza Americana Drive
Baton Rouge, LA 70816

Subject: Solicitation of Views

Dear Ms. Beam:

Enclosed is a copy of the City of Slidell's Flood Insurance Rate Maps (FIRM's) indicating the proposed project.

During the construction, there must be allowance for the adequate flow of water and assurance that there will be no back up of water. There must be no instance of the creation of flooding where there was no flooding prior to construction. At this time, consideration must be given to the responsibility for cleaning debris and keeping the surrounding area clear so as not to interfere with its function.

In order to assure compliance with the City of Slidell requirements for the National Flood Insurance Program (NFIP), and ensure that appropriate permits are obtained, please contact the floodplain administrator for the City of Slidell. The contact person is: Donna O'Dell, P.E., P.O. Box 828, Slidell, LA, 70459, and telephone no. 985-646-4270.

We thank you for the opportunity to comment on this project. If you need additional information, please contact our office, (225) 379-3005.

Sincerely,

Susan Veillon, CFM
Floodplain Management Program Coordinator

pc: Donna O'Dell, P.E.

Beam, Elizabeth

From: Susan Veillon (DOTD) <Susan.Veillon@LA.GOV>
Sent: Tuesday, June 04, 2013 1:41 PM
To: Beam, Elizabeth
Subject: SOV, LA 11, SLIDELL,LA
Attachments: LA 11, FROM GAUSE TO I12, SLIDELL, ST TAMMANY PH, MAP 1 OF 2.pdf; LA 11, FROM GAUSE TO I12, SLIDELL, ST TAMMANY PH, MAP 2 OF 2.pdf; LA 11, SLIDELL, ST TAMMANY PARISH.doc

Beth,

I will mail the original. Thanks.

Susan

Susan Veillon, CFM
Floodplain Management Program Coordinator
1201 Capitol Access Road, 5th Floor
Baton Rouge, LA 70802
Phone 225-379-3005
Fax 225-379-3002

Beam, Elizabeth

From: Robert Lott <Robert.Lott@LA.GOV>
Sent: Tuesday, July 09, 2013 3:48 PM
To: Beam, Elizabeth
Subject: FW: US 11 Norfolk Southern Railroad

FYI

Thanks,
Bobby Lott
DOTD - Environmental Section
225-242-4504

From: Charles E. Williams [<mailto:eddie@stpgov.org>]
Sent: Tuesday, July 09, 2013 10:55 AM
To: Robert Lott
Subject: US 11 Norfolk Southern Railroad

Robert,

Sorry for the late response. The letter was lost in transmittal to me. If possible, St Tammany Parish would like to participate in the environmental assessment of of the US 11 project. If you need to contact me, my cell is 985-630-5087.

Thanks
Eddie Williams, PE
Director of Engineering
St Tammany Parish

Beam, Elizabeth

From: Steve Stefancik <steves@stpgov.org>
Sent: Friday, May 17, 2013 3:50 PM
To: Beam, Elizabeth
Subject: Comments on State Project No. H.000688.2

Follow Up Flag: FollowUp
Flag Status: Flagged

This is a very critical project for the improvement of traffic and transportation in this area. I do not know of any detrimental environmental impacts to the area.

I do not believe that the "no build" alternative is a viable concept and should not be considered.

Steve Stefancik
St. Tammany Parish Council
District 11



JAY DARDENNE
LIEUTENANT GOVERNOR

State of Louisiana
OFFICE OF THE LIEUTENANT GOVERNOR
DEPARTMENT OF CULTURE, RECREATION & TOURISM
OFFICE OF CULTURAL DEVELOPMENT

CHARLES R. DAVIS
DEPUTY SECRETARY

PAM BREAUX
ASSISTANT SECRETARY

May 20, 2013

Elizabeth Beam
Arcadis U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge, LA 70816

Re: Section 106 Request for Additional Information
State Project No. H.000688.2
US 11 Norfolk Southern Railroad
St. Tammany Parish, LA

Dear Ms. Beam:

Thank you for your letter of May 3, 2013, concerning the above-referenced undertaking. We are unable to complete the Section 106 review at this time due to the submittal of insufficient documentation. We will need the following information to complete our review for the aforementioned project:

- Name of federal agency, agency involvement (Funding, license/permit, etc. and description of the undertaking (Detailed description of project).
- Applicant contact information (Name, address, phone number and email address).
- Agency contact information (Name, address, phone number and email address).
- Description of the Area of Potential Effects (APE). The APE can be direct or indirect. It is defined as "the geographic area or areas within which an undertaking may cause changes in the character or use of historic properties, if any such properties exist." (Include the latitude/longitude of the undertaking location and APE)
- Description of all historic properties within and adjacent to the APE. The historic standing structure is any structure fifty years of age and older. Under Section 106, it is the responsibility of the federal agency or its designee to identify all structures listed or eligible for listing in the National Register of Historic Places.
- Detailed project scope of work including design plans.
- Map and site plan showing APE and exact location of project undertaking.

Elizabeth Beam
May 20, 2013
Page 2

Photographs of the entire APE and project location. Photographs of all historic (fifty years of age and older) within the APE. Buildings should be documented showing diagonal views of front and side and rear and opposite side of the building. All photos should be keyed to a site map and project plans if applicable.

If you have any questions, please contact Mike Varnado in the Division of Historic Preservation at (225) 219-4596 or mvarnado@crt.la.gov.

Sincerely,



Pam Breaux
State Historic Preservation Officer

PB:MV:s

Beam, Elizabeth

From: Paul Carroll <pcarroll@stpgov.org>
Sent: Thursday, May 23, 2013 1:29 PM
To: Beam, Elizabeth
Cc: Alan Pelegrin; Elizabeth D. Smythe; Daniel Bond; Donna ODell; Charles E. Williams; Rebecca Lala
Subject: US 11 Norfolk Southern Railroad Environmental Assessment Comments

I have reviewed the proposed widening of HWY 11 between I-12 and Gause Boulevard.

One concern would be that the inlet capacity and storage capacity of the large on-line retention pond adjacent to HWY 11 not be reduced. St. Tammany Parish has a flood model of this pond which was made by the New Orleans District USACE for a future flood control project which utilizes this pond. We request that the consultant works with St. Tammany Parish, the City of Slidell, and the USACE to ensure compatibility with the W-14 SELA project.

Where practical, it would be helpful if widening could be aligned to minimize impacts to existing businesses.

Thanks

Paul Carroll, P.E.
Drainage Engineer
St. Tammany Parish Government, Dept. of Engineering
898-2750

Any e-mail may be construed as a public document, and may be subject to a public records request.

United States Department of Agriculture



Natural Resources Conservation Service
3737 Government Street
Alexandria, LA 71302

(318) 473-7751
Fax: (318) 473-7626

May 6, 2013

Beth Beam
Arcadis U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge, LA 70816

RE: US 11 Norfolk Southern Railroad - State Project No.: H.000688.2

Dear Ms. Beam:

I have reviewed the above referenced project for potential requirements of the Farmland Protection Policy Act (FPPA) and potential impact to Natural Resource Conservation Service projects in the immediate vicinity.

Projects are subject to FPPA requirements if they may irreversibly convert farmland (directly or indirectly) to nonagricultural use and are completed by a federal agency or with assistance from a federal agency. For the purpose of FPPA, farmland includes prime farmland, unique farmland, and land of statewide or local importance. Farmland subject to FPPA requirements can be forest land, pastureland, cropland, or other land, but not water or urban built-up land.

The project map submitted with your request indicates that the proposed construction areas will not impact prime farmland and therefore is exempt from the rules and regulations of the Farmland Protection Policy Act (FPPA) - Subtitle I of Title XV, Section 1539-1549. Furthermore, we do not predict impacts to NRCS work in the vicinity.

For specific information about the soils found in the project area, please visit our Web Soil Survey at the following location: <http://websoilsurvey.nrcs.usda.gov/>

Please direct all future correspondence to me at the address shown above.

Respectfully,

A handwritten signature in blue ink that reads "Sarah Haymaker".

Sarah Haymaker
Acting State Conservationist



BOBBY JINDAL
GOVERNOR

State of Louisiana
DEPARTMENT OF WILDLIFE AND FISHERIES
OFFICE OF WILDLIFE

ROBERT J. BARHAM
SECRETARY
JIMMY L. ANTHONY
ASSISTANT SECRETARY

Date May 16, 2013

Name Elizabeth Beam

Company ARCADIS

Street Address 10352 Plaza Americana Drive

City, State, Zip Baton Rouge, LA 70816

Project US 11 Norfolk Southern Railroad
State Project No. H.000688.2

Project ID 1592013

Invoice Number 13051610

Personnel of the Habitat Section of the Coastal & Nongame Resources Division have reviewed the preliminary data for the captioned project. After careful review of our database, no impacts to rare, threatened, or endangered species or critical habitats are anticipated for the proposed project. No state or federal parks, wildlife refuges, scenic streams, or wildlife management areas are known at the specified site within Louisiana's boundaries.

The Louisiana Natural Heritage Program (LNHP) has compiled data on rare, endangered, or otherwise significant plant and animal species, plant communities, and other natural features throughout the state of Louisiana. Heritage reports summarize the existing information known at the time of the request regarding the location in question. The quantity and quality of data collected by the LNHP are dependent on the research and observations of many individuals. In most cases, this information is not the result of comprehensive or site-specific field surveys; many natural areas in Louisiana have not been surveyed. This report does not address the occurrence of wetlands at the site in question. Heritage reports should not be considered final statements on the biological elements or areas being considered, nor should they be substituted for on-site surveys required for environmental assessments. LNHP requires that this office be acknowledged in all reports as the source of all data provided here. If at any time Heritage tracked species are encountered within the project area, please contact the LNHP Data Manager at 225-765-2643. If you have any questions, or need additional information, please call 225-765-2357.

Sincerely,


Amity Bass, Coordinator
Natural Heritage Program



FEMA

FEDERAL EMERGENCY MANAGEMENT AGENCY
REGION VI
MITIGATION DIVISION

NOTICE REVIEW/ENVIRONMENTAL CONSULTATION

We have no comments to offer. We offer the following comments:

WE WOULD REQUEST THAT THE PARISH FLOODPLAIN ADMINISTRATOR BE CONTACTED FOR THE REVIEW AND POSSIBLE PERMIT REQUIREMENTS FOR THIS PROJECT. IF FEDERALLY FUNDED, WE WOULD REQUEST PROJECT TO BE IN COMPLIANCE WITH EO11988 & EO 11990.

Alan Pelegrin
FPA
St. Tammany Parish
21490 Koop Drive
Mandeville, LA 70471
985-898-2574

REVIEWER:

Mayra G. Diaz
Floodplain Management and Insurance Branch
Mitigation Division
(940) 898-5541

DATE: May 7, 2013



RECEIVED FIC MAIL ROOM
FEMA REGION VI
2013 APR 30 P 1:29

Mr. Greg Solvey
Attn: Myra G. Diaz,
Natural Hazards Program Specialist
FEMA Region VI
800 North Loop 288
Denton, TX 76201

ARCADIS U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge
Louisiana 70816
Tel 225 292 1004
Fax 225 218 9677
www.arcadis-us.com

Subject:
Solicitation of Views and
Initiation of Section 106 Consultation
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana
State Project No. H.000688.2
F.A.P No. H000688

INFRASTRUCTURE

Date:
3 May 2013

Dear Mr. Solvey:

Contact:
Beth Beam

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns and reach agreeable decisions while taking into account the interests of all parties.

Extension:
215

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

Email:
elizabeth.beam@
arcadis-us.com

A Stage 0 Feasibility Study for this project was completed in 2008. A project summary and figures are attached for your review. If you would like to review the Stage 0 Study in its entirety, please request a copy from Beth Beam by e-mail at elizabeth.beam@arcadis-us.com or by U.S. mail to Ms. Beth Beam, ARCADIS, 10352 Plaza Americana Drive, Baton Rouge, Louisiana 70816. A project overview, project study area map, and figures are attached for your review.

Our ref:
LA003143.0000
LDOTD/3143/C/1/kp

We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

Imagine the result



Mr. Greg Solvey
3 May 2013

Closing

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by **Monday, June 3, 2013**. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688.2 in your reply.

Sincerely,

ARCADIS U.S., Inc.

A handwritten signature in black ink that reads "Elizabeth Beam". The signature is fluid and cursive.

Elizabeth Beam, AICP
Senior Planner/Scientist

Attachments



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY
REGION 6
1445 ROSS AVENUE, SUITE 1200
DALLAS, TX 75202-2733

May 8, 2013

Ms. Elizabeth Beam, AICP
Senior Planner/Scientist
Arcadis U.S., Inc.
10352 Plaza Americana Dr.
Baton Rouge, LA 70816

Dear Ms. Beam:

We have received your May 3, 2013, letter via email requesting our evaluation of the potential environmental impacts which might result from the following project:

**Bridge Replacement & Lane Widening
US 11 over Norfolk Southern Railroad
from I-12 south to US Hwy 190
SP No. H.000688.2
F.A.P. No. H.000688.2
St. Tammany Parish, Louisiana**

The project, is located on the Southern Hills aquifer system which has been designated a sole source aquifer by the EPA. Based on the information provided for the project, we have determined that the project, as proposed, should not have an adverse effect on the quality of the ground water underlying the project site.

This approval of the proposed project does not relieve the applicant from adhering to other State and Federal requirements, which may apply. This approval is based solely upon the potential impact to the quality of ground water as it relates to the EPA's authority pursuant to Section 1424(e) of the Safe Drinking Water Act.

If you did not include the Parish; a legal description; project location and the latitude and longitude if available, please do so in future Sole Source Aquifer correspondence.

If you have any questions on this letter or the sole source aquifer program please contact me at (214) 665-7133.

Sincerely yours,

A handwritten signature in blue ink, appearing to read "Michael Bechdol".

Michael Bechdol, Coordinator
Sole Source Aquifer Program
Ground Water/UIC Section

cc: Jesse Means, LDEQ
Noel Ardoin, LDOTD



RECEIVED
JUN 10 2013

BOBBY JINDAL
GOVERNOR

State of Louisiana
DEPARTMENT OF NATURAL RESOURCES
OFFICE OF CONSERVATION

BY:

STEPHEN CHUSTZ
INTERIM SECRETARY

JAMES H. WELSH
COMMISSIONER OF CONSERVATION

June 5, 2013

TO: Ms. Elizabeth Beam
Senior Planner/Scientist
Arcadis U.S. Inc.
10352 Plaza Americana Drive
Baton Rouge, Louisiana 70816

RE: Solicitation of Views
State Project No. H.000688.2
US 11 Norfolk Southern Railroad
St. Tammany Parish

Dear Ms. Beam:

In response to your letter dated May 3, 2013, concerning the referenced matter, please be advised that the Office of Conservation collects and maintains many types of information regarding oil and gas exploration, production, distribution, and other data relative to the petroleum industry as well as related and non-related injection well information, surface mining and ground water information and other natural resource related data. Most information concerning oil, gas and injection wells for any given area of the state, including the subject area of your letter can be obtained through records search via the SONRIS data access application available at:

<http://www.dnr.louisiana.gov>

A review of our computer records for the referenced project area indicates that there is, or was, present in the project area a well that was drilled in the search for oil and gas. The DNR water well database indicates that there are registered water wells in the vicinity of the project area. Also, unregistered water wells may be located in the area.

The Office of Conservation maintains records of all activities within its jurisdiction in paper, microfilm or electronic format. These records may be accessed during normal business hours, Monday through Friday, except on State holidays or emergencies that require the Office to be closed. Please call 225-342-5540 for specific contact information or for directions to the Office of Conservation, located in the LaSalle Building, 617 North Third Street, Baton Rouge, Louisiana. For pipelines and other underground hazards, please contact Louisiana One Call at 1-800-272-3020 prior to commencing operations. Should you need to direct your inquiry to any of our Divisions, you may use the following contact information:

<u>Division</u>	<u>Contact</u>	<u>Phone No.</u>	<u>E-mail Address</u>
Engineering	Jeff Wells	225-342-5638	jeff.wells@la.gov
Pipeline	Steven Giambrone	225-342-2989	steven.giambrone@la.gov
Injection & Mining	Laurence Bland	225-342-5515	laurence.bland@la.gov
Geological	Mike Kline	225-342-3335	mike.kline@la.gov
Environmental	Gary Snellgrove	225-342-7222	gary.snellgrove@la.gov

If you have difficulty in accessing the data via the referenced website because of computer related issues, you may obtain assistance from our technical support section by selecting Help on the SONRIS tool bar and submitting an email describing your problems and including a telephone number where you may be reached.

Sincerely,

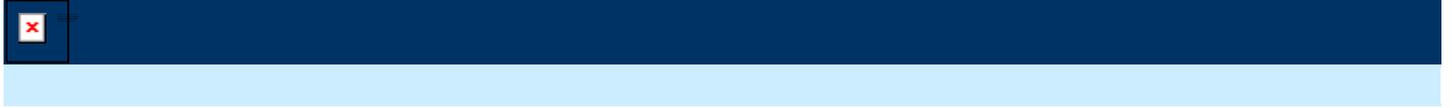


James H. Welsh
Commissioner of Conservation

JHW:MSK

Beam, Elizabeth

From: Office of Coastal Management <bpel.mail@la.gov>
Sent: Monday, August 12, 2013 2:16 PM
To: LEAANN.BAKER@LA.GOV; Beam, Elizabeth
Subject: P20130672- comment received



PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

Coastal Use Permit Application Information

Applicant: LA DOTD
Project: US 11 Norfolk Southern Railroad Environmental Assessment Route US 11
Project Parish(es): SAINT TAMMANY

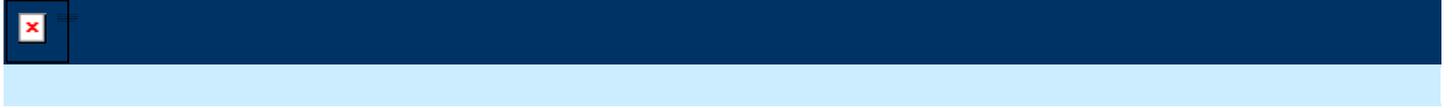
OCM Analyst: Sharon Mccarthy
Preliminary Determination: SOV Application Required
Application Modification: N/A

Comment from Other has been received for the referenced Coastal Use Permit Application. Please be advised that you may be required to address any issues raised by these comments.

[View Comments](#)

Beam, Elizabeth

From: Office of Coastal Management <bpel.mail@la.gov>
Sent: Tuesday, July 09, 2013 2:45 PM
To: LEAANN.BAKER@LA.GOV; SHARON.TRAHAN@LA.GOV; Beam, Elizabeth
Subject: P20130672- comment received



PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

Coastal Use Permit Application Information

Applicant: LA DOTD
Project: US 11 Norfolk Southern Railroad Environmental Assessment Route US 11
Project Parish(es): SAINT TAMMANY

OCM Analyst: Sharon Mccarthy
Preliminary Determination: SOV Application Required
Application Modification: N/A

Comment from Other has been received for the referenced Coastal Use Permit Application. Please be advised that you may be required to address any issues raised by these comments.

[View Comments](#)

Beam, Elizabeth

From: Office of Coastal Management <bpel.mail@la.gov>
Sent: Tuesday, May 28, 2013 1:12 PM
To: LEAANN.BAKER@LA.GOV; SHARON.TRAHAN@LA.GOV; Beam, Elizabeth
Subject: P20130672- comment received

Follow Up Flag: Flag for follow up
Flag Status: Flagged



PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

Coastal Use Permit Application Information

Applicant: LA DOTD
Project: US 11 Norfolk Southern Railroad Environmental Assessment Route US 11
Project Parish(es): SAINT TAMMANY

OCM Analyst: Sharon Mccarthy
Preliminary Determination: SOV Application Required
Application Modification: N/A

Comment from Other has been received for the referenced Coastal Use Permit Application. Please be advised that you may be required to address any issues raised by these comments.

[View Comments](#)

Beam, Elizabeth

From: Office of Coastal Management <bpel.mail@la.gov>
Sent: Monday, May 20, 2013 4:17 PM
To: LEAANN.BAKER@LA.GOV; SHARON.TRAHAN@LA.GOV; TIM.KILLEEN@LA.GOV;
DBOND@STPGOV.ORG; DBUTLER@WLF.LA.GOV; Beam, Elizabeth;
UCM_MAIL@LA.GOV
Subject: P20130672 - Processing Complete



PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

Coastal Use Permit Application Information

Applicant: LA DOTD
Project: US 11 Norfolk Southern Railroad Environmental Assessment Route US 11
Project Parish(es): SAINT TAMMANY

OCM Analyst: Sharon Mccarthy
Final Determination: SOV Application Required
Application Modification: N/A

Processing of the above application has been completed. Click the link below to view the final determination:

[Authorization](#)

P20130672



Ms. Christine Charrier
Program Manager
Office of Coastal Management, Permits Section
LA Department of Natural Resources
617 North Third Street, Suite 1078
Baton Rouge, Louisiana 70804

ARCADIS U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge
Louisiana 70816
Tel 225.292.1004
Fax 225.218.9677
www.arcadis-us.com

Subject:
Environmental Assessment
US 11 Norfolk Southern Railroad
Route US 11
St. Tammany Parish, Louisiana
State Project No. H.000688.2
F.A.P. No. H000688

INFRASTRUCTURE

30 July 2013

Dear Ms. Charrier:

On behalf of the Federal Highway Administration (FHWA) in cooperation with the Louisiana Department of Transportation and Development (LADOTD), we invite you to an officials meeting for the above-referenced project. The meeting will be held at the Slidell City Auditorium, 2056 2nd Street, Slidell, Louisiana 70458, from **3:00 p.m. to 4:00 p.m. on Thursday, August 22, 2013**. The consultant team will present a project overview and representatives from LADOTD, FHWA, and the consultant team will facilitate a discussion on issues material to the environmental review process. The officials meeting will be followed by a brief **key stakeholders meeting from 4:00 p.m. to 4:30 p.m.**

Contact:
Beth Beam

Extension:
215

Email:
elizabeth.beam@arcadis-us.com

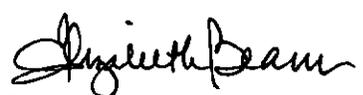
We would also like to remind you that a **public meeting will be held at the same location on the same day from 5:00 p.m. to 7:00 p.m.** The public has been invited to this meeting to learn more about the project, discuss issues, and ask questions. Comments will be accepted at the public meeting and by mail postmarked no later than **Monday, September 2, 2013**.

Our ref:
LA003143.0001.00003
LADOTD/3143/C/8/jk

If you would like to receive invitations for meetings and other notices via email, please reply to me at elizabeth.beam@arcadis-us.com to be added to the project database.

Sincerely,

ARCADIS U.S., Inc.


Elizabeth Beam, AICP
Associate Project Manager

2013 AUG -1 PM 1:39
PROJECT COASTAL DRUGS

Imagine the result

P20130672



DEPARTMENT OF THE ARMY
NEW ORLEANS DISTRICT, CORPS OF ENGINEERS
P. O. BOX 60267
NEW ORLEANS, LOUISIANA 70160-0267

REPLY TO
ATTENTION OF

Operations Division
Operations Manager,
Completed Works

Elizabeth Beam, AICP
Senior Planner/ Scientist
ARCADIS U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge, LA 70816

Dear Ms. Beam:

This is in response to your Solicitation of Views request dated May 3, 2013, concerning the Route US-11 widening and bridge replacement at Slidell in St. Tammany Parish, Louisiana.

We have reviewed your request for potential Department of the Army regulatory requirements and impacts on any Department of the Army projects.

We do not anticipate any adverse impacts to any Corps of Engineers projects.

Based on the limited information provided, we have determined that information and signatures obtained from recent maps, aerial photography, and local soil surveys concerning the proposed project are indicative of the occurrence of waters of the United States, including wetlands. Department of the Army (DA) permits are required prior to the deposition and/or redistribution of dredged or fill material into jurisdictional waters and wetlands.

This preliminary determination is advisory in nature. If an approved delineation is needed, please furnish us with the detailed field data concerning vegetations, soils, and hydrology that we require for all jurisdictional decisions. The fact that a field wetland delineation/determination has not been completed does not alleviate your responsibility to obtain the proper DA permits prior to working in jurisdictional wetlands or waters occurring on this property.

Please be advised that this property is in the Louisiana Coastal Zone. For additional information regarding coastal use permit requirements, contact Ms. Christine Charrier, Office of Coastal Management, Louisiana Department of Natural Resources at (225) 342 7953.

Off-site locations of activities such as borrow, disposals, haul-and detour-roads and work mobilization site developments may be subject to Department of the Army regulatory

OFFICE OF COASTAL MANAGEMENT

2013 JUL -9 PM 2:09

RECEIVED

requirements and may have an impact on a Department of the Army project.

You should apply for said permit well in advance of the work to be performed. The application should include sufficiently detailed maps, drawings, photographs, and descriptive text for accurate evaluation of the proposal.

Please contact Mr. Robert Heffner, of our Regulatory Branch by telephone at (504) 862-1288, or by e-mail at Robert.A.Heffner@usace.army.mil for questions concerning wetlands determinations or need for on-site evaluations. Questions concerning regulatory permit requirements may be addressed to Mr. Michael Farabee by telephone at (504) 862-2292 or by email at Michael.V.Farabee@usace.army.mil.

Future correspondence concerning this matter should reference our account number MVN-2013-01421-SA. This will allow us to more easily locate records of previous correspondence, and thus provide a quicker response.

We apologize for missing the target date of June 3, 2013 listed in your request. Thank you for your patience in this matter.

Sincerely,

Karen L. Clement
Solicitation of Views Manager

Copy Furnished:

Ms. Christine Charrier
Coastal Zone Management
Department of Natural Resources
Post Office Box 44487
Baton Rouge, Louisiana 70804-4487



State of Louisiana
DEPARTMENT OF NATURAL RESOURCES
OFFICE OF COASTAL MANAGEMENT

05/20/2013

ARCADIS U.S., INC.
10352 PLAZA AMERICANA DRIVE
BATON ROUGE, LA 70816
Attn: Elizabeth Beam

RE: P20130672, Solicitation of Views
LA DOTD

Description: Proposed US 11 Norfolk Southern Railroad Project (State Project No. H.000688.2) includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes from US Hwy. 11 to US Hwy. 190.

Location: Lat 30° 18' 30"N / Long 89° 46' 20"W; US Hwy 11 from I-12 south to US Hwy 190 in Slidell.

Saint Tammany Parish, LA

Dear Elizabeth Beam:

We have received your Solicitation of Views for the above referenced project, which has been found to be inside the Louisiana Coastal Zone. In order for us to properly review and evaluate this project, we require that a complete Coastal Use Permit Application packet (Joint Application Form, locality maps, project illustration plats with plan and cross section views, etc.) along with the appropriate application fee be submitted to our office. Using your complete application, we can provide you with an official determination, and begin the processing of any Coastal Use Permit that may be required for your project. You may obtain a free application packet by calling our office at (225) 342-7591 or (800)-267-4019, or by visiting our website at <http://www.dnr.state.la.us/crm/coastmgt/cup/cup.asp>.

We recommend that, during your planning process, you make every effort to minimize impacts to vegetated wetlands. As our legislative mandate puts great emphasis on avoiding damages to these habitats, in many cases the negotiations involved in reducing such disturbances and developing the required mitigation to offset the lost habitat values delay permit approval longer than any other factor. Additionally, the following sensitive features may require additional processing time by the appropriate resource agencies: Map ID: 001.HP.13 Slidell Ring Levee; Coastal Protection and Restoration Authority (CPRA); contact Mandy Green @ 225-342-1357 or mandy.green@la.gov for more information.

Should you desire additional consultation with our office prior to submitting a formal application, we recommend that you call and schedule a pre-application meeting with our Permit Section staff. Such a preliminary meeting may be helpful, especially if a permit application that is as complete as possible

is presented for evaluation at the pre-application meeting.

If you have any questions, would like to request an application packet or would like to schedule a pre-application meeting, please contact Sharon Mccarthy at (225) 342-6140 or Sharon.McCarthy@la.gov.

Sincerely,

A handwritten signature in black ink that reads "Karl L. Morgan". The signature is written in a cursive style with a long, sweeping underline.

Karl L. Morgan
Administrator

Karl L. Morgan/sm

Attachments

P20130672, Solicitation of Views
LA DOTD
05/20/2013
Page 3

Final Plats:

1) [P20130672](#) [Final Plats](#) [05/07/2013](#)

cc: Jessica Diez, OCM w/plats
Tim Killeen, CMD/FI w/plats
Saint Tammany Parish w/plats
LA DOTD w/plats

BOBBY JINDAL
GOVERNOR



STEPHEN CHUSTZ
INTERIM SECRETARY

State of Louisiana
DEPARTMENT OF NATURAL RESOURCES
OFFICE OF COASTAL MANAGEMENT

05/09/2013

ARCADIS U.S., INC.
10352 PLAZA AMERICANA DRIVE
BATON ROUGE, LA 70816
Attn: Elizabeth Beam

**RE: P20130672, Solicitation of Views
LA DOTD**

Description: US 11 Norfolk Southern Railroad Project (State Project No. H.000688.2) includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes

Location: Lat 30° 18' 30"N / Long 89° 46' 20"W; US Hwy 11 from I-12 south to US Hwy 190 in Slidell.

Saint Tammany Parish, LA

Dear Elizabeth Beam:

You are hereby advised that your application for a Coastal Use Permit (CUP) has been determined to be complete and review by the State for compliance with the Louisiana Coastal Resource Program (LCRP) and consistency with the federal Coastal Zone Management Act (CZMA) has begun. Additionally, it has been determined that your proposed activity is a use of state concern in accordance with Louisiana Revised Statute 49:214.5.

All correspondence and calls regarding this application should reference the Coastal Use Permit Number (P#) indicated above. Please note that all information concerning your application is in our database and is updated throughout the day as changes to the status of the application occur.

Your application can be found on our [Webpage](#).

Should you have any questions, please check the online database or contact the assigned permit analyst: Sharon Mccarthy at (225) 342-6140 or Sharon.McCarthy@la.gov. Be sure to reference the above Coastal Use Permit Number.



Permit Coordinator

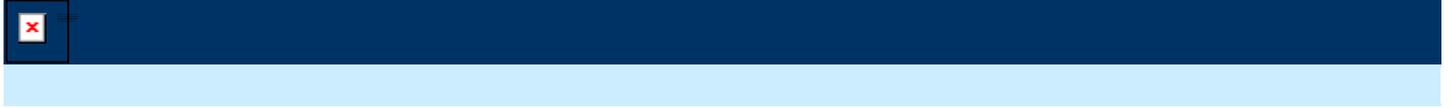
CM

cc: Martin Mayer, COE

LA DOTD

Beam, Elizabeth

From: Office of Coastal Management <bpel.mail@la.gov>
Sent: Tuesday, May 07, 2013 3:49 PM
To: LEAANN.BAKER@LA.GOV; Beam, Elizabeth
Subject: P20130672 - Joint Permit Application Received



PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

Coastal Use Permit Application Information

Applicant: LA DOTD
Project: US 11 Norfolk Southern Railroad Environmental Assessment Route US 11
Project Parish(es): SAINT TAMMANY

OCM Analyst:
Preliminary Determination:
Application Modification: N/A

Thank you for using Office of Coastal Management's on-line application process. Your application has been received and has been assigned the following number: **P20130672**

You will be contacted within 5 business days regarding the status of your application. You may also follow the progress of your application on-line at:

[Item Tracking \(text\)](#)
[Application](#)
[Make Comments](#)
[Application Invoice](#)

Beam, Elizabeth

From: Christine Charrier <Christine.Charrier@LA.GOV>
Sent: Tuesday, May 07, 2013 3:12 PM
To: Beam, Elizabeth
Subject: RE: US 11 NS RR Bridge EA (H000688): Solicitation of Views

Thank you Ms. Beam for including us early in the process. We will review the information and provide a response by the deadline.

Christine Charrier
OCM/PMD – Permits Program Manager
614 N. Third Street, Suite 1078
Baton Rouge, LA 70804
Ph: 225-342-7953
Fax: 225-342-6760
Email: christine.charrier@la.gov
Webpage: www.dnr.louisiana.gov

From: Beam, Elizabeth [<mailto:Elizabeth.Beam@arcadis-us.com>]
Sent: Monday, May 06, 2013 11:58 AM
To: Christine Charrier
Subject: US 11 NS RR Bridge EA (H000688): Solicitation of Views

Ms. Charrier:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns. In addition to identifying any concerns or issues as mentioned, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested.

A project overview, project study area map, and figures are attached for your review.

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by Monday, June 3, 2013. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688 in your reply.

Thank you.

Beth Beam MS, AICP | Senior Planner/Scientist | elizabeth.beam@arcadis-us.com
ARCADIS U.S., Inc. | 10352 Plaza Americana Drive | Baton Rouge, LA 70816
T 225.292.1004 | M 225.335.0134 | F 225.218.9677
www.arcadis-us.com

ARCADIS, Imagine the result

Please consider the environment before printing this email.

NOTICE: This e-mail and any files transmitted with it are the property of ARCADIS U.S., Inc. and its affiliates. All rights, including without limitation copyright, are reserved. The proprietary information contained in this e-mail message, and any files transmitted with it, is intended for the use of the recipient(s) named above. If the reader of this e-mail is not the intended recipient, you are hereby notified that you have received this e-mail in error and that any review, distribution or copying of this e-mail or any files transmitted with it is strictly prohibited. If you have received this e-mail in error, please notify the sender immediately and delete the original message and any files transmitted. The unauthorized use of this e-mail or any files transmitted with it is prohibited and disclaimed by ARCADIS U.S., Inc. and its affiliates. Nothing herein is intended to constitute the offering or performance of services where otherwise restricted by law.



State of Louisiana
Department of Health and Hospitals
Office of Public Health

May 30, 2013

Elizabeth Beam
ARCADIS U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge, LA 70816

Re: Solicitation of Views and Initiation of Section 106 Consultation US 11 Norfolk Southern
Railroad Environmental Assessment, Route US 11, St. Tammany Parish, Louisiana, State Project
No. H000688.2, F.A.P No. H000688.

This office is in receipt of a Solicitation of Views regarding the above referenced project(s).

Based upon the information received from your office we have no objection to the referenced project(s) at this time. The applicant shall be aware of and comply with any and all applicable Louisiana State Sanitary Code regulations (LAC 51, as applicable). Furthermore, should additional project data become available to this office that in any way amend the information upon which this office's response has been based, we reserve the right of additional comments on the referenced project(s).

In the event of any future discovery of evidence of non-compliance with the Louisiana Administrative Code Title 51 (Public Health-Sanitary Code) and the Title 48 (Public Health-General) regulations or any applicable public health laws or statutes which may have escaped our awareness during the course of this cursory review, please be advised that this office's preliminary determination on this Solicitation of View of the project(s) shall not be construed as absolving the applicant of responsibility, if any, with respect to compliance with the Louisiana Administrative Code Title 51 (Public Health-Sanitary Code) and the Title 48 (Public Health-General) regulations or any other applicable public health laws or statutes.

Sincerely,

A handwritten signature in cursive script that reads "Yuanda Zhu".

Yuanda Zhu
Louisiana Department of Health and Hospitals, Office of Public Health
Engineering Services
Telephone: (225) 342-7432
Electronic mail: yuanda.zhu@la.gov

Beam, Elizabeth

From: Tim Mathison <tmathison@cityofslidell.org>
Sent: Tuesday, May 28, 2013 5:51 PM
To: Beam, Elizabeth
Cc: Freddy D. Drennan; Tim Mathison
Subject: LA003143.0000

Follow Up Flag: FollowUp
Flag Status: Flagged

Dear Ms. Beam,

On behalf of Slidell Mayor Freddy Drennan I am responding to your letter dated May 3, 2013, relative to the LADOTD State Project No. H.000688.2, F.A.P. No. H000688.

Our comments/concerns are as follows:

- (1) The storage capacity of the large detention pond adjacent to Hwy. 11 should not be reduced by the widening of the highway;**
- (2) The design consultants coordinate with the City of Slidell, St. Tammany Parish and the U.S. Army Corps of Engineers so as to ensure compatibility of the project with various drainage projects in the area;**
- (3) Maintain or improve current drainage capacities along the entire project area;**
- (4) Minimize the impact the widening of the highway would have on adjacent businesses;**
- (5) An at grade railroad crossing should not be considered due to safety concerns;**
- (6) A No Build Alternative would not be acceptable.**

In the event we think of any other comments or concerns we will present them to you prior to the June 3rd deadline.

Thank you.

Tim Mathison, Esq.

Chief Administrative Officer

City of Slidell

2045 Second Street, Ste. 304

Slidell, Louisiana 70458

985-646-4330

CONFIDENTIALITY NOTICE: This electronic message, including any attachments, is intended to be privileged and confidential information for the sole use of the intended recipient(s). Any unauthorized review, use, disclosure or distribution is prohibited. If you have received this electronic mail transmission in error, notify the sender by reply e-mail and delete all copies from your system.

Beam, Elizabeth

From: Hauck, Allison <ahauck@ldaf.state.la.us>
Sent: Tuesday, May 21, 2013 3:24 PM
To: Beam, Elizabeth
Subject: Letter from Brad Spicer
Attachments: No comment letter example - Brad.doc

May 21, 2013

Ms. Beth Beam, ARCADIS
10352 Plaza Americana Drive
Baton Rouge, Louisiana 70816

RE: Solicitation of Views and
Initiation of Section 106 Consultation
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana
State Project No. H. 000688.2
F.A.P. No. H000688

Dear Ms. Beam:

This office has no comment or objection to this project.

Sincerely,

Bradley E. Spicer
Assistant Commissioner
Louisiana Department of Ag & Forestry
Office of Soil & Water Conservation

BES:ah



Mr. Brad Rieck
 Deputy Supervisor
 U.S. Fish & Wildlife Service
 Lafayette Field Office
 646 Cajundome Boulevard, Suite 400
 Lafayette, LA 70506

ARCADIS U.S., Inc.
 10352 Plaza Americana Drive
 Baton Rouge
 Louisiana 70816
 Tel 225 292 1004
 Fax 225 218 9677
 www.arcadis-us.com

Subject:
 Solicitation of Views and
 Initiation of Section 106 Consultation
 US 11 Norfolk Southern Railroad
 Environmental Assessment
 Route US 11
 St. Tammany Parish, Louisiana
 State Project No. H.000688.2
 F.A.P No. H000688

This project has been reviewed for effects to Federal trust resources under our jurisdiction and currently protected by the Endangered Species Act of 1973 (Act). The project, as proposed, () will have no effect on those resources. This is not likely to adversely affect those resources. rcw
 This finding fulfills the requirements under Section 7(a)(2) of the Act.

Karin Boitell *5/13/13*
 Acting Supervisor Date
 Louisiana Field Office
 U.S. Fish and Wildlife Service

INFRASTRUCTURE

Date:
 3 May 2013

Dear Mr. Rieck:

Contact:
 Beth Beam

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns and reach agreeable decisions while taking into account the interests of all parties.

Extension:
 215

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

Email:
 elizabeth.beam@arcadis-us.com

A Stage 0 Feasibility Study for this project was completed in 2008. A project summary and figures are attached for your review. If you would like to review the Stage 0 Study in its entirety, please request a copy from Beth Beam by e-mail at elizabeth.beam@arcadis-us.com or by U.S. mail to Ms. Beth Beam, ARCADIS, 10352 Plaza Americana Drive, Baton Rouge, Louisiana 70816. A project overview, project study area map, and figures are attached for your review.

Our ref:
 LA003143.0000
 LDOTD/3143/C/1/kp

We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

Imagine the result



Mr. Brad Rieck
3 May 2013

Closing

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by **Monday, June 3, 2013**. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688.2 in your reply.

Sincerely,

ARCADIS U.S., Inc.

A handwritten signature in black ink that reads "Elizabeth Beam". The signature is fluid and cursive, with the first name and last name clearly distinguishable.

Elizabeth Beam, AICP
Senior Planner/Scientist

Attachments



DEPARTMENT OF THE ARMY
NEW ORLEANS DISTRICT, CORPS OF ENGINEERS
P. O. BOX 60267
NEW ORLEANS, LOUISIANA 70160-0267

RECEIVED
JUL 05 2013

BY:

JUL 03 2013

REPLY TO
ATTENTION OF

Operations Division
Operations Manager,
Completed Works

Elizabeth Beam, AICP
Senior Planner/ Scientist
ARCADIS U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge, LA 70816

Dear Ms. Beam:

This is in response to your Solicitation of Views request dated May 3, 2013, concerning the Route US-11 widening and bridge replacement at Slidell in St. Tammany Parish, Louisiana.

We have reviewed your request for potential Department of the Army regulatory requirements and impacts on any Department of the Army projects.

We do not anticipate any adverse impacts to any Corps of Engineers projects.

Based on the limited information provided, we have determined that information and signatures obtained from recent maps, aerial photography, and local soil surveys concerning the proposed project are indicative of the occurrence of waters of the United States, including wetlands. Department of the Army (DA) permits are required prior to the deposition and/or redistribution of dredged or fill material into jurisdictional waters and wetlands.

This preliminary determination is advisory in nature. If an approved delineation is needed, please furnish us with the detailed field data concerning vegetations, soils, and hydrology that we require for all jurisdictional decisions. The fact that a field wetland delineation/determination has not been completed does not alleviate your responsibility to obtain the proper DA permits prior to working in jurisdictional wetlands or waters occurring on this property.

Please be advised that this property is in the Louisiana Coastal Zone. For additional information regarding coastal use permit requirements, contact Ms. Christine Charrier, Office of Coastal Management, Louisiana Department of Natural Resources at (225) 342 7953.

Off-site locations of activities such as borrow, disposals, haul-and detour-roads and work mobilization site developments may be subject to Department of the Army regulatory

requirements and may have an impact on a Department of the Army project.

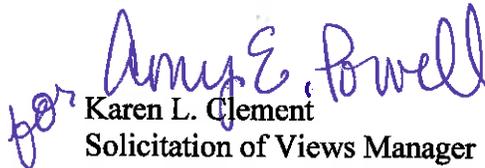
You should apply for said permit well in advance of the work to be performed. The application should include sufficiently detailed maps, drawings, photographs, and descriptive text for accurate evaluation of the proposal.

Please contact Mr. Robert Heffner, of our Regulatory Branch by telephone at (504) 862-1288, or by e-mail at Robert.A.Heffner@usace.army.mil for questions concerning wetlands determinations or need for on-site evaluations. Questions concerning regulatory permit requirements may be addressed to Mr. Michael Farabee by telephone at (504) 862-2292 or by email at Michael.V.Farabee@usace.army.mil.

Future correspondence concerning this matter should reference our account number MVN-2013-01421-SA. This will allow us to more easily locate records of previous correspondence, and thus provide a quicker response.

We apologize for missing the target date of June 3, 2013 listed in your request. Thank you for your patience in this matter.

Sincerely,


for Karen L. Clement
Solicitation of Views Manager

Copy Furnished:

Ms. Christine Charrier
Coastal Zone Management
Department of Natural Resources
Post Office Box 44487
Baton Rouge, Louisiana 70804-4487

Beam, Elizabeth

From: Beth Altazan-Dixon <Beth.Dixon@LA.GOV>
Sent: Friday, May 10, 2013 10:23 AM
To: Beam, Elizabeth
Subject: DEQ SOV 130503/0800US 11 Norfolk Southern Railroad EA-Route US 11

May 10, 2013

Elizabeth Beam, Senior Planner/Scientist
ARCADIS U.S., Inc.
10352 Plaza Americana Drive
Baton Rouge, LA 70816
Elizabeth.Beam@arcadis-us.com

RE: 130503/0800 US 11 Norfolk Southern Railroad EA-Route US 11
H.000688.2 LADOTD Funding
St. Tammany Parish

Dear Ms. Beam:

The Department of Environmental Quality (LDEQ), Business and Community Outreach Division has received your request for comments on the above referenced project.

After reviewing your request, the Department has no objections based on the information provided in your submittal. However, for your information, the following general comments have been included. Please be advised that if you should encounter a problem during the implementation of this project, you should immediately notify LDEQ's Single-Point-of-contact (SPOC) at (225) 219-3640.

- Please take any necessary steps to obtain and/or update all necessary approvals and environmental permits regarding this proposed project.
- If your project results in a discharge to waters of the state, submittal of a Louisiana Pollutant Discharge Elimination System (LPDES) application may be necessary.
- If the project results in a discharge of wastewater to an existing wastewater treatment system, that wastewater treatment system may need to modify its LPDES permit before accepting the additional wastewater.
- All precautions should be observed to control nonpoint source pollution from construction activities. LDEQ has stormwater general permits for construction areas equal to or greater than one acre. It is recommended that you contact the LDEQ Water Permits Division at (225) 219-9371 to determine if your proposed project requires a permit.
- If your project will include a sanitary wastewater treatment facility, a Sewage Sludge and Biosolids Use or Disposal Permit application or Notice of Intent must be submitted no later than January 1, 2013. Additional information may be obtained on the LDEQ website at <http://www.deq.louisiana.gov/portal/tabid/2296/Default.aspx> or by contacting the LDEQ Water Permits Division at (225) 219- 9371.
- If any of the proposed work is located in wetlands or other areas subject to the jurisdiction of the U.S. Army Corps of Engineers, you should contact the Corps directly regarding permitting issues. If a Corps permit is required, part of the application process may involve a water quality certification from LDEQ.
- All precautions should be observed to protect the groundwater of the region.
- Please be advised that water softeners generate wastewaters that may require special limitations depending on local water quality considerations. Therefore if your water system improvements include water softeners, you are advised to contact the LDEQ Water Permits to determine if special water quality-based limitations will be necessary.
- Any renovation or remodeling must comply with LAC 33:III.Chapter 28, Lead-Based Paint Activities; LAC 33:III.Chapter 27, Asbestos-Containing Materials in Schools and State Buildings (includes all training and accreditation); and LAC 33:III.5151, Emission Standard for Asbestos for any renovations or demolitions.
- If any solid or hazardous wastes, or soils and/or groundwater contaminated with hazardous constituents are encountered during the project, notification to LDEQ's Single-Point-of-Contact (SPOC) at (225) 219-3640 is required. Additionally, precautions should be taken to protect workers from these hazardous constituents.

Currently, St. Tammany Parish is classified as attainment with the National Ambient Air Quality Standards and has no general conformity determination obligations.

Please send all future requests to my attention. If you have any questions, please feel free to contact me at (225) 219-3958 or by email at beth.dixon@la.gov.

Sincerely,



Beth Altazan-Dixon, EPS III
Performance Management
LDEQ/Office of the Secretary
Business and Community Outreach and Incentives Division
P.O. Box 4301 (602 N. 5th Street)
Baton Rouge, LA 70821-4301
Phone: 225-219-3955
Fax: 225-325-8148
Email: beth.dixon@la.gov



APPENDIX C-2

Solicitation of Views Tribes

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Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Mr. Bryant J. Celestine
Historic Preservation Officer
Alabama Coushatta Tribe of Texas
571 State Park Road 56
Livingston, TX 77351

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Celestine:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Mr. Earl J. Barbry, Jr.
THPO
Tunica-Biloxi Indians of Louisiana
Attn: Museum Division Offices
P.O. Box 1589
Marksville, LA 71351

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Barbry:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Ms. Natalie Harjo
Historic Preservation Officer
Seminole Nation of Oklahoma
P.O. Box 1498
Wewoka, OK 74884

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Harjo:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Ms. Dana Masters
THPO
Jena Band of Choctaws
P.O. Box 14
Jena, LA 71324

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Masters:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Dr. Ian Thompson
THPO
Choctaw Nation of Oklahoma
P.O. Box 1210
Durant, OK 74702

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Dr. Thompson:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Ms. Jean Ann Lambert
THPO
Quapaw Tribe of Oklahoma
5681 S. 630 Road
Quapaw, OK 74363

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Lambert:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Mr. Kenneth Carleton
Archaeologist & THPO
Mississippi Band of Choctaw Indians
P.O. Box 6257
Philadelphia, MS 39350

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Carleton:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Ms. Kimberly Walden
Cultural Director
Chitimacha Tribe of Louisiana
P.O. Box 661
Clarenton, LA 70523

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Walden:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Dr. Linda Langley
Cultural Preservation Officer
Coushatta Tribe of Louisiana
P.O. Box 818
Elton, LA 70532

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Dr. Langley:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Mr. Robert Cast
THPO
Caddo Nation
P.O. Box 487
Binger, OK 73009

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Cast:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



Environmental Section
P.O. Box 94245 | Baton Rouge, LA 70804-9245
phone: 225-242-4502 | fax: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

May 8, 2013

Mr. Paul Bachouse
THPO
Seminole Tribe of Florida
30290 Josie Billie Highway, PMB 1004
Clewiston, FL 33440

Subject:
State Project No. H.000688.2
F.A.P No. H000688
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Bachouse:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Attachments
cc: Scott Nelson (FHWA)



**Solicitation of Views
US 11 Norfolk Southern Railroad
Environmental Assessment
Route US 11
St. Tammany Parish, Louisiana
State Project No. H.000688.2
F.A.P No. H000688**

PROJECT OVERVIEW

Description of the Project. The US 11 Norfolk Southern Railroad project includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes for the segment of US Highway 11 (US 11) from Interstate 12 (I-12) south to US Highway 190 (US 190; Gause Boulevard) in Slidell, St. Tammany Parish, Louisiana. The logical termini for the proposed project are US 11 at I-12 and US 11 at US 190. The project consists of providing all necessary services required to prepare an Environmental Assessment (EA) in accordance with the National Environmental Policy Act, as amended, and the Federal Highway Administration's regulations and guidelines.

Purpose and Need for the Project. The purpose of the proposed project is to replace a structurally deficient bridge, while increasing capacity and alleviating congestion.

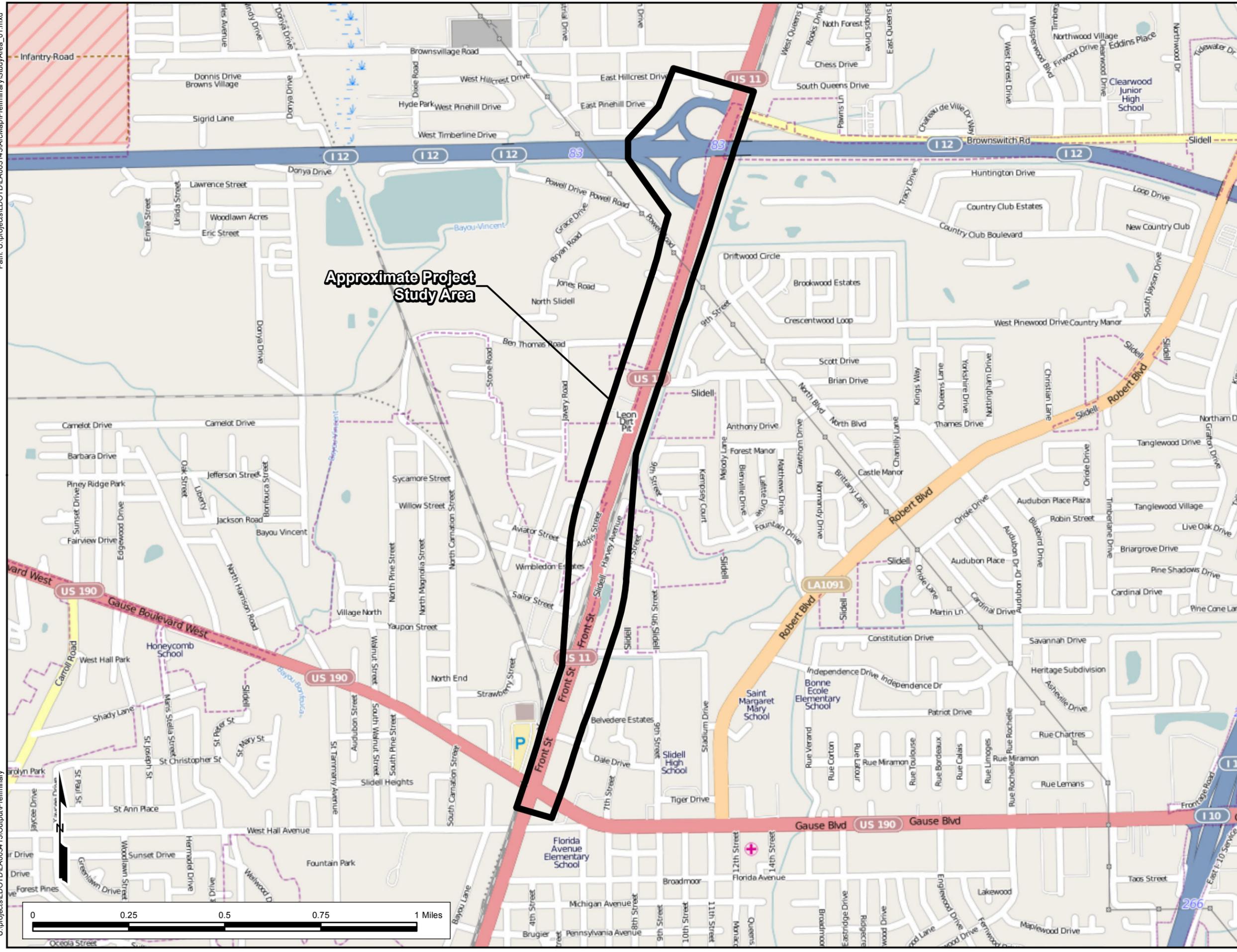
Project Study Area. The project area is located to the east of Airport Road and west of Robert Boulevard (LA 1091) in Slidell, Louisiana. The project study area extends south along US 11 from I-12 to the intersection of US 11 at US 190. The EA will investigate the potential for effects to cultural resources, threatened and endangered species, natural resources, and the human environment within the study area. The proposed project would improve existing roadway infrastructure and will require additional right-of-way. A location map that illustrates the project study area is attached (Figure 1).

Build and No Build Alternatives. The bridge over the Norfolk Southern Railroad, constructed in 1937, is a two-lane concrete/steel bridge with no shoulders. The bridge spans 828 feet and is 28 feet in width. The most recent LADOTD structure inventory and appraisal (March 2010) and bridge inspection report (May 2009) identified the bridge as structurally deficient noting poor condition of the substructure and superstructure; severe collision damage to the rails at multiple locations; spalling; and corrosion with section loss.

Currently, one build alternative will be considered for evaluation in the EA. This alternative includes roadway widening from the two lanes to four lanes with median and shoulders. This widening will require the replacement of the existing bridge over the railroad (Figures 2-a, 2-b). Up to two additional alternatives will be developed for evaluation in the EA. The No Build Alternative, which assumes that this project will not be built, will also be considered.

Recent US 11 Corridor Improvements. Several improvements have been recently completed within the US 11 corridor. Within the project study area, these improvements include the addition of left-turn lanes on US 11 north and southbound at the east and westbound approaches to I-12; additional turn lanes on the eastbound and westbound I-12 ramps; addition of a right-turn lane along northbound US 11 to Brown Switch Road; and the addition of a right-turn lane from I-12 south along US 11 to Powell Drive.

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PROJECT LOCATION AND STUDY AREA MAP

US 11 Norfolk Southern Railroad Environmental Assessment
 Route US 11
 St. Tammany Parish, Louisiana
 State Project No. H.000688.2
 F.A.P No. H000688

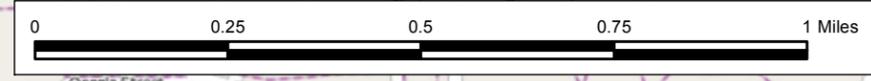
Legend

Preliminary Study Area

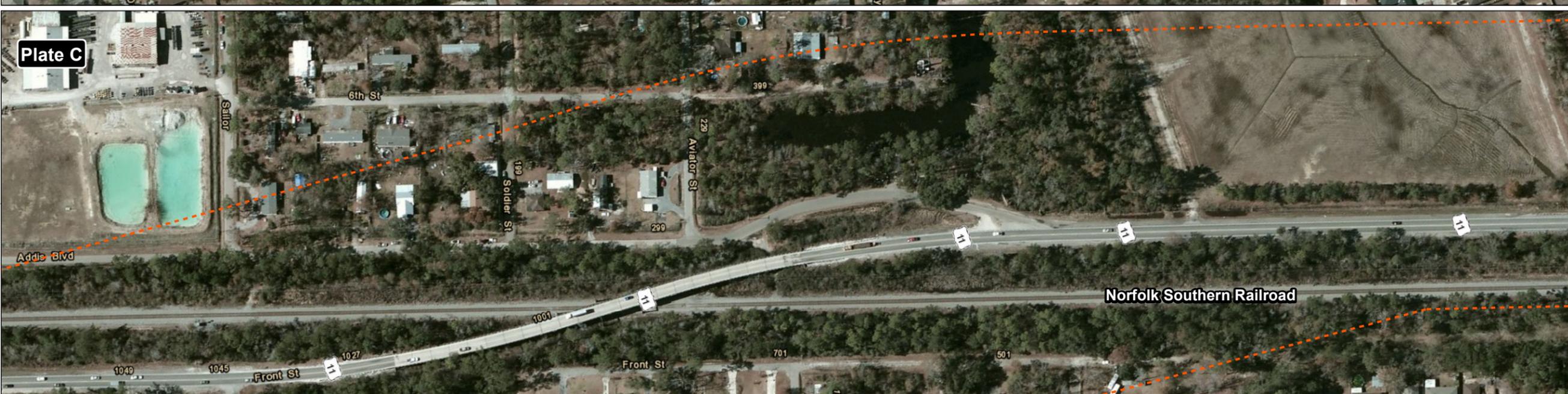
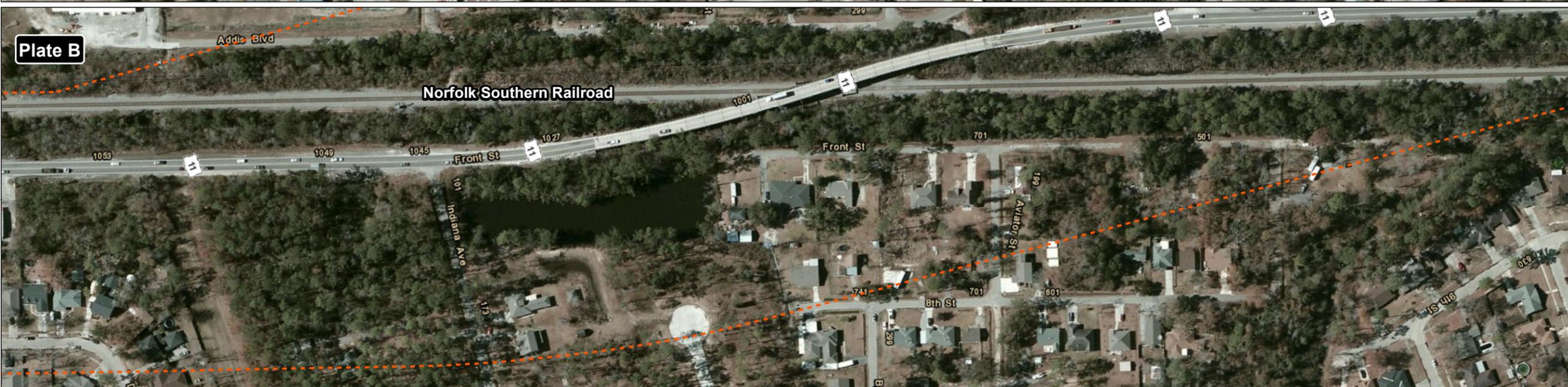
Reference: ESRI Base Map Data, Open Street Map



DRAWING BY: JWC	DATE: 05.01.2013
PROJECT NUMBER: LA003143.0000	FIGURE NUMBER: 1



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US 11 Norfolk Southern Railroad
 Environmental Assessment
 State Project No. H.000688.2
 E.A.P No. H000688
 Route US 11
 St. Tammany Parish, Louisiana

CORRIDOR PLATES

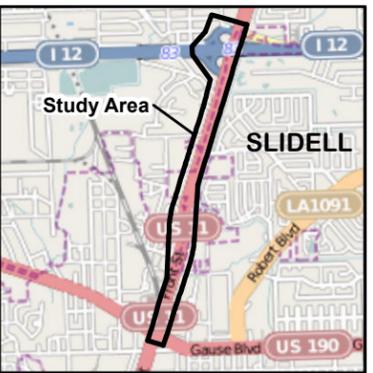
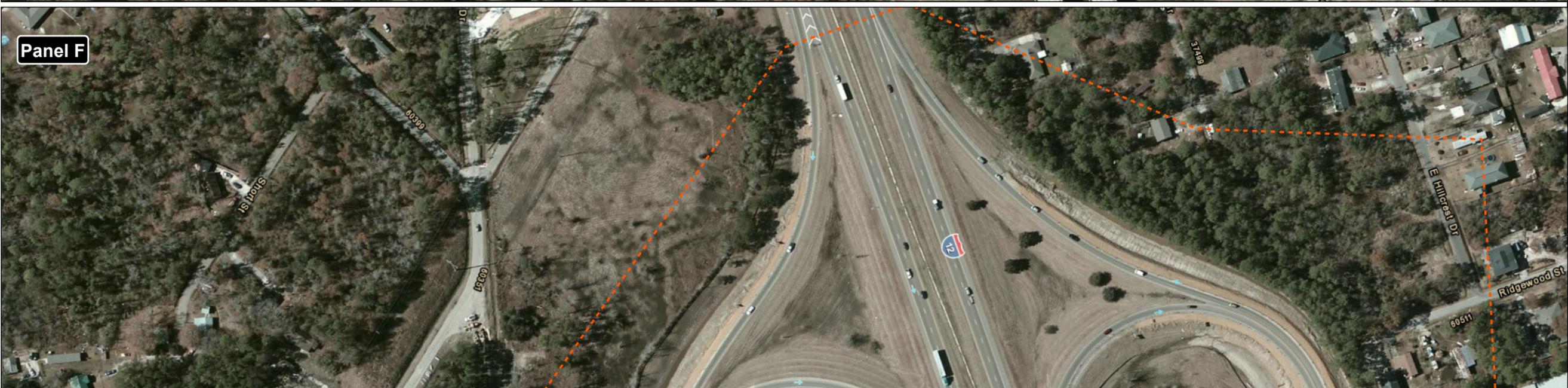
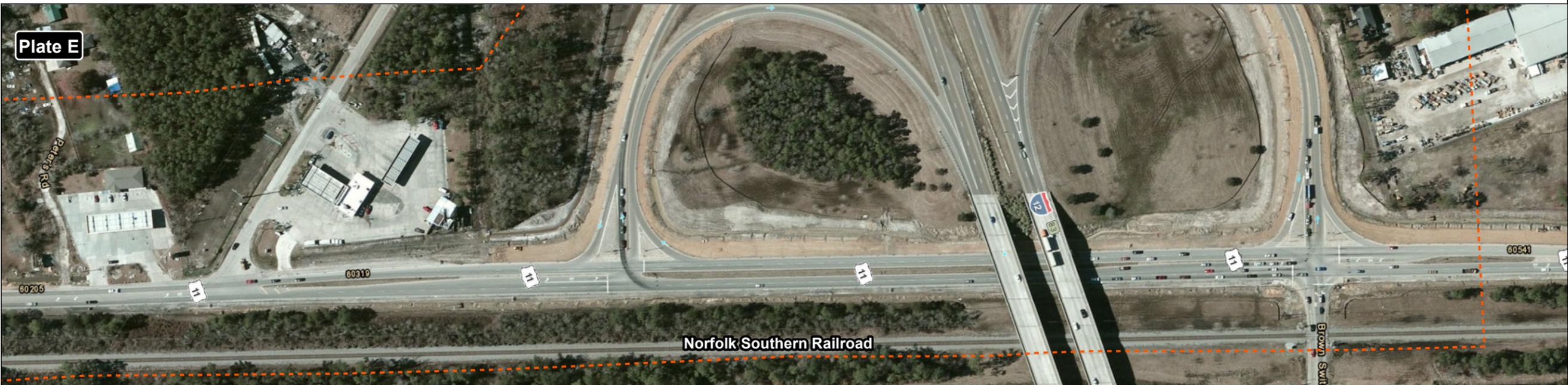
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--- Preliminary Study Area

Reference: ESRI Base Maps & Imagery

Date: 04/23/2013	Project Number: LA003413.0000
Figure No.:	
2 - a	

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US 11 Norfolk Southern Railroad
 Environmental Assessment
 State Project No. H.000688.2
 F.A.P No. H000688
 Route US 11
 St. Tammany Parish, Louisiana

CORRIDOR PLATES

Legend
 - - - Preliminary Study Area



Reference: ESRI Base Maps & Imagery



Date: 04/23/2013 Project Number: LA003413.0000

Figure No.: **2 - b**

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US 11 Norfolk Southern Railroad
SOV Mailing List
Environmental Assessment
H.000688.2

Cat 1	Cat 2	Salutation	Courtesy	First	M	Last Name	Title	Org 1	Org 2	Address	City	State	Zip
Federal	T	Mr. Celestine	Mr.	Bryant	J.	Celestine	Historic Preservation Officer	Alabama Coushatta Tribe of Texas		571 State Park Road 56	Livingston	TX	77351
Federal	T	Mr. Barbry	Mr.	Earl	J.	Barbry, Jr.	THPO	Tunica-Biloxi Indians of Louisiana	Attn: Museum Division Offices	P.O. Box 1589	Marksville	LA	71351
Federal	T	Ms. Harjo	Ms.	Natalie		Harjo	Historic Preservation Officer	Seminole Nation of Oklahoma		P.O. Box 1498	Wewoka	OK	74884
Federal	T	Ms. Masters	Ms.	Dana		Masters	THPO	Jena Band of Choctaws		P.O. Box 14	Jena	LA	71324
Federal	T	Dr. Thompson	Dr.	Ian		Thompson	THPO	Choctaw Nation of Oklahoma		P.O. Box 1210	Durant	OK	74702
Federal	T	Ms. Lambert	Ms.	Jean Ann		Lambert	THPO	Quapaw Tribe of Oklahoma		5681 S. 630 Road	Quapaw	OK	74363
Federal	T	Mr. Carleton	Mr.	Kenneth		Carleton	Archaeologist & THPO	Mississippi Band of Choctaw Indians		P.O. Box 6257	Philadelphia	MS	39350
Federal	T	Ms. Walden	Ms.	Kimberly		Walden	Cultural Director	Chitimacha Tribe of Louisiana		P.O. Box 661	Clarenton	LA	70523
Federal	T	Dr. Langley	Dr.	Linda		Langley	Cultural Preservation Officer	Coushatta Tribe of Louisiana		P.O. Box 818	Elton	LA	70532
Federal	T	Mr. Cast	Mr.	Robert		Cast	THPO	Caddo Nation		P.O. Box 487	Binger	OK	73009
Federal	T	Mr. Bachouse	Mr.	Paul		Bachouse	THPO	Seminole Tribe of Florida		30290 Josie Billie Highway, PMB 1004	Clewiston	FL	33440

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Beam, Elizabeth

From: Robert Lott <Robert.Lott@LA.GOV>
Sent: Thursday, November 20, 2014 1:33 PM
To: Beam, Elizabeth
Subject: FW: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

FYI

Thanks,
Bobby Lott
LADOTD – Environmental Section
225.242.4504

From: Michelle Hanks
Sent: Thursday, November 20, 2014 9:14 AM
To: Robert Lott
Subject: FW: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Bobby,

This should be added to your records and documentation.

Michelle

From: Lindsey Bilyeu [<mailto:lbilyeu@choctawnation.com>]
Sent: Monday, October 06, 2014 1:55 PM
To: Michelle Hanks
Subject: RE: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Ms. Hanks,

The Choctaw Nation of Oklahoma thanks the Louisiana DOTD for the correspondence regarding the above referenced project. St. Tammany Parish, LA lies in the Choctaw Nation of Oklahoma's area of historic interest. The Choctaw Nation is unaware of any cultural or sacred sites located in the immediate project area. Due to the presence of historic sites located in the area and due to the SHPO's concerns, the Choctaw Nation Historic Preservation Department respectfully defers to the Louisiana SHPO. However, as the project lies in an area of historic interest to the Tribe, we ask that work be stopped and our office contacted immediately in the event that Native American cultural material or human remains are encountered. If you have any questions, please contact our office at 580-924-8280 ext. 2631.

Thank You,

Lindsey D. Bilyeu
Senior Section 106 Reviewer
Historic Preservation Department
Choctaw Nation of Oklahoma
P.O. Box 1210
Durant, OK 74701
580-924-8280 ext. 2631

From: Michelle Hanks [<mailto:Michelle.Hanks@LA.GOV>]
Sent: Monday, September 15, 2014 2:06 PM
To: Lindsey Bilyeu
Subject: FW: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Lindsey,

My apologies, I intended to include you in the original email this morning. Please let me know if you have any questions.

Thank you,
Michelle

From: Michelle Hanks
Sent: Monday, September 15, 2014 11:29 AM
To: 'ithompson@choctawnation.com'
Cc: 'Beam, Elizabeth'; Robert Lott; robert.mahoney@dot.gov
Subject: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Good morning Ian,

Attached are the requested documents and a memo for your records regarding the above captioned project. If you have any questions or concerns, please let me know.

Thank you,

Michelle

Michelle Whipp Hanks, B.A., M.A.
Environmental Impact Specialist
La DOTD-Environmental
(225) 242-4514

This message is intended only for the use of the individual or entity to which it is addressed and may contain information that is privileged, confidential and exempt from disclosure. If you have received this message in error, you are hereby notified that we do not consent to any reading, dissemination, distribution or copying of this message. If you have received this communication in error, please notify the sender immediately and destroy the transmitted information. Please note that any view or opinions presented in this email are solely those of the author and do not necessarily represent those of the Choctaw Nation.

Beam, Elizabeth

From: Robert Lott <Robert.Lott@LA.GOV>
Sent: Monday, July 01, 2013 12:58 PM
To: Beam, Elizabeth
Subject: FW: LADOT, H.000688.2, US 11 Norfolk Southern Railroad, St. Tammany Parish, LA

Follow Up Flag: Follow up
Flag Status: Flagged

FYI

Thanks,
Bobby Lott
DOTD - Environmental Section
225-242-4504

From: Noel Ardoin
Sent: Monday, July 01, 2013 12:01 PM
To: Robert Lott
Subject: FW: LADOT, H.000688.2, US 11 Norfolk Southern Railroad, St. Tammany Parish, LA

From: Johnnie L. Jacobs [<mailto:jjacobs@choctawnation.com>]
Sent: Wednesday, June 26, 2013 10:41 AM
To: Noel Ardoin
Subject: LADOT, H.000688.2, US 11 Norfolk Southern Railroad, St. Tammany Parish, LA

Dear Noel,

Thank you for the correspondence regarding the above referenced project. St. Tammany Parish is within the historic area of interest to the Choctaw Nation of Oklahoma. Please forward our office a copy of the SHPO comments once received. If you have any further questions, please let us know.

Thank you,

Ms. Johnnie Jacobs
NHPA Section 106 Coordinator
Choctaw Nation of Oklahoma
Historic Preservation Department
P.O. Box 1210
Durant, OK 74701
jjacobs@choctawnation.com

This message is intended only for the use of the individual or entity to which it is addressed and may contain information that is privileged, confidential and exempt from disclosure. If you have received this message in error, you are hereby notified that we do not consent to any reading, dissemination, distribution or copying of this message. If you have received this communication in error, please notify the sender immediately and destroy the transmitted information. Please note that any view or opinions presented in this email are solely those of the author and do not necessarily represent those of the Choctaw Nation.

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APPENDIX C-3

Section 106

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JAY DARDENNE
LIEUTENANT GOVERNOR

State of Louisiana
OFFICE OF THE LIEUTENANT GOVERNOR
DEPARTMENT OF CULTURE, RECREATION & TOURISM
OFFICE OF CULTURAL DEVELOPMENT

CHARLES R. DAVIS
DEPUTY SECRETARY

FAM BREAU
ASSISTANT SECRETARY

9 July 2014

Noel Ardoin
Environmental Engineer
Dept of Transportation and Development
PO Box 94245
Baton Rouge, LA 70804-9245

Re: Draft Report
La Division of Archaeology Report No. 22-4693
Phase I Cultural Resources Survey for the Environmental Assessment of the US 11 Norfolk Southern Railroad, Route US 11, St. Tammany Parish, Louisiana

Dear Ms. Ardoin:

We acknowledge receipt your letter dated 5 June 2014 and two copies of the above-referenced report. We have completed our review of this report and offer the following comments.

In reference to historic standing structures, we concur with your evaluation that the US 11 Norfolk Southern Railroad Bridge (52-02393), Old Ice House (52-02387), and 1184 Front Street residential property (52-02398) are eligible for listing in the National Register of Historic Places. However, we are of the opinion that the residential property (52-02398) would be eligible under Criterion C as a Craftsman Style house in the context of the City of Slidell and the Old Ice House (52-02387) would be eligible under Criterion A for its role as the first power station for the City of Slidell. The replacement of the US 11 Norfolk Southern Bridge (52-02393) and the expansion of the required right-of-way upon the Old Ice House (52-02387) and 1184 Front Street residence (52-02398) would constitute an Adverse Effect on historic properties as defined in the Section 106 Regulations (36 CFR 800). As such, we invite you to consult with our office in order to begin the Memorandum of Agreement process.

We concur that the portion of site 16ST255 within the project right-of-way is not eligible for nomination to the National Register. Our office has no further concerns for archaeological resources for this project.

We look forward to receiving two bound copies of the final report along with a pdf of the report. If you have any questions, please contact Chip McGimsey in the Division of Archaeology by email at cmcgimsey@crt.la.gov or by phone at 225-219-4598.

Sincerely,

Pam Breau
State Historic Preservation Officer

PB:crm



Environmental Section
PO Box 94245 | Baton Rouge, LA 70804-9245
ph: 225-242-4502 | fx: 225-242-4500

Bobby Jindal, Governor
Sherri H. LeBas, P.E., Secretary

June 5, 2014

STATE PROJECT NO. H.000688
F.A.P. NO. H000688
NAME: US 11 NORFOLK SOUTHERN R.R.OVERPASS
ROUTE: US 11
STRUCTURE NO.: 62520180400531(Recall No. 059090)
PARISH: ST. TAMMANY

Ms. Pam Breaux
State Historic Preservation Officer
Office of Cultural Development
Louisiana Department of Culture, Recreation and Tourism
P.O. Box 44247, Capitol Station
Baton Rouge, LA 70804

SUBJECT: Draft Report: *Phase I Cultural Resources Survey for the Environmental Assessment of the US 11 Norfolk Southern Railroad, Route US 11, St. Tammany Parish, Louisiana*

Dear Ms. Breaux:

Enclosed for your review and comment are two copies of the above-titled draft Cultural Resource Survey report, prepared by Earth Search, Inc., dated April 2014.

The Louisiana Department of Transportation and Development (LADOTD) is proposing a project to replace the existing two-lane bridge over the Norfolk Southern Railroad with a four-lane bridge on US 11 in Slidell, St. Tammany Parish. Structure No. 62520180400531 (Recall No. 059090) is a concrete deck and bents with steel I-beam bridge, built in 1937, and is 828 feet long and 24 feet wide. The project proposes widening the segment of US 11 from I-12, south to US 190 (Gause Boulevard), from two lanes to four lanes. During construction, the traffic will be maintained on the existing bridge. During bridge construction, two lanes of the new bridge will be constructed and traffic will be shifted to the new bridge while the current bridge is removed and the second half of the bridge is constructed. No roadway closures are anticipated along US 11. The project is approximately 1.7 miles in length. Approximately 10.7 acres of additional right-of-way (ROW) will be required.

The Area of Potential Effects (APE) for this proposed project is the limits of the required ROW, 10.7 acres. The study identified all structures greater than 50 years of age within a .25 mile (400 m) buffer on either side of the existing highway centerline. On June 18, 2013 and November 13,

2013, staff from Earth Search, Inc. (ESI) conducted a cultural resources survey to identify historic properties. One archaeological site (16ST255) was identified within the direct APE (see Table 1). It is recommended ineligible for the National Register of Historic Places (NRHP) and no additional archaeological investigations of the site within the direct APE are recommended.

Five standing structures, greater than fifty years of age were identified within the direct APE: 52-02387, 52-02398, 52-02380, 52-02366, and 52-02393 (see Table 1). Two of these structures are recommended eligible, the bridge has been previously determined eligible for listing on the NRHP and the remaining structures are recommended not eligible. Although there were a number of structures greater than 50 years of age identified within the .25 mile buffer the proposed project will have no impact on these properties and therefore no further consultation is requested.

Table 1: Standing Structures and Archaeological Sites identified within the direct APE.

Standing Structure No./ Archaeological Site No.	Description	NRHP Eligibility	Effect Determination	Recommendations
52-02387	1398 Front St.: Old Ice House (currently used as restaurant)	Eligible	Adverse Effect	Sec 106 Consultation to seek ways to avoid, minimize, or mitigate Adverse Effect
52-02398	1184 Front St: Abandoned Residence with detached garage	Eligible	Adverse Effect	Sec 106 Consultation to seek ways to avoid, minimize, or mitigate Adverse Effect
52-02380	1104 Front St.: Business Building Slidell Vet Clinic	Not Eligible	N/A	No further work recommendation
52-02366	Highway 11 and Boykin Lane (no address): Abandoned building unknown function	Not Eligible	N/A	No further work recommendation
52-02393	US 11 Norfolk Southern Railroad Bridge Str. No. 62520180400531	Previously Determined Eligible	Adverse Effect	Sec 106 Consultation to seek ways to avoid, minimize, or mitigate Adverse Effect

STATE PROJECT NO: H.000688
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16ST255	Remains of Historic Gas Station	Not Eligible	N/A	No further work recommendation
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The Old Ice House (52-02387), located at 1398 Front Street, is recommended eligible for the NRHP under Criterion C; the building retains its integrity of form, style, and materials. It is also recommended eligible und Criterion D; it is possible that the building and intact equipment and ice machinery could provide important information regarding early, rural utilities and ice production/storage. This building lies within the direct APE, approximately 16 meters east of the existing pavement edge. It is recommended that the Old Ice House be avoided during construction if possible. If avoidance is not possible, mitigation measures should be taken.

The residential structure (52-02398), located at 1184 Front Street, is recommended eligible for the NRHP under Criterion A as an example of a property that exhibits characteristics of a broad pattern in our history associated with transportation and the shift from horse-drawn carriage to automobile use. The residence has a detached automobile garage and secondary residential apartment above the garage. The structure is also recommended eligible under Criterion C for its integrity of form, style, and material. It is also a locally unique form without substantial modern modifications. The building lies within the direct APE, approximately 10 meters east of the existing pavement edge. It is recommended that the building be avoided during construction if possible. If avoidance is not possible, mitigation measures should be taken.

LADOTD, in conjunction with FHWA, request your concurrence on the eligibility determinations of Structures 52-02387, 52-02398, 52-02380, 52-02366, and site 16ST255 and the proposed recommendations (see Table 1). If you have any questions or comments, contact Michelle Whipp at 225-242-4514 or michelle.whipp@la.gov.

Sincerely,

Noel Ardoin
Environmental Engineer Administrator

Enclosures

NA/mw

cc: SHPO File

FHWA

Robert Lott via email: Robert.lott@la.gov



Preserving America's Heritage

September 15, 2014

Mr. Carl Highsmith
Project Delivery Team Leader
Federal Highway Administration
Louisiana Division
5304 Flanders Drive, Suite A
Baton Rouge, LA 70808

Ref: *Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad
St. Tammany Parish, Louisiana*

Dear Mr. Highsmith:

The Advisory Council on Historic Preservation (ACHP) has received your notification and supporting documentation regarding the adverse effects of the referenced undertaking on a property or properties listed or eligible for listing in the National Register of Historic Places. Based upon the information provided, we have concluded that Appendix A, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, of our regulations, "Protection of Historic Properties" (36 CFR Part 800), does not apply to this undertaking. Accordingly, we do not believe that our participation in the consultation to resolve adverse effects is needed. However, if we receive a request for participation from the State Historic Preservation Officer (SHPO), Tribal Historic Preservation Officer (THPO), affected Indian tribe, a consulting party, or other party, we may reconsider this decision. Additionally, should circumstances change, and it is determined that our participation is needed to conclude the consultation process, please notify us.

Pursuant to 36 CFR §800.6(b)(1)(iv), you will need to file the final Memorandum of Agreement (MOA), developed in consultation with the Louisiana State Historic Preservation Office (SHPO), and any other consulting parties, and related documentation with the ACHP at the conclusion of the consultation process. The filing of the MOA, and supporting documentation with the ACHP is required in order to complete the requirements of Section 106 of the National Historic Preservation Act.

Thank you for providing us with the notification of adverse effect. If you have any questions or require further assistance, please contact Mr. Chris Wilson at 202-517- 0229 or via e-mail at cwilson@achp.gov.

Sincerely,

LaShavio Johnson
Historic Preservation Technician
Office of Federal Agency Programs

ADVISORY COUNCIL ON HISTORIC PRESERVATION

401 F Street NW, Suite 308 • Washington, DC 20001-2637
Phone: 202-517-0200 • Fax: 202-517-6381 • achp@achp.gov • www.achp.gov

Beam, Elizabeth

From: Michelle Hanks <Michelle.Hanks@LA.GOV>
Sent: Monday, September 15, 2014 9:27 AM
To: Beam, Elizabeth; Robert Lott
Subject: FW: Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad, St. Tammany Parish, LA (H000688)
Attachments: la.fhwa.replacement of US 11 bridge over norfolk southern railroad.np.15sept14.pdf

Beth,

I just received this. Another piece completed.

Michelle

From: Robert.Mahoney@dot.gov [<mailto:Robert.Mahoney@dot.gov>]
Sent: Monday, September 15, 2014 9:15 AM
To: Michelle Hanks; Michelle Hanks; Noel Ardoin
Cc: Scott.Nelson@dot.gov; Carl.Highsmith@dot.gov
Subject: FW: Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad, St. Tammany Parish, LA (H000688)

The ACHP will not participate, as of now, in development of the MOA for the subject project.

A copy of the signed MOA will need to be sent to the ACHP.

BobM.

From: OFAP [<mailto:OFAP2@achp.gov>]
Sent: Monday, September 15, 2014 8:23 AM
To: Highsmith, Carl (FHWA)
Cc: Mahoney, Robert (FHWA); Zachary, Denise (FHWA); Naber, MaryAnn (FHWA); Phil Boggan
Subject: Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad, St. Tammany Parish, LA

From: Office of Federal Agency Programs

Advisory Council on Historic Preservation

Attached is our letter on the subject undertaking (in Adobe Acrobat PDF format)

If you have any questions concerning our letter, please contact:

Chris Wilson
cwilson@achp.gov
202 517-0229
Case # 8403

Note: Please do not reply to this email.